



# CITY OF ANN ARBOR ENGINEERING

## EARHART ROAD IMPROVEMENTS (GEDDES - US23)

### NOTES

FOR PROTECTION OF UNDERGROUND UTILITIES AND IN CONFORMANCE WITH PUBLIC ACT 174 OF 2013, THE CONTRACTOR SHALL CALL 811 OR 1-800-482-7171 A MINIMUM OF THREE FULL WORKING DAYS, EXCLUDING SATURDAYS, SUNDAYS, AND HOLIDAYS, PRIOR TO BEGINNING EACH EXCAVATION IN AREAS WHERE PUBLIC UTILITIES HAVE NOT BEEN PREVIOUSLY LOCATED. MEMBERS WILL THUS BE ROUTINELY NOTIFIED. THIS DOES NOT RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY OF NOTIFYING UTILITY OWNERS WHO MAY NOT BE A PART OF THE "MISS DIG" ALERT SYSTEM.

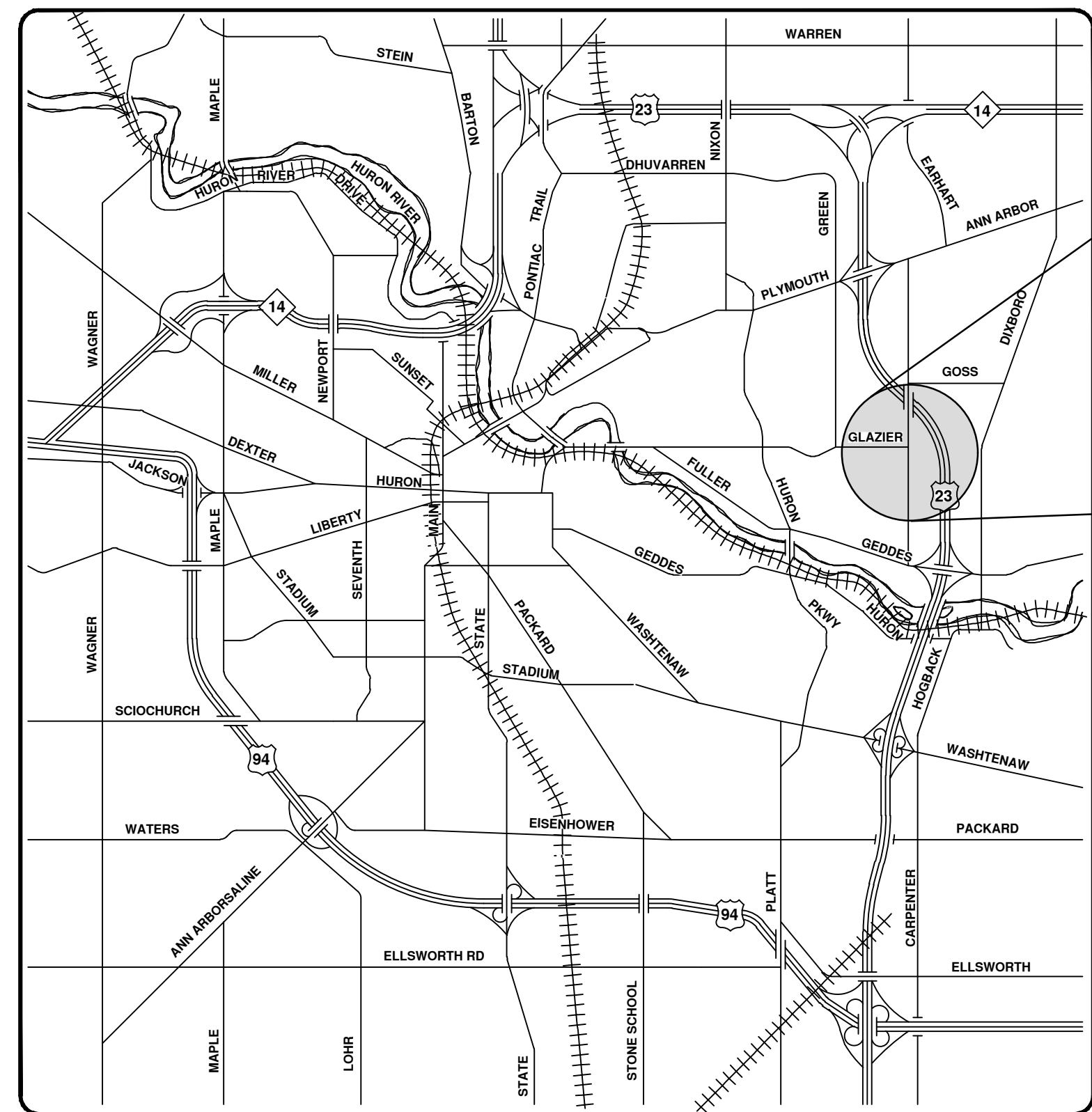
THE UNDERGROUND LOCATIONS SHOWN FOR NATURAL GAS, TELEPHONE, ELECTRICAL POWER, CABLE TV AND FIBER OPTIC LINES ARE APPROXIMATE. THE CITY OF ANN ARBOR ASSUMES NO RESPONSIBILITY FOR THEIR ACCURATE REPRESENTATION IN THIS DRAWING. MISS DIG MUST BE CONTACTED PRIOR TO CONSTRUCTION TO LOCATE THESE UTILITIES.

THE CONSTRUCTION COVERED BY THESE PLANS SHALL CONFORM TO THE 1994 EDITION OF THE CITY OF ANN ARBOR PUBLIC SERVICES DEPARTMENT STANDARD SPECIFICATIONS, ITS DETAILS, WHICH ARE INCLUDED BY REFERENCE, AND THIS PROJECT'S CONTRACT DOCUMENTS. THE OMISSION OF ANY CURRENT STANDARD DETAIL DOES NOT RELIEVE THE CONTRACTOR FROM THIS REQUIREMENT.

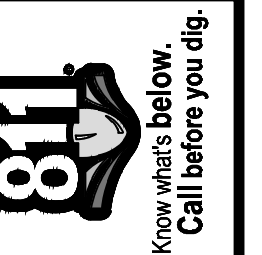
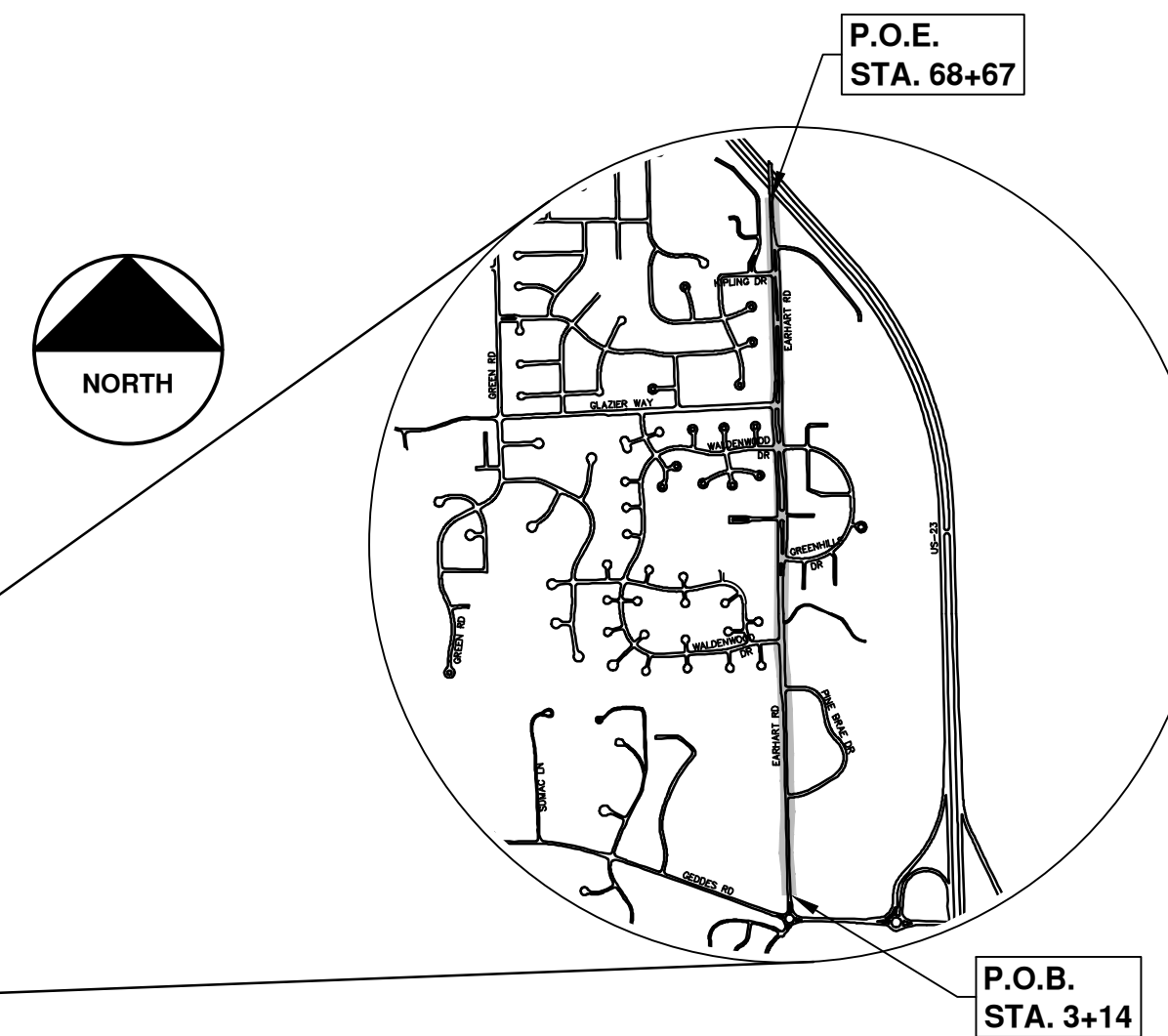
Sheet List Table	
Sheet Number	Sheet Title
1	Cover Sheet
2	Standard Notes
3	Legend
4	SESC Notes and Details
5	Details
6	Details
7	Details
8	Earhart Road Typical Sections
9	Earhart Road Typical Sections
10	Alternate Pedestrian Route (APR) Detour
11	Alternate Pedestrian Route (APR) Bypass
12	TPAR Ramps
13	TPAR Walkway Devices
14	Detour Plan
Maintenance of Traffic - Ph I	
15	Sta. 0+00 - Sta. 4+00
16	Sta. 4+00 - Sta. 22+00
17	Sta. 22+00 - Sta. 40+00
18	Sta. 40+00 - Sta. 58+00
19	Sta. 58+00 - P.O.E.
Maintenance of Traffic - Ph II	
20	Sta. 0+00 - Sta. 4+00
21	Sta. 4+00 - Sta. 22+00
22	Sta. 22+00 - Sta. 40+00
23	Sta. 40+00 - Sta. 58+00
Removals	
25	Sta. 3+14 - Sta. 11+00
26	Sta. 11+00 - Sta. 20+00
27	Sta. 20+00 - Sta. 29+00
28	Sta. 29+00 - Sta. 38+00
29	Sta. 38+00 - Sta. 47+00
30	Sta. 47+00 - Sta. 56+00
31	Sta. 56+00 - Sta. 65+00
Road Plan & Profile	
33	Sta. 3+14 - Sta. 5+50
34	Sta. 5+50 - Sta. 9+50
35	Sta. 9+50 - Sta. 13+50
36	Sta. 13+50 - Sta. 17+50
37	Sta. 17+50 - Sta. 21+50
38	Sta. 21+50 - Sta. 25+50
39	Sta. 25+50 - Sta. 29+50
40	Sta. 29+50 - Sta. 34+00
41	Sta. 34+00 - Sta. 43+00
42	Sta. 43+00 - Sta. 52+00
43	Sta. 52+00 - Sta. 61+00
44	Sta. 61+00 - Sta. 67+82

Storm Sewer Plan & Profile	
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47	R100, R101, R102, R103, R104, R105, R106
48	R107, R108, R109, R110, R111
Intersection Grades	
49	Waldenwood Dr (S) and Green Hills Dr (S)
50	Ridgmaar Sq
51	Waldenwood Dr (N) and Green Hills Dr (N)
52	Glazier Way
53	Glacier Hills Cir
54	
Pavement Markings	
55	Sta. 2+29 - Sta. 11+00
56	Sta. 11+00 - Sta. 20+00
57	Sta. 20+00 - Sta. 29+00
58	Sta. 29+00 - Sta. 38+00
59	Sta. 38+00 - Sta. 42+50
60	49 Sta. 42+50 - Sta. 47+00
61	Sta. 47+00 - Sta. 51+50
62	Sta. 51+50 - Sta. 60+50
63	
Cross Sections	
64	Northbound 31+10 to 33+50
65	Northbound 33+75 to 36+50
66	Northbound 36+75 to 39+50
67	Northbound 39+75 to 42+50
68	Northbound 42+75 to 45+50
69	Northbound 45+75 to 49+25
70	Southbound 34+00 to 36+75
71	Southbound 37+00 to 39+75
72	Southbound 40+00 to 42+75
73	Southbound 43+00 to 45+75
74	Southbound 46+00 to 49+25

R.F.P. 23-25, FILE No. 2021023

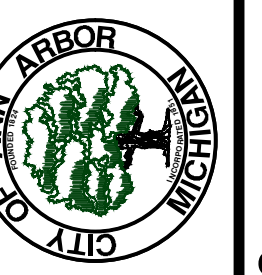


VICINITY MAP



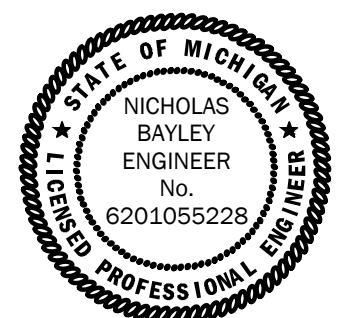
REV.	DESCRIPTION	DATE	CC/DF	NB	CHECKED
00	OUT TO BID	4-27-23			

CITY OF ANN ARBOR  
PUBLIC SERVICES  
301 EAST HURON STREET  
PO BOX 864  
ANN ARBOR MI 48106-0864  
www.a3gov.org



CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING  
SCALE  
DRAWING No. 2021023-1  
SHEET No. 1 OF 74

PREPARED UNDER THE SUPERVISION OF  
*Nicholas Bayley*  
NICHOLAS BAYLEY, P.E. - MI LICENSE No. 6201055228  
PROJECT ENGINEER  
4/27/2023  
DATE



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**GENERAL NOTES:**

1. Driveways and entrances to buildings, real property, and the like shall not be blocked except for short durations and only when approved by the Engineer. Vehicular and pedestrian access shall be maintained at all times. It shall be the Contractor's responsibility to coordinate all necessary driveway closures with the property owner(s) and resident(s) in the areas of construction.
2. The location and depth of all existing utilities and service leads are to be field verified by the Contractor prior to construction.
3. During non-working hours no more than ten (10) feet of trench shall remain open; any open trench shall be properly secured with protective fencing. This work shall be included in the items of work being undertaken and will not be paid for separately.
4. The location of material stock piles and on-site staging areas shall be approved by the Engineer.
5. For mainline HMA Paving, the width of the mat for each pass of the paver shall be not less than 10.5', nor greater than 16', except as noted in the plans and as directed by the Engineer. The Engineer will direct the layout of all HMA Longitudinal Joints during construction.
6. All excavation required for roadway grading within the project limits, including proposed curbs, pavement and infiltration trench, shall be included in "Machine Grading, Modified, \_\_\_\_\_".
7. All excavation required for project grading within the project limits, including proposed sidewalks and sidewalk ramps, shall be included in "Sidewalk Grading" and "Sidewalk Ramp Grading".
8. Excavation and backfill behind curb and gutter shall be included in "Machine Grading, Modified, \_\_\_\_\_". All backfill under proposed concrete pavements such as drive approaches, ramps, sidewalk, etc., shall be MDOT Class II Granular Material, compacted to 95% of its max. dry density and will be paid for as "Subbase, CIP, Class II, Modified." Backfill for other areas must be approved by the Engineer and compacted to 95% of its max. dry density. No payment will be made for sub-base or aggregate base that extends beyond 12" behind the back of curb. Reference the Typical Cross Sections.
9. Excavation and backfill for sidewalks and sidewalk ramps shall be included in "Sidewalk Grading" and "Sidewalk Ramp Grading". All backfill under proposed concrete pavements such as drive approaches, ramps, sidewalk, etc., shall be MDOT Class II Granular Material, compacted to 95% of its max. dry density and will be paid for as "Subbase, CIP, Class II, Modified." Backfill for other areas must be approved by the Engineer and compacted to 95% of its max. dry density. No payment will be made for sub-base or aggregate base that extends beyond 6" behind the back of walk. Reference the Typical Cross Sections.
7. Where existing sewer and/or drainage structures are to be removed, they shall be properly disposed of off-site and the excavation shall be backfilled with MDOT Class II Granular Material compacted to 95% of its max. dry density. This work shall be included in the appropriate contract items and will be paid for at the corresponding contract unit price.
8. All Structures shall receive new castings, as specified on the Standard Casting Schedule. The existing castings shall be neatly stacked on-site in a single location so that City of Ann Arbor forces can retrieve them at a later date. The Contractor shall assist City forces by loading them into City trucks. All costs associated with storing, stockpiling, and loading castings into City vehicles shall be included in the item of work "Mobilization, Max. \_\_\_\_\_" and will not be paid for separately.
9. All fittings, hydrants, valves and castings removed during construction shall become the property of the City of Ann Arbor. The Contractor shall coordinate pick up by the City of Ann Arbor Public Works.
10. Payment for drainage structure sumps, where specified, shall be included in the payment for the various drainage structures sizes and/or types.
11. Where pipes of different sizes or materials are joined, Engineer approved flexible couplings with stainless steel shear rings shall be used. The Contractor's purchase price for these devices, including shipping, shall be paid as an extra. Prior to payment for this item, the Contractor shall submit receipts for the Engineer's review and approval. All other costs associated with the installation of these devices shall be included in the payment for the sewer.
12. If the Contractor encounters existing edge drain(s) during construction of the proposed edge drains, inlet leads, or catch basins, it shall be capped at each end to prevent material from entering the pipe. The cost of this work will not be paid for separately, but shall be included in the particular item of work being performed.
13. In areas where edge drain cannot be installed in accordance with the details, the edge drain shall be installed at the depth as indicated on the plans, or as directed by the Engineer. In no case shall the edge drain be installed at a grade less than 0.50% or at a depth less than 3.25' below the top of pavement.
14. Existing street name signs, guide, bus stop, and regulatory signs which conflict with the proposed construction shall be removed prior to construction, stored in a manner which will prevent damage, and re-set in locations as directed by the Engineer. This work will not be paid for separately, but shall be included in the pay item "Minor Traf Devices".
15. All curb, sidewalk, driveway approach removals shall be approved by the Engineer before the work is performed.
16. Place 4" (minimum) thickness Class II Granular Material compacted to 95% of its max. dry density under concrete sidewalk as shown on the details. This work shall be included in the contract items "Subbase, CIP, Class II, Modified."
17. Place 8" (minimum) MDOT Dense Graded Aggregate 21AA, compacted to 95% of its max. dry density under drive approaches. This work shall be included in the contract item "Aggregate Base."
18. Prior to placing the adjacent paving pass on the leveling and wearing courses of HMA, the Contractor shall cut and remove 6" to 8" of the previously placed pavement by means of a couler wheel. The Engineer reserves the right to reject any method(s) for cutting the pavement that does not provide a satisfactory edge as determined by the Engineer. Any method(s) employed by the Contractor shall be completely effective. The cut edge shall have a uniform bead of Craftco Joint Adhesive applied. The removal of this HMA material, cleaning the HMA surface and pavement edge, and condition of the resulting edge must be approved by the Engineer prior to proceeding with the placement of the succeeding pass of HMA. The base course of HMA will only have its edges tacked in accordance with standard paving practices. All costs associated with complying with these requirements will be included in the pay item "Edge Trimming".

EARHART ROAD BENCHMARKS		
BM #	ELEV	DESCRIPTION
AA1033	814.118	CITY OF ANN ARBOR VERTICAL CONTROL STATION. BRASS DISC IN CENTER OF ROUNDABOUT
1	814.540	EAST SIDE OF CONC. LIGHT BASE 55'+/- NORTH OF CL OF ENTRANCE TO CONCORDIA UNIV. MAINTENANCE BARN
AA2032	843.380	CITY OF ANN ARBOR BENCH MARK
2	883.330	SOUTH SIDE OF CONC. CROSS BASE JUST SOUTH OF ST PAUL LUTHERAN SCHOOL SIGN, NW CORNER ENTRANCE OF SCHOOL DRIVEWAY.
3	872.450	SOUTH FLANGE BOLT ON HYDRANT SE CORNER OF EARHART & PINE BRAE. 20'+/- FROM CL OF PINE BRAE, 36'+/- FROM CL OF EARHART. NEAR HOUSE #491
4	855.970	NE BOLT ON CONC. BASE OF LIGHT POLE ON THE NW CORNER OF EARHART & WALDENWOOD, 75'+/- FROM CL OF EARHART.
1	872.119	CHISEL "X" ON NE BOLT OF LAMP POST ON SOUTHSIDE OF SOUTH ENTRANCE TO GREENHILLS DRIVE AND 76± EAST OF CENTERLINE OF EARHART ROAD.
2	888.672	TOP OF SE BOLT ON FLANGE OF HYDRANT, EAST SIDE OF EARHART ROAD, 4' BACK OF CURB AND 182± NORTH OF ENTRANCE TO EARHART VILLAGE UNITS 810-832 OFF EARHART ROAD.
3	894.025	TOP OF SW BOLT ON FLANGE OF HYDRANT, NW CORNER OF EARHART ROAD AND WALDENWOOD DRIVE.
4	893.891	BOAT SPIKE IN NE FACE OF UTILITY POLE AT SOUTHWEST CORNER OF GLAZIER WAY AND EARHART ROAD.
5	892.352	RAILROAD SPIKE IN WEST FACE OF UTILITY POLE, EAST SIDE OF EARHART ROAD, 10' FROM BACK OF CURB AND 234± NORTH OF CENTERLINE OF GLACIER HILLS DRIVE SOUTH
6	895.813	RAILROAD SPIKE WEST FACE OF UTILITY POLE AT THE SE CORNER OF EARHART ROAD AND MAIN ENTRANCE OF GLACIER HILLS #1200
7	888.399	RAILROAD SPIKE WEST FACE OF UTILITY POLE ON EAST SIDE OF EARHART ROAD 8' FROM BACK OF CURB AND 148± SOUTH OF CENTERLINE OF KIPLING DR.
8	893.903	RAILROAD SPIKE WEST FACE OF UTILITY POLE ON THE EAST SIDE OF EARHART ROAD, 35± FROM BACK OF CURB AND 95± NORTH FROM CENTERLINE OF GLACIER HILLS DR. NORTH ENTRANCE
9	892.060	CITY BENCHMARK #2009 DISC IN ROUND MONUMENT, 12.8± S OF BACK OF CURB AND 13.1± E OF BACK OF CURB IN THE SOUTH ISLAND AT THE INTERSECTION OF GLAZIER WAY & EARHART ROAD.

PERMITS REQUIRED TO BE OBTAINED BY THE CONTRACTOR PRIOR TO THE BEGINNING OF CONSTRUCTION.	
PERMIT	ISSUING AUTHORITY
LANE CLOSURE PERMIT*	CITY OF ANN ARBOR ENGINEERING
"NO PARKING" SIGNS PERMIT*	CITY OF ANN ARBOR ENGINEERING
GRADING/SOIL EROSION & SEDIMENTATION CONTROL PERMIT*	CITY OF ANN ARBOR CUSTOMER SERVICE
RIGHT-OF-WAY PERMIT*	CITY OF ANN ARBOR CUSTOMER SERVICE
* NO COST TO CONTRACTOR	

PERMITS REQUIRED TO BE OBTAINED BY THE CITY OF ANN ARBOR PRIOR TO THE BEGINNING OF CONSTRUCTION.	
PERMIT	ISSUING AUTHORITY
EGLWATER MAIN CONSTRUCTION PERMIT	MICHIGAN DEPARTMENT OF ENVIRONMENT, GREAT LAKES, AND ENERGY

CONTACT INFORMATION		
PUBLIC UTILITIES	OWNER	CONTACT
WATER	CITY OF ANN ARBOR PUBLIC WORKS W.R. WHEELER SERVICE CENTER 4251 STONE SCHOOL ROAD ANN ARBOR, MI 48108	(734) 794-6350
SANITARY		
STORM		
FORESTRY		
SIGNS SIGNALS STREET LIGHTS	MARK MORENO (734) 794-6361	
FIBER OPTIC	CITY OF ANN ARBOR INFORMATION TECHNOLOGY LARCOM CITY HALL 301 E. HURON STREET ANN ARBOR, MI 48107	(734) 794-6550
PRIVATE UTILITIES	OWNER	CONTACT
GAS	DTE ENERGY 3150 E. MICHIGAN AVE, YPSILANTI TOWNSHIP, MI 48198	ROBERT CZAPIEWSKI (734) 544-7818
ELECTRIC	DTE ENERGY WESTERN WAYNE SERVICE CENTER 8001 HAGGERTY ROAD BELLEVILLE, MI 48111	ANTHONY IGNASIAK (734) 397-4447
CABLE	COMCAST 27800 FRANKLIN ROAD SOUTHFIELD, MI 48034	RON SOUTHERLAND (313) 999-8300
PHONE	AT&T 550 S. MAPLE ROAD ANN ARBOR, MI 48103	STEVEN ALLSHOUSE (734) 996-5381
FIBER OPTIC	MCI 2800 N. GLENFILLE ROAD RICHARDSON, TX 75082	DEAN BOYERS (972) 729-6016
FIBER OPTIC	WINDSTREAM 1295 S LINDEN ROAD, SUITE B FLINT, MI 48532	GREG SERICH (810) 244-3500
STREET LIGHTING	DTE ENERGY 8001 HAGGERTY ROAD BELLEVILLE, MI 48111	LANCE ALLEY (734) 397-4188

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			4-27-23	DATE			

DESCRIPTION

			DO	REV.			

DESCRIPTION

CITY OF ANN ARBOR  
PUBLIC SERVICES  
301 EAST HURON STREET  
PO BOX 8647  
ANN ARBOR MI 48107-8647  
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CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING  
EARHART ROAD IMPROVEMENTS (GEDDES - US23)

STANDARD NOTES

SCALE: NTS

DRAWING No.

2021023-2

SHEET No.

2 OF 74

**EXISTING LEGEND**

	FIRE HYDRANT
	GATE VALVE IN BOX
	GATE VALVE IN WELL
	STOP BOX
	WATER VAULT
	WELL
	CATCH BASIN (SQ)
	CATCH BASIN (RD)
	STORM MANHOLE
	NON-CURB CATCH BASIN (SQ)
	END SECTION
	SANITARY MANHOLE
	CLEAN-OUT
	POST
	PEDESTRIAN SIGNAL
	SIGN
	HAND HOLE
	ORNAMENTAL LIGHT
	FLOOD LIGHT
	UNKNOWN MANHOLE
	TELEPHONE MANHOLE
	TELEPHONE RISER
	GAS VALVE
	GAS VENT
	GAS BOX
	ELECTRICAL RISER
	TRANSFORMER
	UTILITY POLE
	LAMP POLE
	GUY ANCHOR
	GUY POLE
	MONITORING WELL
	MAILBOX
	SOIL BORING
	TRAVERSE POINT
	BENCH MARK
	IRON PIPE
	MON BOX

	WATER MAIN
	WATER MAIN ABANDONED
	STORM SEWER
	STORM SEWER ABANDONED
	SANITARY SEWER
	SANITARY SEWER ABANDONED
	GAS MAIN
	GAS MAIN (DEAD)
	ELECTRICAL OVER HEAD
	ELECTRICAL UNDER GROUND
	ELECTRICAL DUCT BANK
	TELEPHONE OVER HEAD
	TELEPHONE UNDER GROUND
	TELEPHONE DUCT BANK
	CABLE TV OVER HEAD
	CABLE TV UNDER GROUND
	FIBER OPTIC
	FIBER OPTIC DUCT BANK
	BOUNDARY
	BUILDING
	CENTERLINE OF DITCH
	CENTERLINE/CROWN OF ROAD
	CONTOUR MAJOR
	CONTOUR MINOR
	EDGE OF WATER
	FLOODPLAIN
	FENCE
	GRAVEL
	GUARDRAIL
	STONE WALL
	R.O.W.
	TREELINE
	WETLAND
	EDGE OF BRUSH
	HEDGE
	TREE (DECIDUOUS)
	TREE (CONIFEROUS)
	SHRUB (DECIDUOUS)
	STUMP
	TREE TO REMAIN & PROTECT (DECIDUOUS) CRITICAL ROOT ZONE (C.R.Z.) = DIAMETER BREST HEIGHT (INCHES) X 10
	TREE TO REMAIN & PROTECT (CONIFEROUS) CRITICAL ROOT ZONE (C.R.Z.) = DIAMETER BREST HEIGHT (INCHES) X 10

**PROPOSED LEGEND**

	HYDRANT (PLAN)
	WATER GATE WELL
	REDUCER
	WATER GATE VALVE
	WATER STOP BOX
	WATER VAULT
	INLET
	DOUBLE INLET
	INLET JUNCTION CHAMBER
	ROUND CATCH BASIN
	STORM MANHOLE
	DRAIN ARROW
	FLARED END SECTION
	SANITARY MANHOLE
	CLEAN-OUT
	BARREL
	SIGN
	PUSH BUTTON
	HAND HOLE
	WATER MAIN
	STORM SEWER
	SANITARY SEWER
	FIBER OPTIC
	ELECTRICAL
	CENTERLINE OF DITCH
	CENTERLINE OF ROAD
	FENCE
	GRAVEL
	SILT FENCE
	PROTECTIVE FENCE
	GUARDRAIL
	LOT/UNIT
	CURB
	TEMPORARY GRADING PERMIT
	CONTOUR MAJOR
	CONTOUR MINOR
	WATER EASEMENT
	STORM EASEMENT
	SANITARY EASEMENT
	R.O.W.
	LIMITS OF CONSTRUCTION
	LIMIT OF GRADING
	STONE WALL
	DETECTABLE WARNING
	ASPHALT
	CONCRETE
	SIDEWALK
	TREE (DECIDUOUS)
	TREE (CONIFEROUS)
	TREE TO BE REMOVED (DECIDUOUS)
	TREE TO BE REMOVED (CONIFEROUS)
	STUMP TO BE REMOVED

**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**

**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**

SCALE: NTS

DRAWING No. **2021023-3**

SHEET No. **3 OF 74**

**LEGEND**

OUT TO BID

DATE 4-27-23

DESCRIPTION

REV.

CC/DF

DRAWN

CHECKED

NO

CITY OF ANN ARBOR  
PUBLIC SERVICES  
301 EAST HURON STREET  
ANN ARBOR, MI 48106-1647  
ANN ARBOR 734-794-4410  
www.a2gov.org

811  
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**GENERAL**  
NOTIFY THE CITY OF ANN ARBOR SOIL EROSION CONTROL OFFICE 48 HOURS PRIOR TO BEGINNING WORK ON THE PROJECT. PHONE: 734-794-6265.

1. THE CONTRACTOR SHALL IMPLEMENT AND MAINTAIN THE SOIL EROSION CONTROL MEASURES AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER AT ALL TIMES DURING CONSTRUCTION. ANY MODIFICATIONS OR ADDITIONS TO THE SOIL EROSION CONTROL MEASURES DUE TO CONSTRUCTION OR CHANGED CONDITIONS SHALL BE AS DIRECTED AND APPROVED BY THE ENGINEER.
2. ALL SOIL EROSION AND SEDIMENTATION CONTROL WORK SHALL CONFORM TO THE PERMIT REQUIREMENTS OF THE CITY OF ANN ARBOR, THE LAWS OF THE STATE OF MICHIGAN, AND THE REQUIREMENTS OF THE CONTRACT DOCUMENTS.
3. DAILY, OR AFTER ANY STORM EVENT, INSPECTIONS OF EROSION CONTROL MEASURES SHALL BE MADE BY THE CONTRACTOR. PERIODIC INSPECTIONS MAY BE MADE BY THE ENGINEER TO DETERMINE THE EFFECTIVENESS OF EROSION AND SEDIMENTATION CONTROL MEASURES. ANY NECESSARY CORRECTIONS SHALL BE MADE WITHOUT DELAY, AND WITHOUT ADDITIONAL COST TO THE CITY OF ANN ARBOR.
4. EROSION AND SEDIMENTATION FROM WORK ON THE SITE SHALL BE CONTAINED ON THE SITE AND NOT BE ALLOWED TO COLLECT ON ANY OFF-SITE AREAS, ROADWAYS OR WATERWAYS.
5. ALL MUD/SOIL TRACKED ONTO ROADWAYS FROM THE SITE DUE TO CONSTRUCTION, SHALL BE PROMPTLY REMOVED BY THE CONTRACTOR. IF SO ORDERED, THE CONTRACTOR SHALL PROVIDE AND OPERATE A VACUUM-TYPE STREET SWEEPER, AT NO ADDITIONAL COST TO THE CITY OF ANN ARBOR.
6. RESTORATION OF ALL DISTURBED AREAS, INCLUDING PLACEMENT OF TOPSOIL, SEED, FERTILIZER AND MULCH AND/OR SOD SHALL BE PERFORMED WITHIN FIVE (5) DAYS OF THE COMPLETION OF FINAL GRADE.
7. CONSTRUCTION OPERATIONS SHALL BE SCHEDULED AND PERFORMED SO THAT PREVENTATIVE SOIL EROSION CONTROL MEASURES ARE IN PLACE PRIOR TO EXCAVATION IN CRITICAL AREAS AND TEMPORARY STABILIZATION MEASURES ARE IN PLACE IMMEDIATELY FOLLOWING BACKFILLING OPERATIONS.
8. SPECIAL PRECAUTIONS WILL BE TAKEN IN THE USE OF CONSTRUCTION EQUIPMENT TO PREVENT SITUATIONS THAT PROMOTE EROSION.
9. PROPER DUST CONTROL SHALL BE MAINTAINED DURING CONSTRUCTION BY USE OF

WATER TRUCKS AND/OR OTHER METHODS APPROVED BY THE ENGINEER.

10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL TEMPORARY SOIL EROSION CONTROL MEASURES AND REMOVAL OF SOME MEASURES UPON AUTHORIZED COMPLETION OF THE PROJECT. FINAL COMPLETION OF PROJECT WILL NOT BE AUTHORIZED UNTIL ALL SITE WORK AND UTILITY CONSTRUCTION IS COMPLETE AND ALL SOILS ARE STABILIZED.
11. THE CONTRACTOR SHALL NOT GRADE INTO ADJACENT PROPERTIES. SILT AND PROTECTIVE FENCE SHALL BE INSTALLED AND MAINTAINED TO PREVENT GRADING, EROSION AND SEDIMENTATION INTO THE ADJACENT PROPERTIES.
12. TREE PROTECTION FENCING MUST REMAIN INTACT UNTIL RESTORATION OF THE SITE IS COMPLETE.

**SEQUENCE OF EROSION CONTROL MEASURES:**

1. THE CONTRACTOR IS TO SUBMIT TO THE ENGINEER, A SEQUENCE OF CONSTRUCTION WITH RESPECT TO THE SOIL EROSION CONTROL MEASURES FOR REVIEW, COMMENT AND APPROVAL. THIS SCHEDULE IS TO INCLUDE INSPECTION AND REPAIR OF ALL TEMPORARY EROSION CONTROL MEASURES DAILY AND WITHIN 24 HOURS OF A STORM EVENT.

**SAMPLE SOIL EROSION AND SEDIMENTATION CONTROL INSTALLATION MINIMUM REQUIREMENTS:**

- 1.1. INSTALL SILT FENCE, TREE PROTECTION FENCING, MUD MATS, INLET FILTERS ON EXISTING DRAINAGE FEATURES, AND ALL OTHER TEMPORARY SOIL EROSION CONTROLS, PRIOR TO ANY CLEARING OR EARTH MOVING OPERATION.
- 1.2. STRIP AND STOCKPILE TOPSOIL. STABILIZE STOCKPILE AS REQUIRED.
- 1.3. INSTALL WATER MAINS, STORM AND SANITARY SEWERS, AND OTHER ENCLOSED DRAINAGE FEATURES. NEW INLET FILTERS SHALL BE INSTALLED IMMEDIATELY FOLLOWING INSTALLATION OF NEW DRAINAGE INLETS.
- 1.4. PERFORM MACHINE GRADING OPERATIONS AND CONSTRUCT PAVEMENTS (MAINLINE, SIDEWALKS, DRIVES, ETC.).
- 1.5. CONTINUALLY MAINTAIN EROSION AND SEDIMENTATION CONTROL MEASURES, AS REQUIRED TO ALLOW DRAINAGE AND SEDIMENT REMOVAL. REMOVE ANY ACCUMULATED SEDIMENT IMMEDIATELY.
- 1.6. COMPLETE ALL FINE GRADING.

- 1.7. TEMPORARY SEED AND INSTALL EROSION CONTROL BLANKET IN ALL DISTURBED AREAS.
- 1.8. REFER TO LANDSCAPE PLANTING PLANS FOR PERMANENT SITE STABILIZATION.
- 1.9. CLEAN OUT STORM SEWER SYSTEMS.
- 1.10. REMEDY ANY NOTED DEFECTS TO THE SATISFACTION OF THE CITY OF ANN ARBOR'S SOIL EROSION AND SEDIMENTATION CONTROL OFFICIAL.
- 1.11. ALL TEMP. SOIL EROSION CONTROL MEASURES MUST BE REMOVED, WITH ENGINEERS APPROVAL, PRIOR TO FINAL INSPECTION

NOTE: THIS SEQUENCE IS FOR INFORMATION ONLY. IT IS INTENDED TO SHOW THE SEQUENCE OF CONSTRUCTION WITH RESPECT TO THE SOIL EROSION AND SEDIMENTATION CONTROL MEASURES. THE CONTRACTOR IS RESPONSIBLE FOR SUBMITTING THEIR OWN DETAILED CONSTRUCTION SEQUENCE AND SCHEDULE TO THE ENGINEER FOR REVIEW, COMMENT, AND APPROVAL.

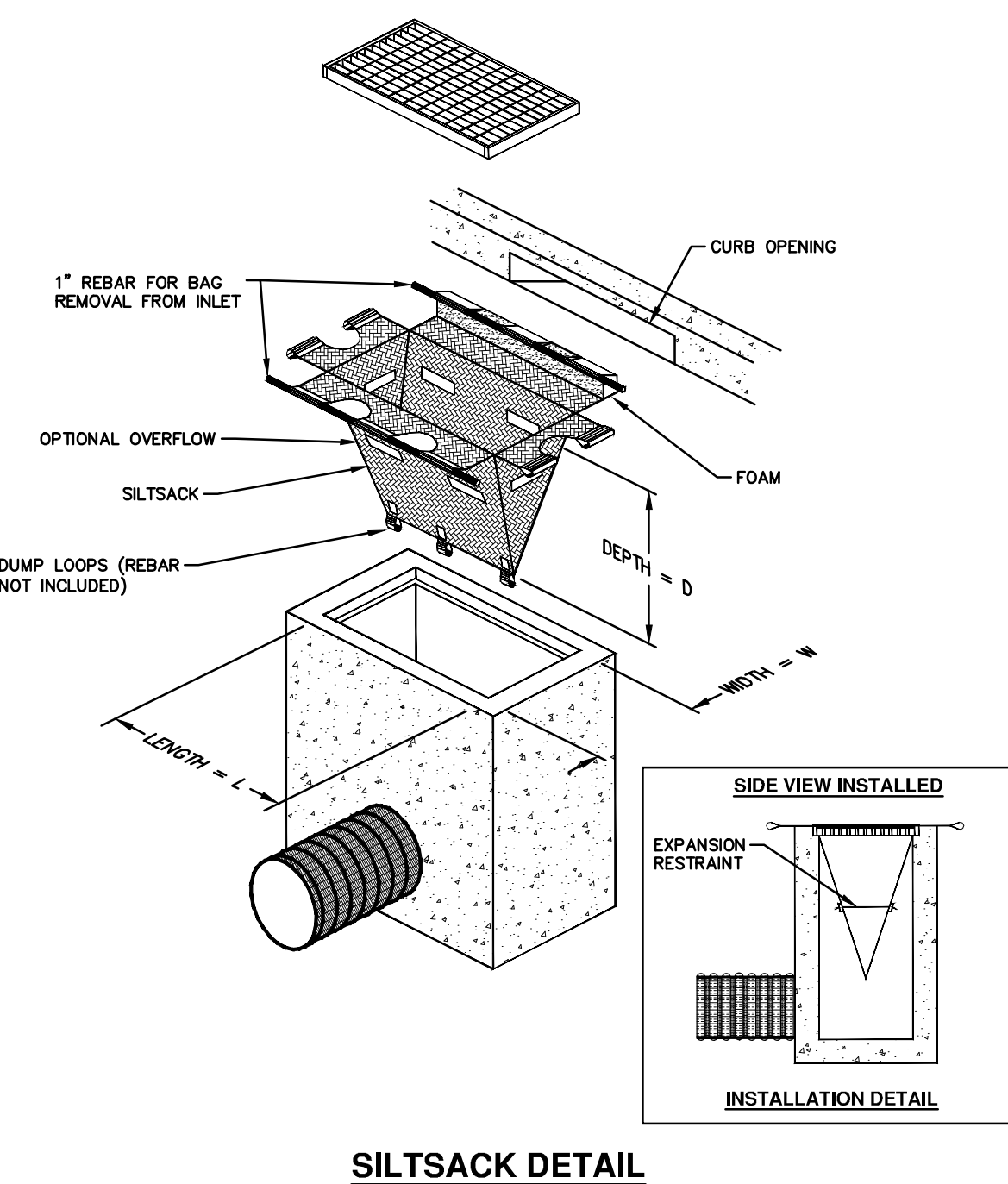
**TEMPORARY SEEDING:**

1. SEED IN ACCORDANCE WITH PROJECT DRAWINGS AND SPECIFICATIONS.
2. ANY DISTURBED AREA NOT PAVED, SEEDED, MULCHED, SODDED OR BUILT UPON BY NOVEMBER 15TH OR JUNE 30TH IS TO BE TEMPORARILY STABILIZED PER SPECIFICATIONS.

THE ESTIMATED COST OF SOIL EROSION AND SEDIMENTATION CONTROL MEASURES, TOPSOIL, SEEDING, AND MULCH = \$17,500.

IMPERVIOUS PROJECT AREA  
2.19 AC - EARHART ROAD

TOTAL AREA OF PROPOSED DISTURBANCE  
2.45 AC - EARHART ROAD



NOTE: THE SILTSACK WILL BE MANUFACTURED FROM A WOVEN POLYPROPYLENE FABRIC THAT MEETS OR EXCEEDS THE FOLLOWING SPECIFICATIONS.

**REGULAR FLOW SILTSACK**

(FOR AREAS OF LOW TO MODERATE PRECIPITATION AND RUN-OFF)

PROPERTIES	REQUIRED VALUE	TEST METHOD
GRAB TENSILE STRENGTH	ASTM D-4632	300 LBS
GRAB TENSILE ELONGATION	ASTM D-4632	20%
PUNCTURE	ASTM D-4833	120 LBS
MULLEN BURST	ASTM D-3786	800 PSF
TRAPEZOID TEAR	ASTM D-4533	120 LBS
UV RESISTANCE	ASTM D-4355	80%
APPARENT OPENING SIZE	ASTM D-4751	40 US SIEVE
FLOW RATE	ASTM D-4491	40 GAL/MIN/SQ FT
PERMITTIVITY	ASTM D-4491	0.55 SEC-1

**HI-FLOW SILTSACK**

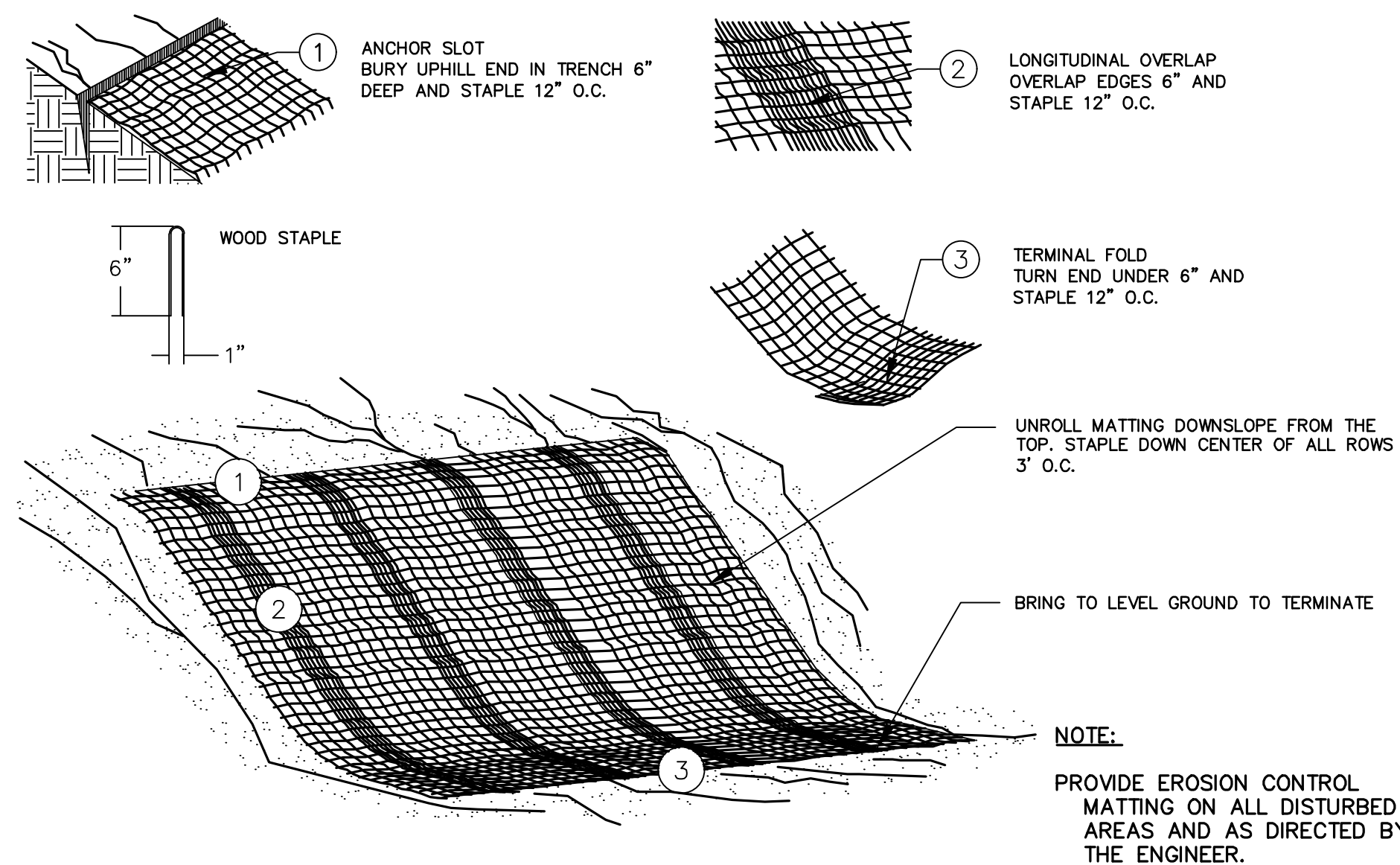
(FOR AREAS OF MODERATE TO HEAVY PRECIPITATION AND RUN-OFF)

PROPERTIES	REQUIRED VALUE	TEST METHOD
GRAB TENSILE STRENGTH	ASTM D-4632	285 LBS
GRAB TENSILE ELONGATION	ASTM D-4632	20%
PUNCTURE	ASTM D-4833	135 LBS
MULLEN BURST	ASTM D-3786	420 PSF
TRAPEZOID TEAR	ASTM D-4533	45 LBS
UV RESISTANCE	ASTM D-4355	90%
APPARENT OPENING SIZE	ASTM D-4751	20 US SIEVE
FLOW RATE	ASTM D-4491	200 GAL/MIN/SQ FT
PERMITTIVITY	ASTM D-4491	1.5 SEC-1

**OIL-ABSORBANT SILTSACK**

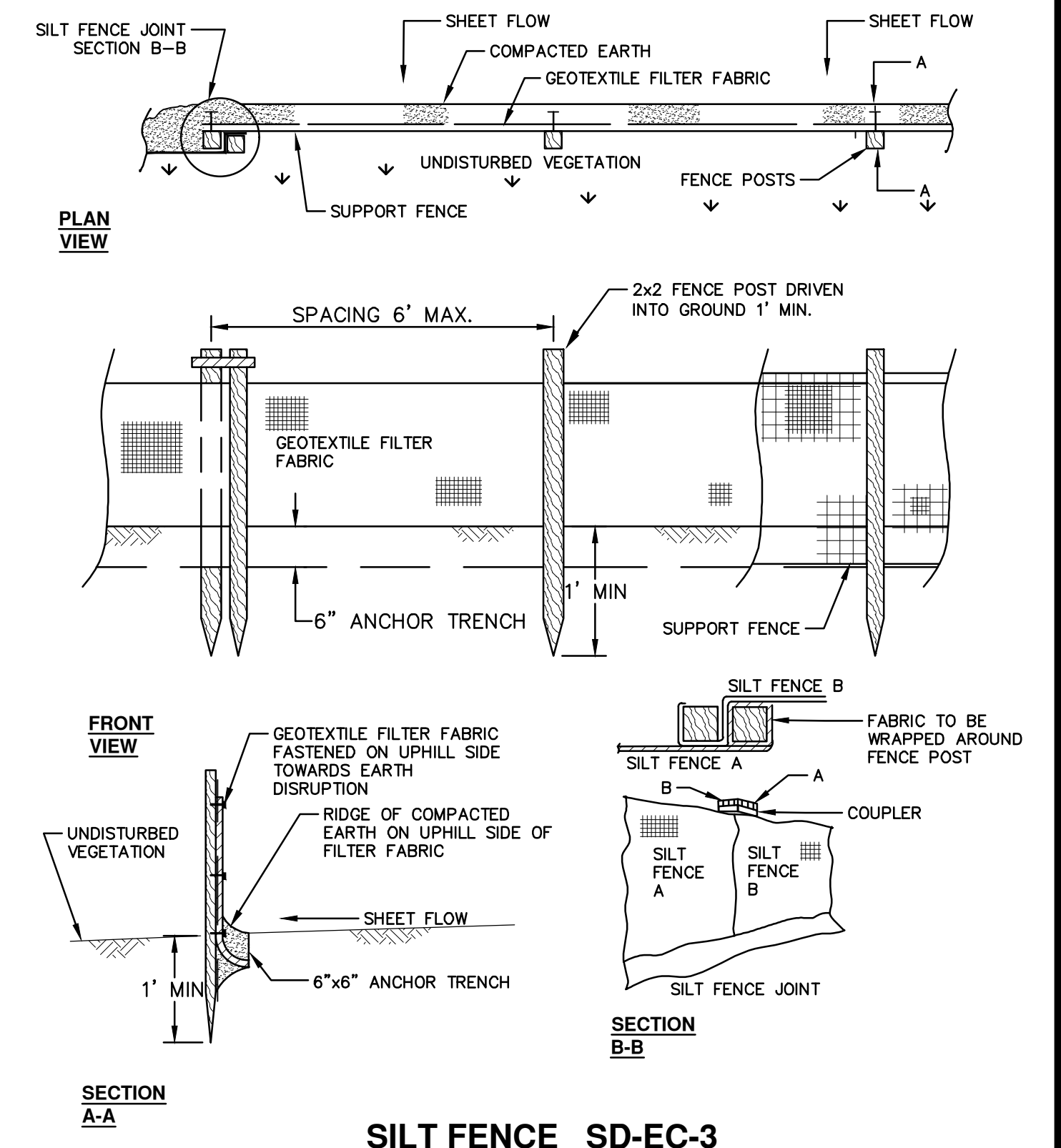
(FOR AREAS WHERE THERE IS A CONCERN FOR OIL RUN-OFF OR SPILLS)

IT IS THE INTENT OF THE PLANS AND SPECIFICATIONS THAT THE CONTRACTOR INSTALL THE REBAR AS SHOWN IN THIS DETAIL TO PROVIDE A FULLY FUNCTIONING UNIT. ALL COSTS ASSOCIATED WITH FURNISHING, CLEANING AS MANY TIMES AS REQUIRED, DISPOSAL OF SEDIMENT, AND REMOVING THE INLET FILTER WHEN NO LONGER NEEDED IS INCLUDED IN THE ITEM OF WORK AND WILL NOT BE PAID FOR SEPARATELY.



**MULCH BLANKET DETAIL**

APPLIES TO ALL AREAS TO BE PERMANENTLY RESTORED WITH GRASS. SEE LANDSCAPE PLANS FOR MORE DETAILS.

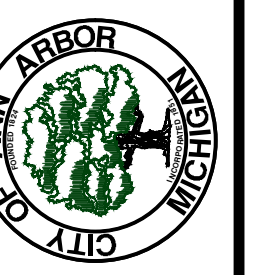


**SILT FENCE SD-EC-3**



NO.	DATE	DESCRIPTION	REV.
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		DRAWN	CHECKED

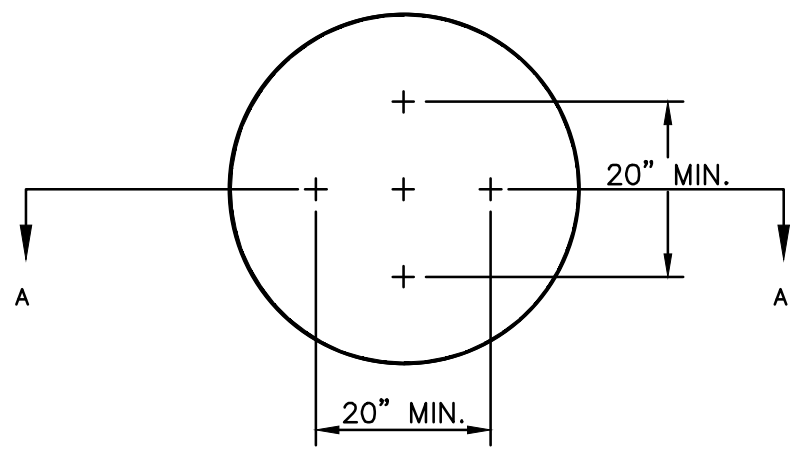
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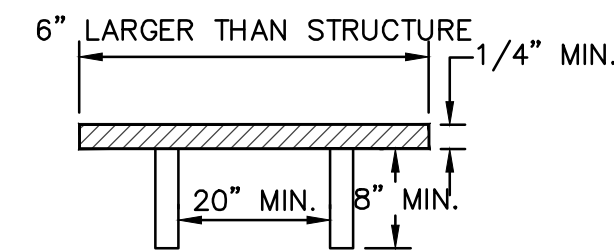
SESC NOTES AND DETAILS

SCALE: NTS  
DRAWING No. 2021023-4  
SHEET No.



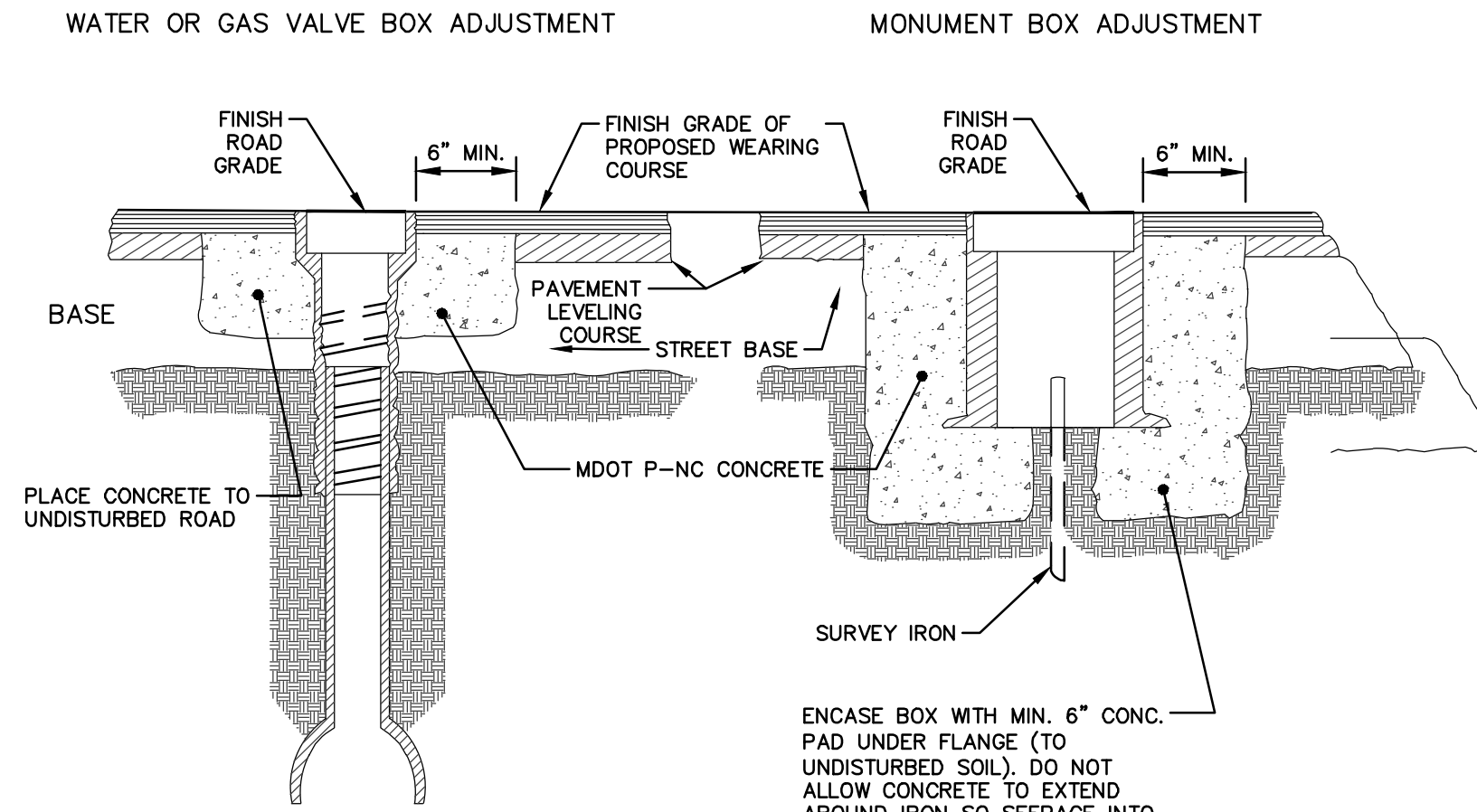
NOTE: PLATE MAY BE CIRCULAR, SQUARE OR RECTANGULAR

SECTION A - A



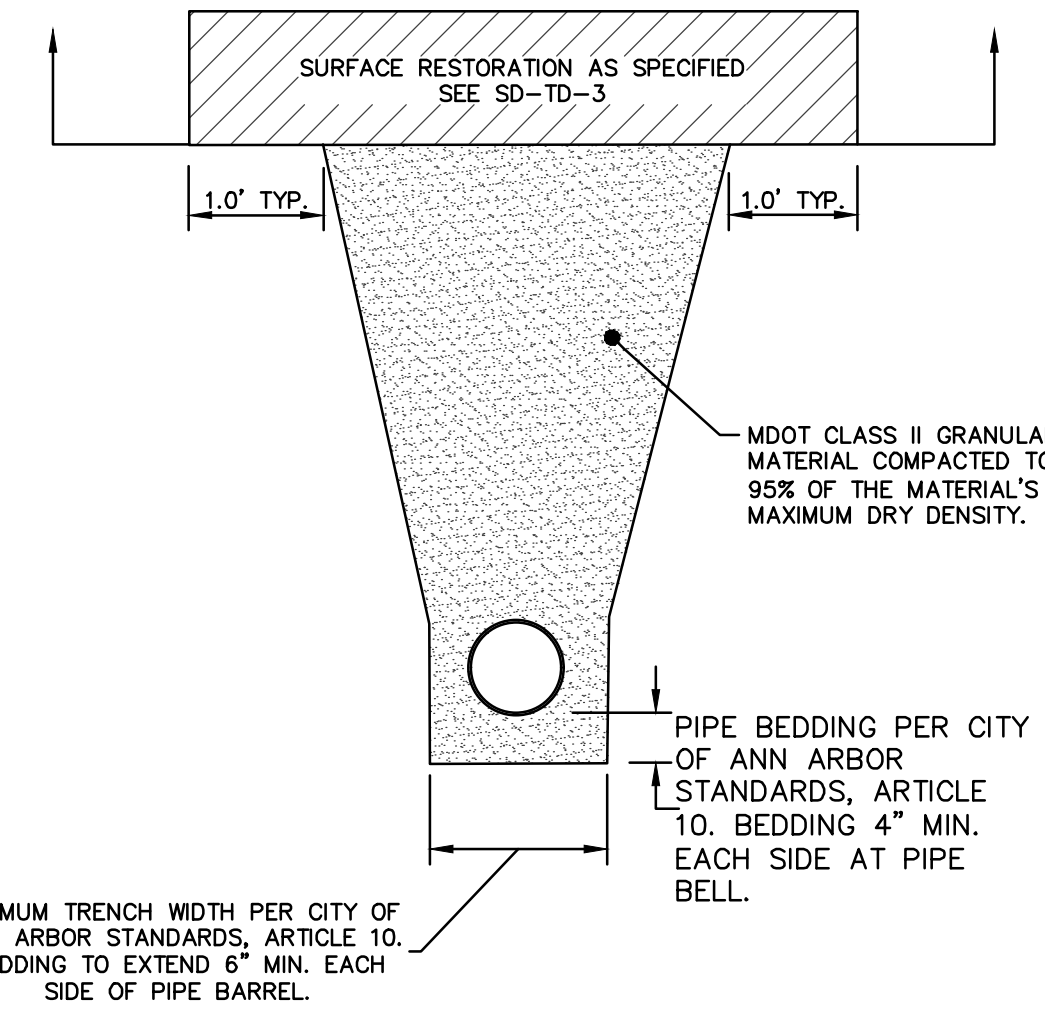
1/4" MIN. THICKNESS AND SUFFICIENT TO CARRY THE CONSTRUCTION LOAD.

**STRUCTURE PLATE SD-GU-8**



- NOTES:
1. GAS VALVE BOXES TO BE ADJUSTED BY THE GAS COMPANY.
  2. PLACE CENTER OF [MONUMENT] BOX OVER SURVEY IRON.
  3. RAISE CASTING TO PROPOSED FINISH STREET GRADE AFTER PLACEMENT OF LEVELING COURSE(S) AND PRIOR TO PLACING FINAL SURFACE COURSE.

**VALVE AND MONUMENT BOX ADJUSTMENT SD-GU-6**



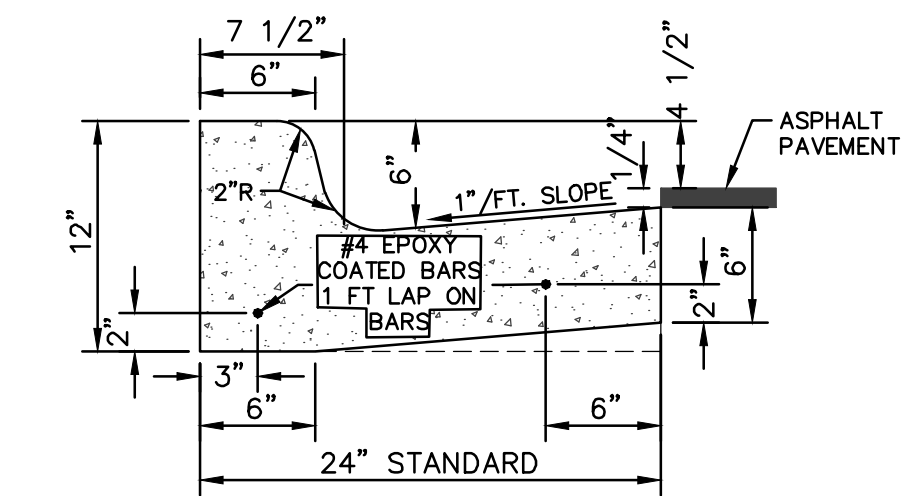
- NOTES:
1. ALL TRENCH EXCAVATION, BEDDING, BACKFILLING, AND SURFACE RESTORATION SHALL COMPLY WITH CITY OF ANN ARBOR STANDARDS, ARTICLE 10.
  2. TRENCH DETAILS SHOW TYPE OF BACKFILL AND TRENCHING REQUIREMENTS ONLY.
  3. ALL TRENCHING TO CONFORM TO ALL APPLICABLE M.I.O.S.H.A. AND CITY STANDARDS.
  4. PIPE BEDDING THICKNESS UNDER CONCRETE PIPE 66" OR LARGER SHALL BE INCREASED TO 6".
  5. SEE SD-TD-1B FOR SANITARY BEDDING AREA DETAIL. SEE SD-TD-4 FOR EDGE DRAIN BEDDING AND BACKFILL.
  6. SURFACE RESTORATION SHALL NOT BE INCLUDED IN THE UNIT PRICE FOR PIPE AND WILL BE PAID FOR SEPARATELY.

**UTILITY TRENCH - TYPE IA EXCLUDING SANITARY SEWER AND EDGE DRAIN SD-TD-1A**

TYPE OF CASTING	MDOT DESIGNATION	EJ CASTING NO.	NEENAH CASTING NO.
MANHOLE AND GATE WELL FRAME AND COVER	Q/B	1040Z W/ TYPE A COVER*	R-1642 W/ TYPE C COVER*
BARRIER CURB INLET FRAME AND COVER	K	7045Z W/ TYPE M1 GRATE	R-3031-B W/ TYPE S GRATE
BARRIER CURB LOW POINT INLET FRAME AND COVER	K	7035Z W/ TYPE M2 GRATE	N/A
GUTTER INLET FRAME AND COVER	R	5080Z W/ TYPE 5000M2 GRATE	R-3448C, W/ TYPE S GRATE
GUTTER LOW POINT INLET FRAME AND COVER	R	7034Z W/ TYPE M GRATE	N/A
MOUNTABLE CURB INLET ASSEMBLY		7065	N/A
YARD DRAIN (BEE HIVE) FRAME AND COVER	G	1040Z, TYPE O2 GRATE	R-2560-E1
WATER VALVE BOX ASSEMBLY IN PAVEMENT		8560	N/A
MONUMENT BOX ASSEMBLY		8360	N/A

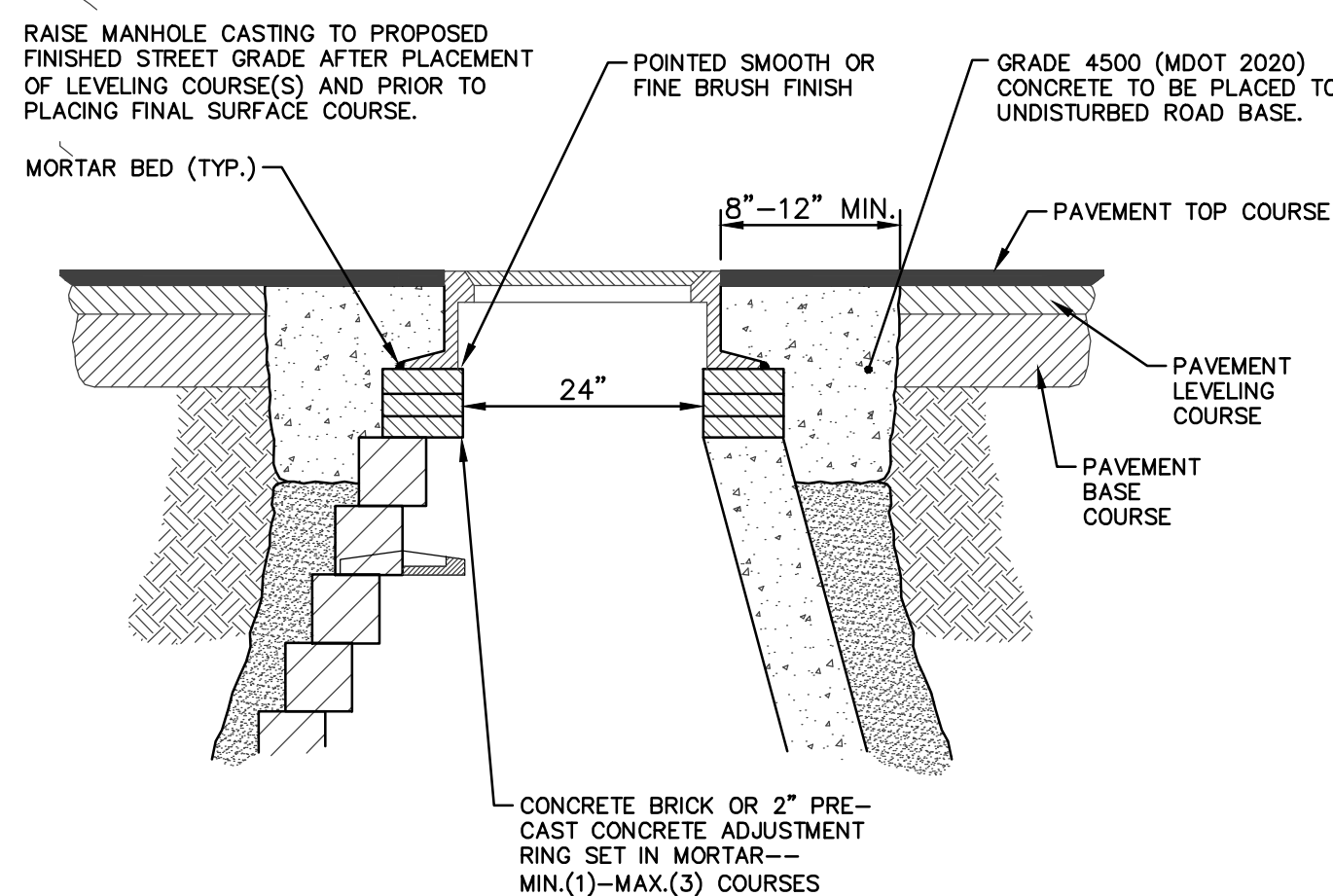
NOTES:  
 \*EACH COVER SHALL HAVE "SANITARY", "STORM", OR "WATER" CAST IN THE SURFACE, WHICHEVER IS APPLICABLE. SANITARY SEWER COVERS SHALL BE GASKETED IN FLOOD PRONE AREAS AS SHOWN ON THE PLANS OR AS DIRECTED BY THE PSAA. FRAMES AND COVERS MUST HAVE MACHINED BEARING SURFACES.

**STANDARD CASTING SCHEDULE SD-GU-1**



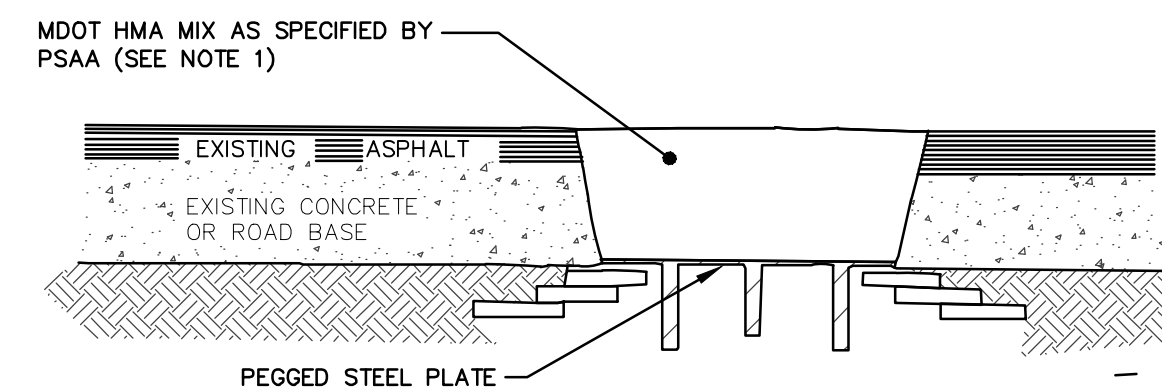
- NOTES:
1. BARRIER CURB AND GUTTER ON ASPHALT STREETS SHALL CONFORM TO THIS DETAIL.
  2. BARRIER CURB AND GUTTER ON CONCRETE STREETS SHALL CONFORM TO MDOT CURB AND GUTTER DETAIL F3.

**CONCRETE CURB AND GUTTER SD-R-1**

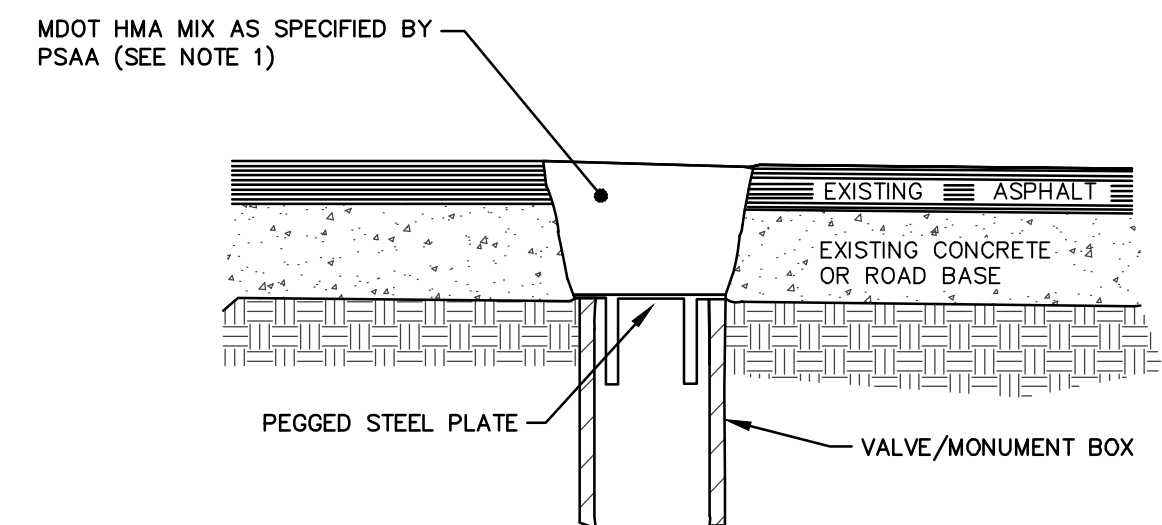


NOTE:  
 1. IF MANHOLE WILL BE PLACED IN GRAVEL ROAD, CASTING TO BE SET 6" TO 8" BELOW ROADWAY GRADE. ALL CONSTRUCTION METHODS SHALL REMAIN AS SHOWN ABOVE.

**MANHOLE CASTING ADJUSTMENT SD-GU-5**



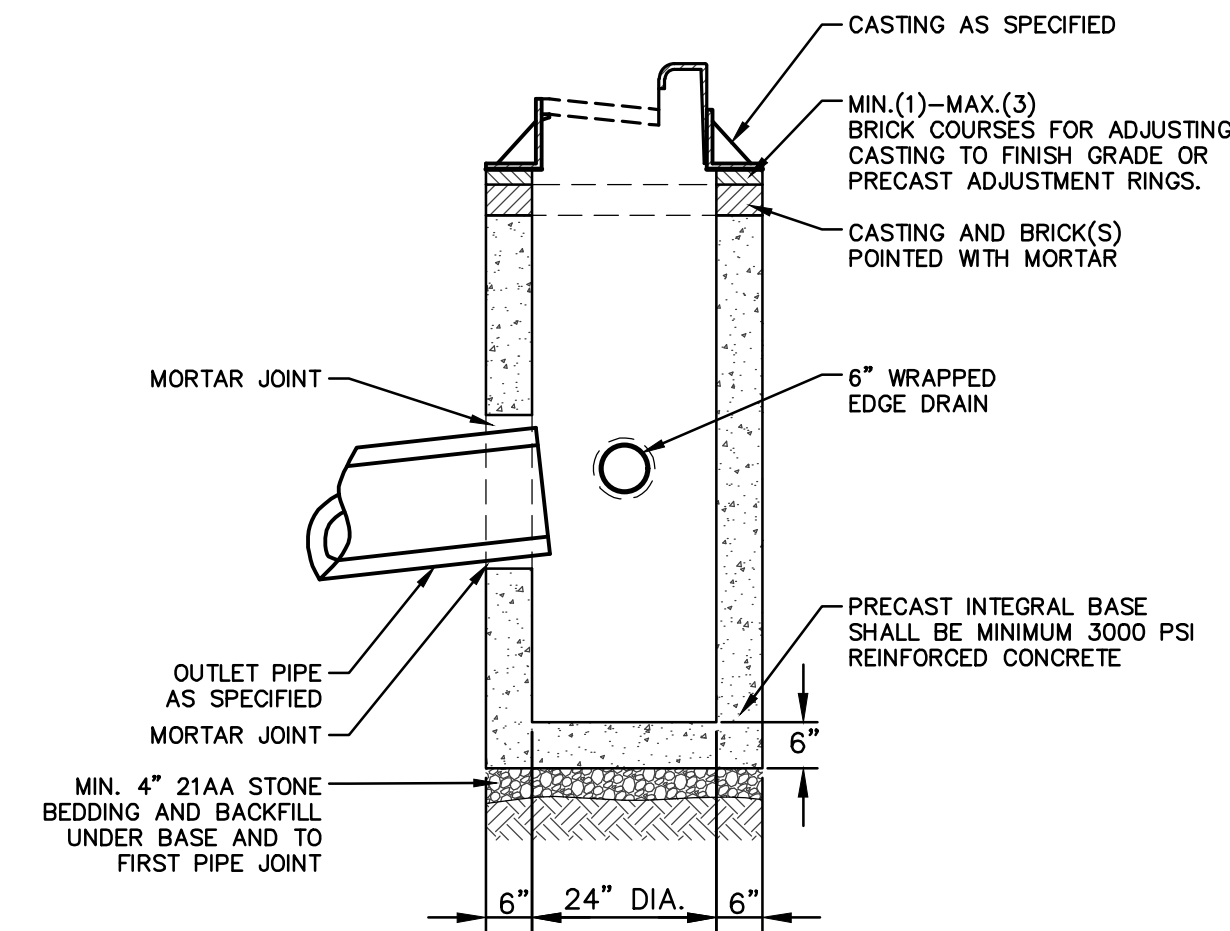
**MANHOLE LOWERING DETAIL**



**VALVE BOX/MONUMENT BOX LOWERING DETAIL (SEE NOTE 3)**

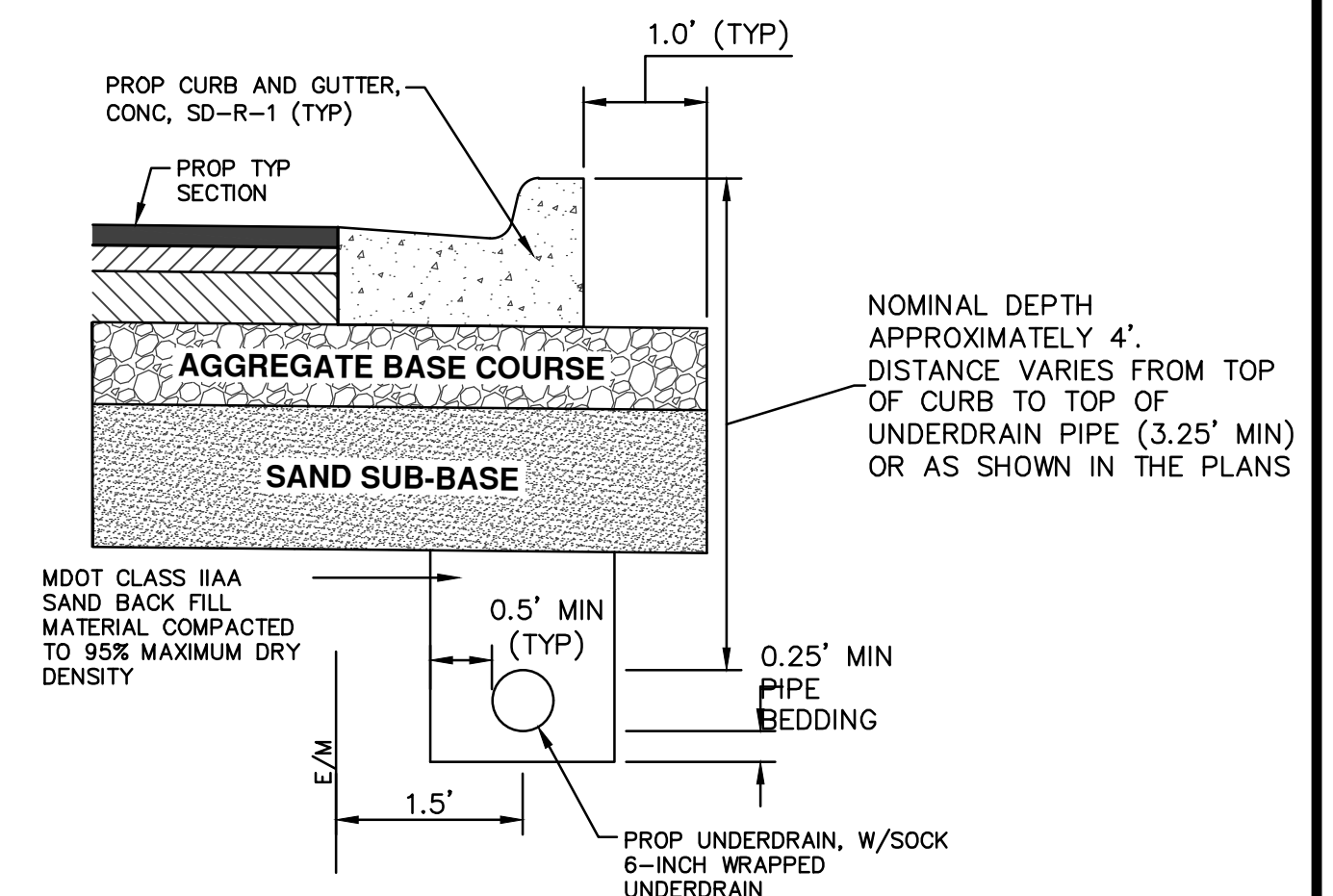
- NOTES:
1. IF TRAFFIC IS TO BE MAINTAINED ON THE ROADWAY BEFORE OR AFTER THE COLD MILLING OPERATION, THE STRUCTURE SHALL BE LOWERED TO THE EXTENT THAT A MINIMUM OF THREE(3) INCHES ASPHALT MATERIAL MDOT HMA MIX AS SPECIFIED BY PSAA, OR ENGINEER APPROVED EQUAL, REMAINS TO SUPPORT TRAFFIC.
  2. IF THE ROADWAY BEING MILLED IS CLOSED TO TRAFFIC, THE STRUCTURE SHALL BE LOWERED SUCH THAT THE STEEL PLATE IS A MINIMUM OF FOUR(4) INCHES BELOW THE PROPOSED ROAD GRADE AND THE RESULTING VOID SHALL BE FILLED WITH MDOT HMA MIX AS SPECIFIED BY PSAA OR ENGINEER APPROVED EQUAL.
  3. WHERE A MONUMENT IS TO BE LOWERED, THE CONTRACTOR SHALL GIVE THE ENGINEER A MINIMUM OF 48 HOURS WRITTEN NOTICE SO THAT THE MONUMENT CAN BE PROPERLY WITNESSED OR PROTECTED. FAILURE TO DO SO SHALL RESULT IN THE ENGINEER REPLACING SAID MONUMENT AT THE CONTRACTOR'S EXPENSE.

**MANHOLE & VALVE/MONUMENT BOX LOWERING SD-GU-9**



- NOTES:
1. MAY BE USED WITH SINGLE OUTLET PIPE AND SINGLE INLET PIPE.
  2. FRONT EDGE OF INLET CASTING SHALL BE FLUSH WITH FRONT EDGE OF GUTTER (EDGE-OF-METAL).

**PRECAST SINGLE INLET SD-ST-3**



- NOTES:
1. IN AREAS WHERE EDGE DRAIN CANNOT BE INSTALLED IN ACCORDANCE WITH THE DETAIL, THE EDGE DRAIN SHALL BE INSTALLED AT THE DEPTH AS INDICATED ON THE PLANS, OR AS DIRECTED BY ENGINEER. IN NO CASE SHALL THE EDGE DRAIN BE INSTALLED AT A GRADE LESS THAN 0.50% OR AT A DEPTH OF LESS THAN 2' BELOW TOP OF PROPOSED PAVEMENT.
  2. FOR PAVEMENT BASE AND SUBBASE THICKNESS, SEE TYPICAL PAVEMENT CROSS-SECTION(S)
  3. TRENCH DETAILS SHOW TYPE OF BACKFILL AND SURFACE RESTORATION ONLY
  4. ALL TRENCHING TO CONFORM TO ALL APPLICABLE M.I.O.S.H.A. STANDARDS
  5. EDGE DRAINS SHALL BE CONNECTED TO A DRAINAGE STRUCTURE AND WILL EXTEND A MINIMUM OF 100 FEET UPSLOPE FROM THE STRUCTURE.
  6. ADDITIONAL LENGTHS OF EDGE DRAIN MAY BE REQUIRED BY THE ENGINEER BASED ON EXISTING SITE CONDITIONS, INCLUDING CONDITION OF THE SUBGRADE.

**TYPICAL EDGE DRAIN TRENCH SD-TD-4**

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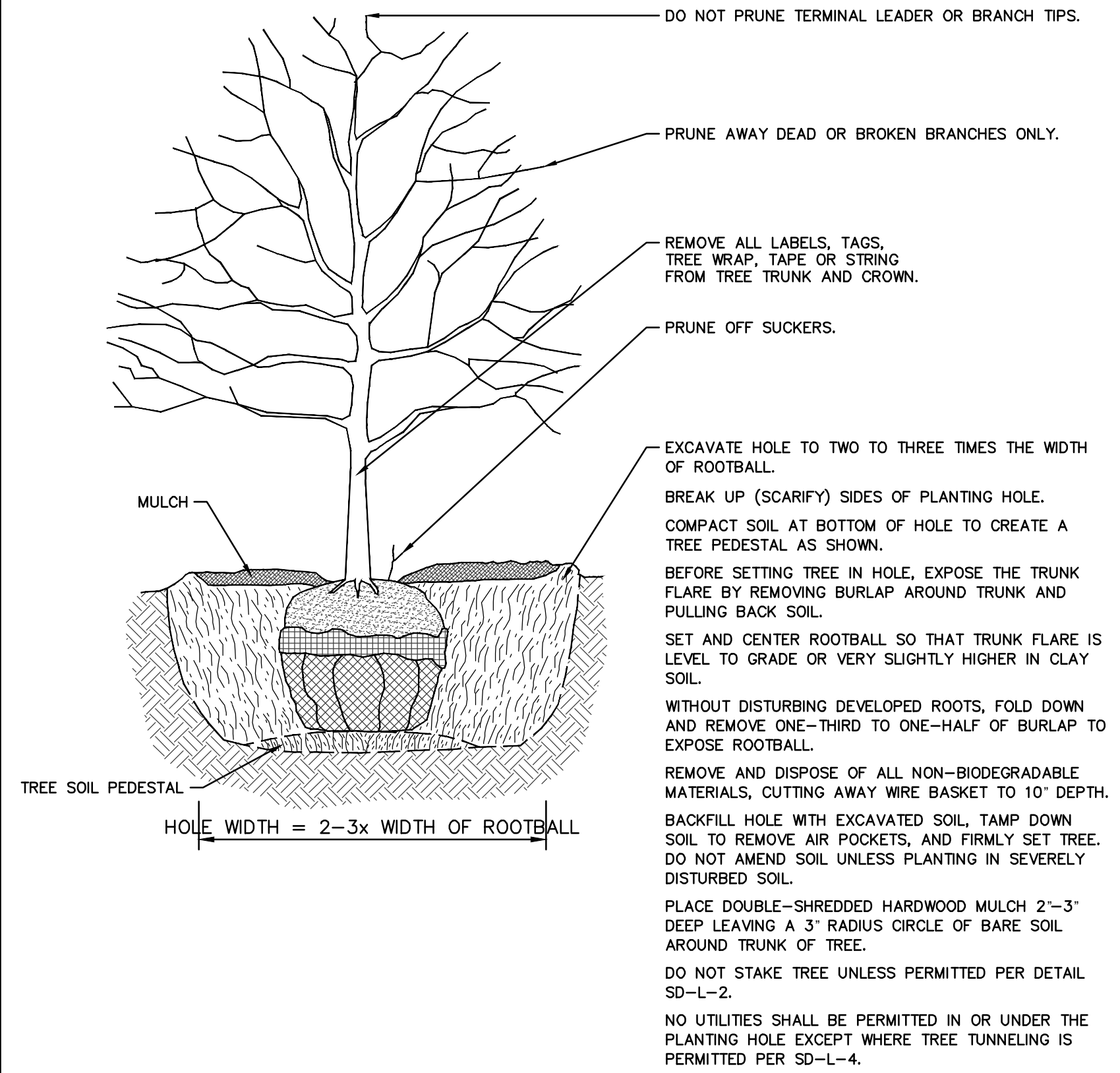
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 ANN ARBOR, MI 48106-6647  
 ANN ARBOR: 734-794-4410  
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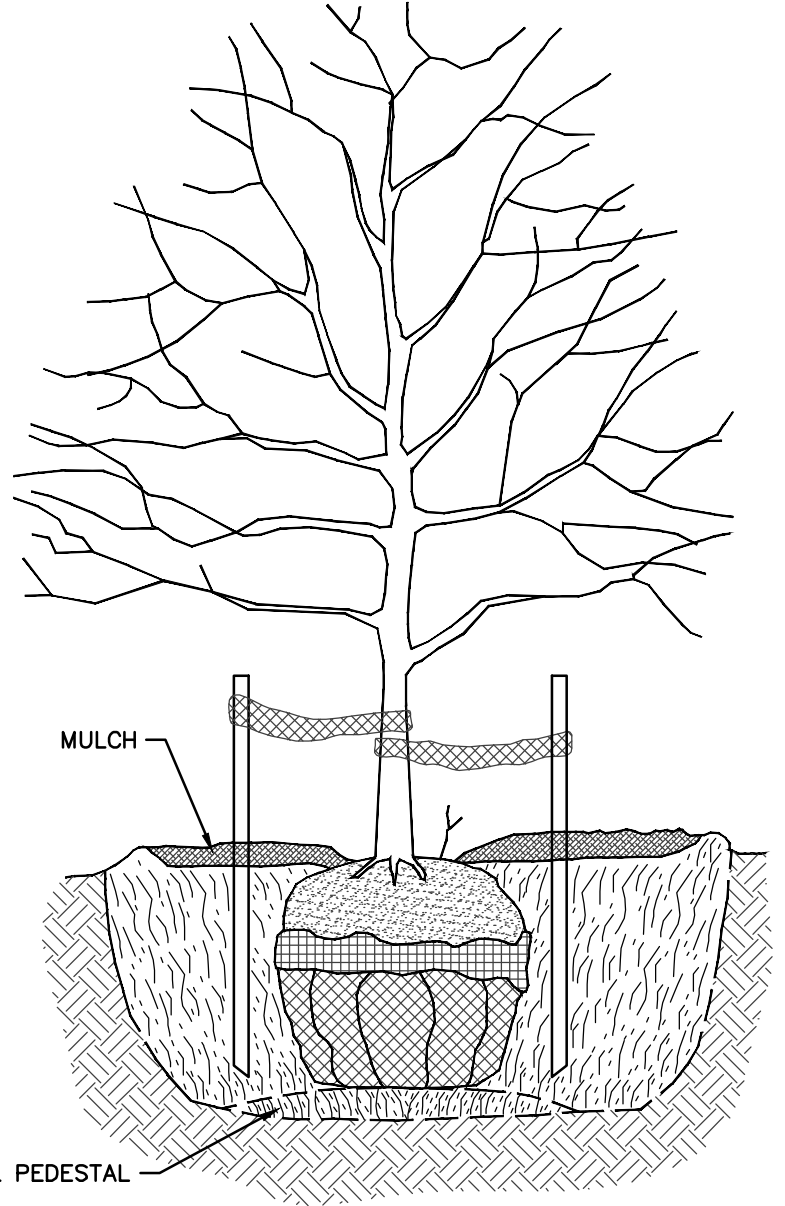


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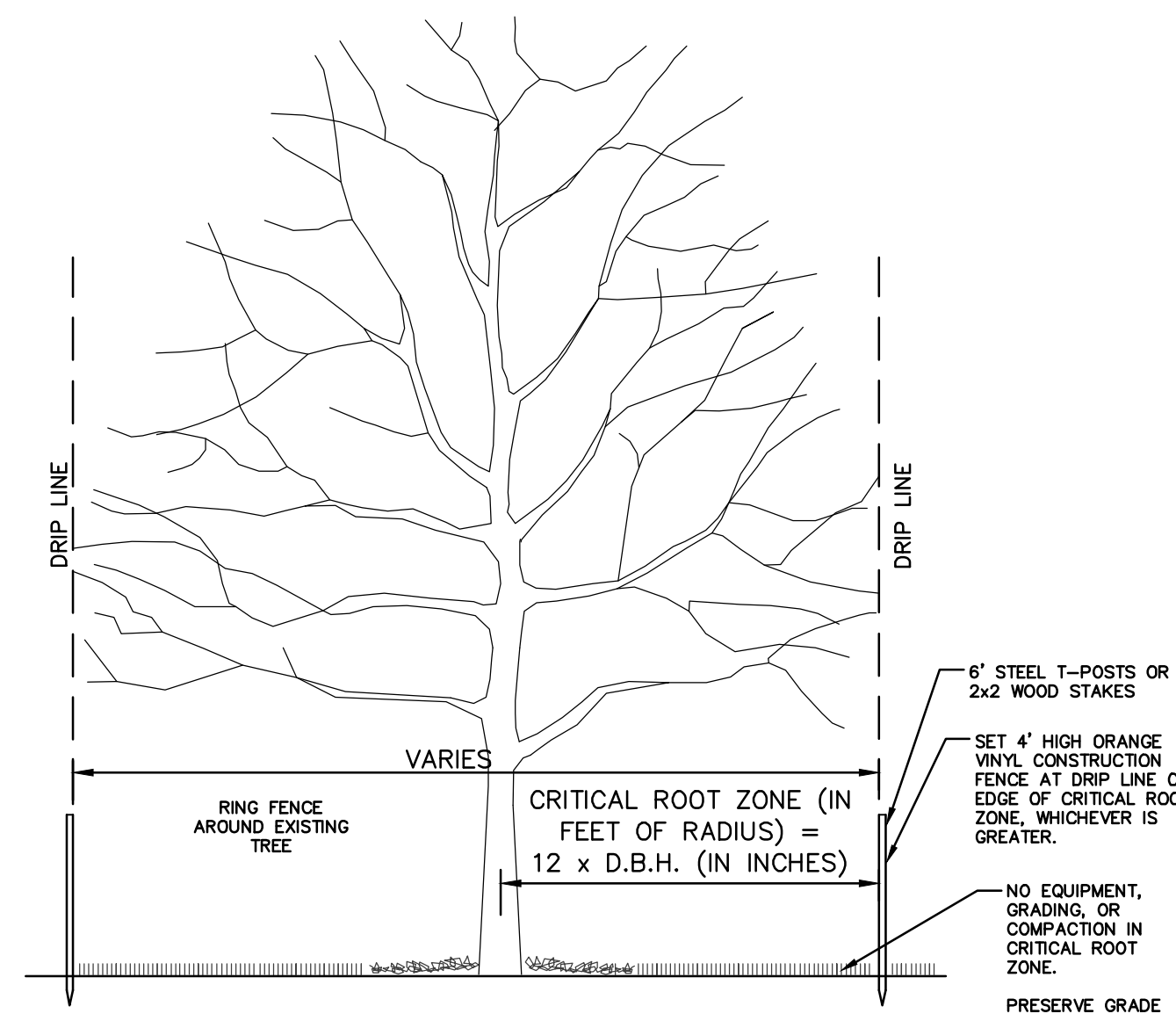
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**TREE PLANTING SD-L-1**

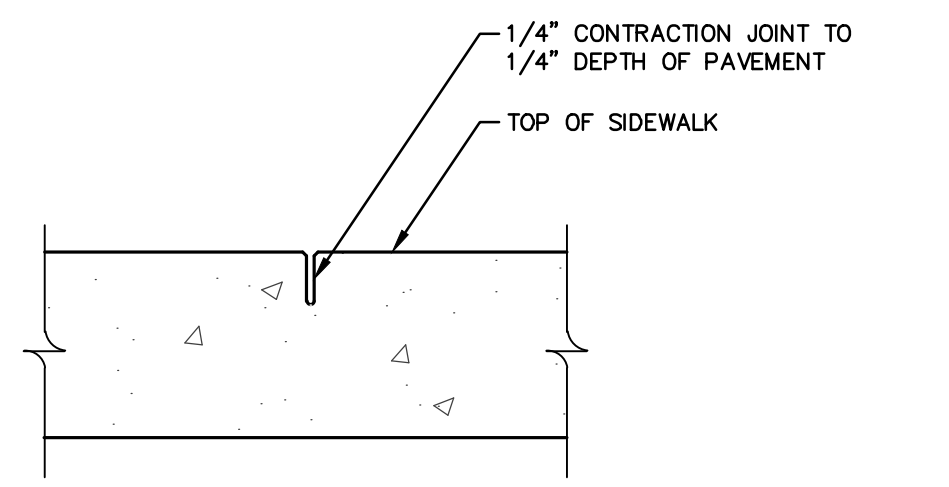


**TREE STAKING SD-L-2**

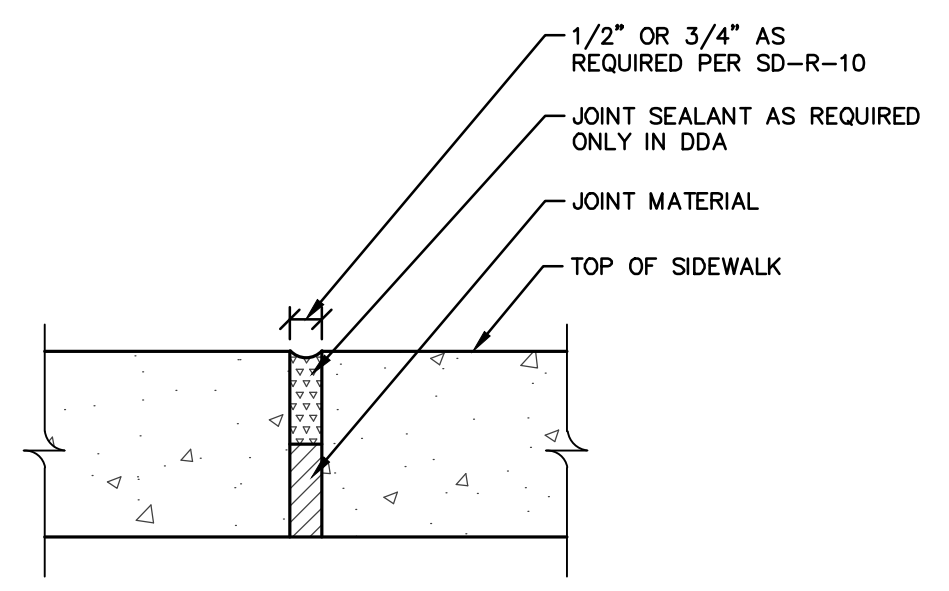


**TREE PROTECTION SD-L-3**

GENERAL NOTES:  
 1. DESIGN MAY UTILIZE TOOLED OR SAW-CUT CONTRACTION JOINT. PLANS MUST INDICATE SELECTION OF JOINT TYPE. PROJECT MUST HAVE EITHER JOINT TYPE, BUT NOT BOTH.

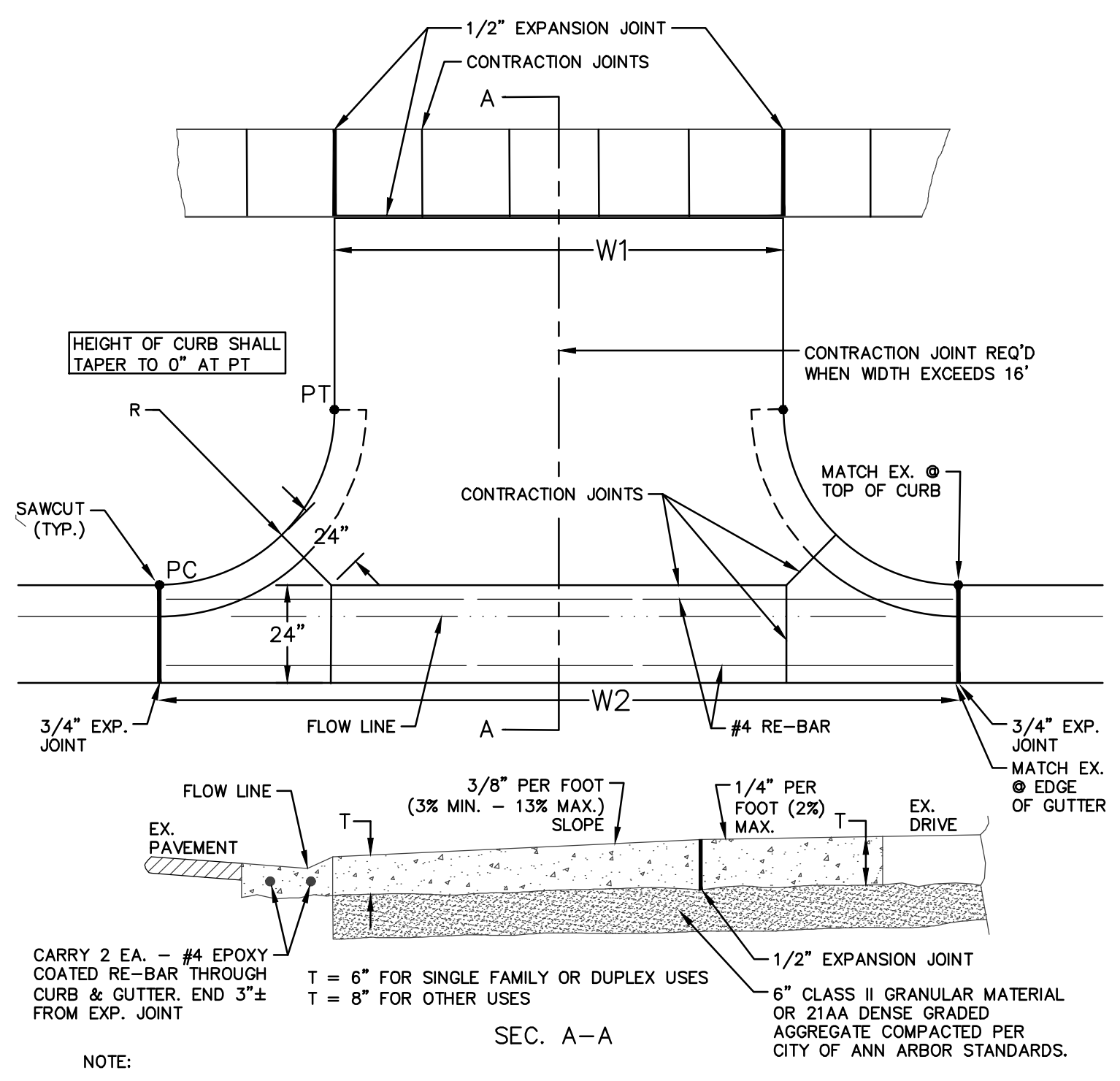


**CONTRACTION JOINT**

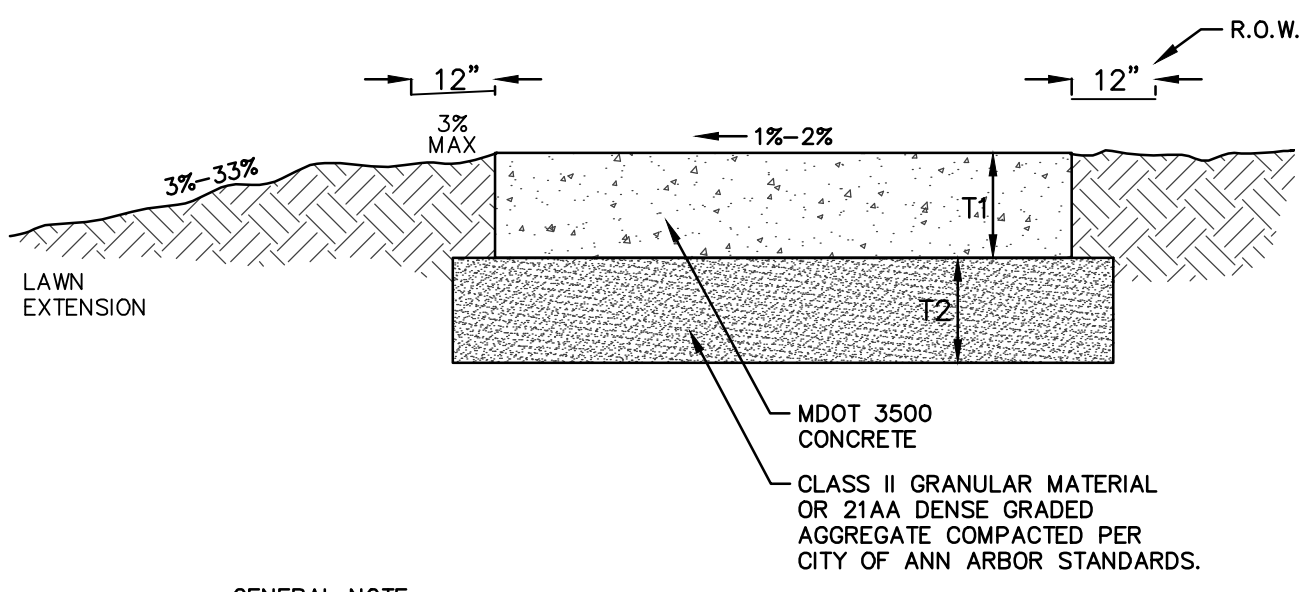


**EXPANSION JOINT**

**SIDEWALK CURB AND GUTTER JOINTS SD-R-11**

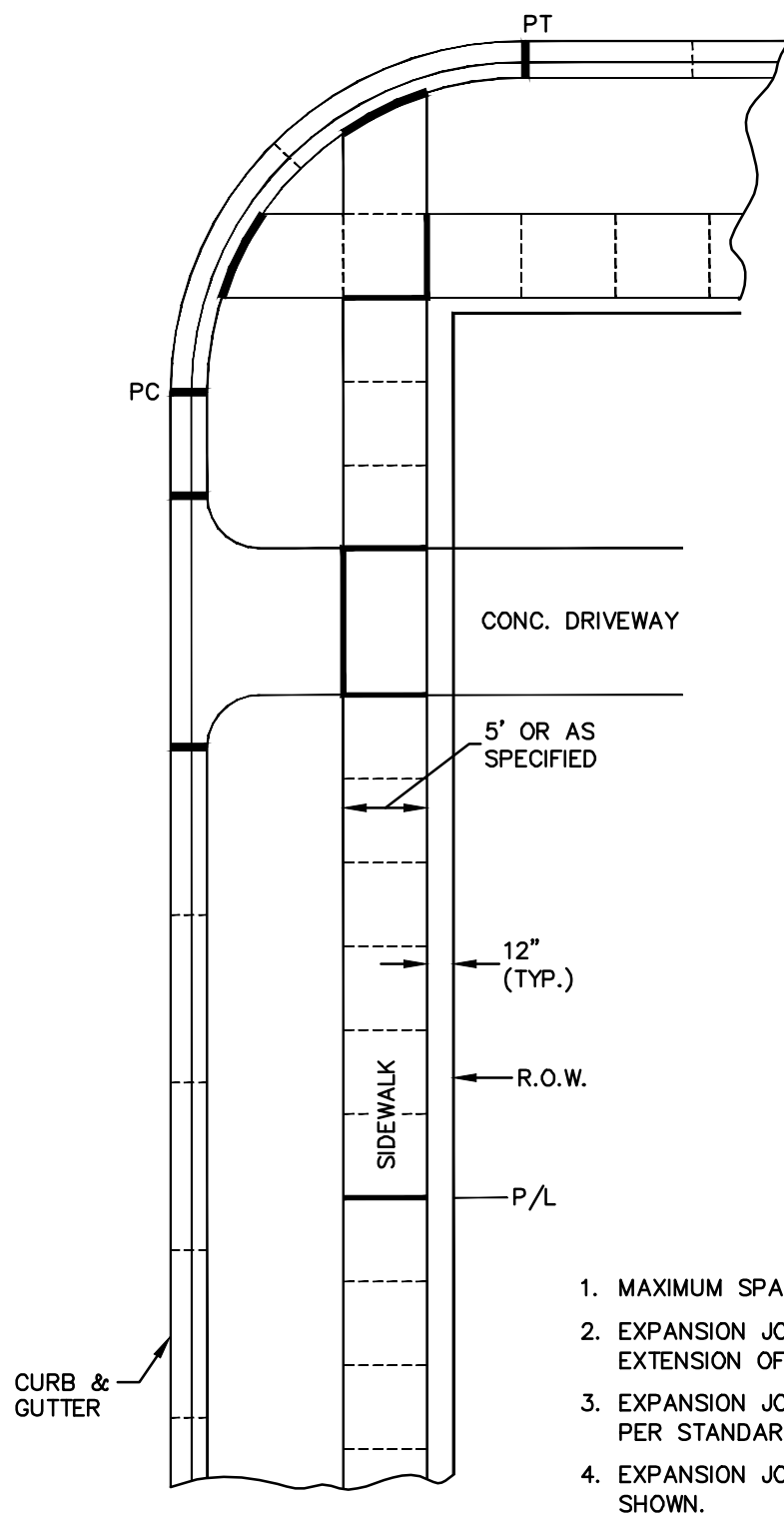


**TYPE M DRIVE APPROACH FOR ASPHALT STREETS WITH BARRIER CURB SD-R-6**



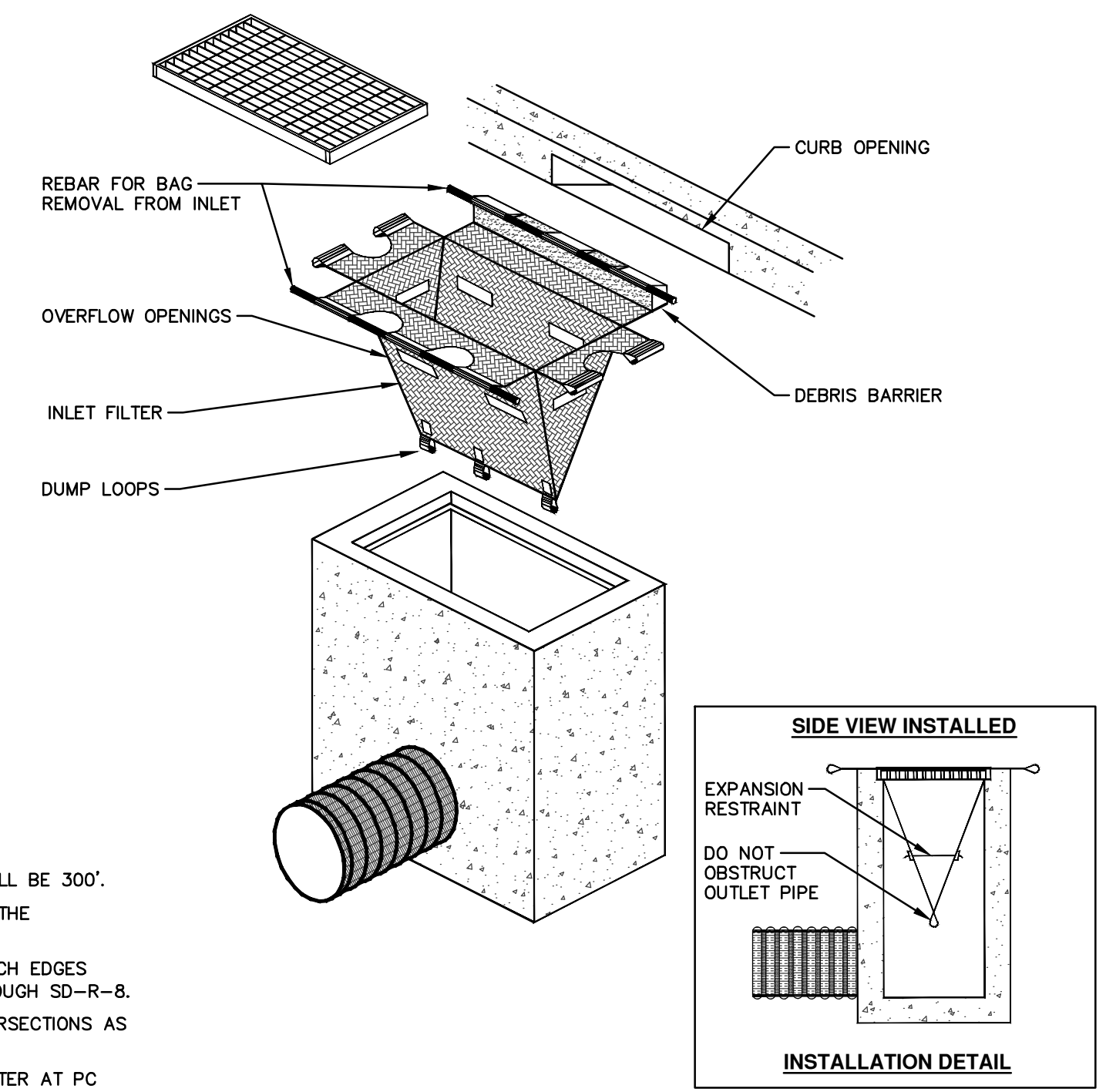
- GENERAL NOTE:  
 SIDEWALKS IN THE DDA SHALL BE CONSTRUCTED PER DETAILS SD-DDA-1 THROUGH SD-DDA-7.
- NOTES:  
 1. STANDARD SIDEWALK WIDTH SHALL BE 5'.  
 2. STANDARD SLAB LENGTH SHALL BE 5'.  
 3. MINIMUM SLAB LENGTH SHALL BE 3' AND MAXIMUM 7'.  
 4. MINIMUM SIDEWALK THICKNESS (T1) SHALL BE 4".  
 5. SIDEWALK THICKNESS (T1) SHALL BE INCREASED AT DRIVE APPROACHES TO 6" FOR SINGLE OR DUPLEX USES AND TO 8" FOR ALL OTHER USES.  
 6. MINIMUM BASE THICKNESS (T2) SHALL BE 4".  
 7. MINIMUM BASE THICKNESS (T2) SHALL BE INCREASED TO 6" AT DRIVE APPROACHES.  
 8. NATIVE MATERIAL IS ACCEPTABLE FOR SIDEWALK REPLACEMENT IF BASE IS STABLE AND FREE OF ORGANIC OR DELETERIOUS MATERIALS.  
 9. SIDEWALK RAMPS SHALL BE CONSTRUCTED AT STREET INTERSECTIONS AS DIRECTED AND SHALL COMPLY WITH THE REQUIREMENTS OF MDOT DETAIL R-28 (LATEST VERSION).  
 10. IF SIDEWALKS ARE APPROVED TO MEANDER WITHIN THE RIGHT-OF-WAY TO PROTECT AND SAVE TREES, SLOPES, ETC., CURVES IN THE SIDEWALK SHALL HAVE A MINIMUM 5' RADIUS, WITH A MINIMUM 3' LAWN EXTENSION.  
 11. EXPANSION AND CONTRACTION JOINTS SHALL BE PROVIDED PER SIDEWALK AND CURB & GUTTER JOINT SPACING DETAIL SD-R-10.

**SIDEWALK CROSS SECTION SD-R-9**



- LEGEND:  
 CONTRACTION JOINTS - - - - -  
 1/2" EXPANSION JOINTS - - - - -  
 3/4" EXPANSION JOINTS - - - - -

**SIDEWALK AND CURB & GUTTER JOINT SPACING SD-R-10**



1. MAXIMUM SPACING BETWEEN ALL EXPANSION JOINTS SHALL BE 300'.  
 2. EXPANSION JOINTS SHALL BE PLACED IN SIDEWALKS AT THE EXTENSION OF ALL PROPERTY LINES.  
 3. EXPANSION JOINTS SHALL BE PLACED AT DRIVE APPROACH EDGES PER STANDARD DRIVE APPROACH DETAILS SD-R-6 THROUGH SD-R-8.  
 4. EXPANSION JOINTS SHALL BE PLACED AT SIDEWALK INTERSECTIONS AS SHOWN.  
 5. EXPANSION JOINTS SHALL BE PLACED IN CURB AND GUTTER AT PC AND PT OF INTERSECTION RADII.  
 6. CONTRACTION JOINT SPACING FOR CURB AND CURB SHALL BE 10' STANDARD AND 8' MINIMUM.  
 7. CONTRACTION JOINTS FOR SIDEWALKS SHALL BE PLACED AT ALL SLAB ENDS (5' TYPICAL, 3' MINIMUM TO 7' MAXIMUM).  
 1. SIZE AND SHAPE OF INLET FILTER SHALL MATCH THE STRUCTURE.  
 2. WHERE CONDITIONS WARRANT, THE FILTER SHALL BE MADE WITH AN OIL-ABSORBENT FILTER WITH A WOVEN PILLOW INSERT.

**INLET PROTECTION SD-SESC-1**

**811** Know what's below. Call before you dig.

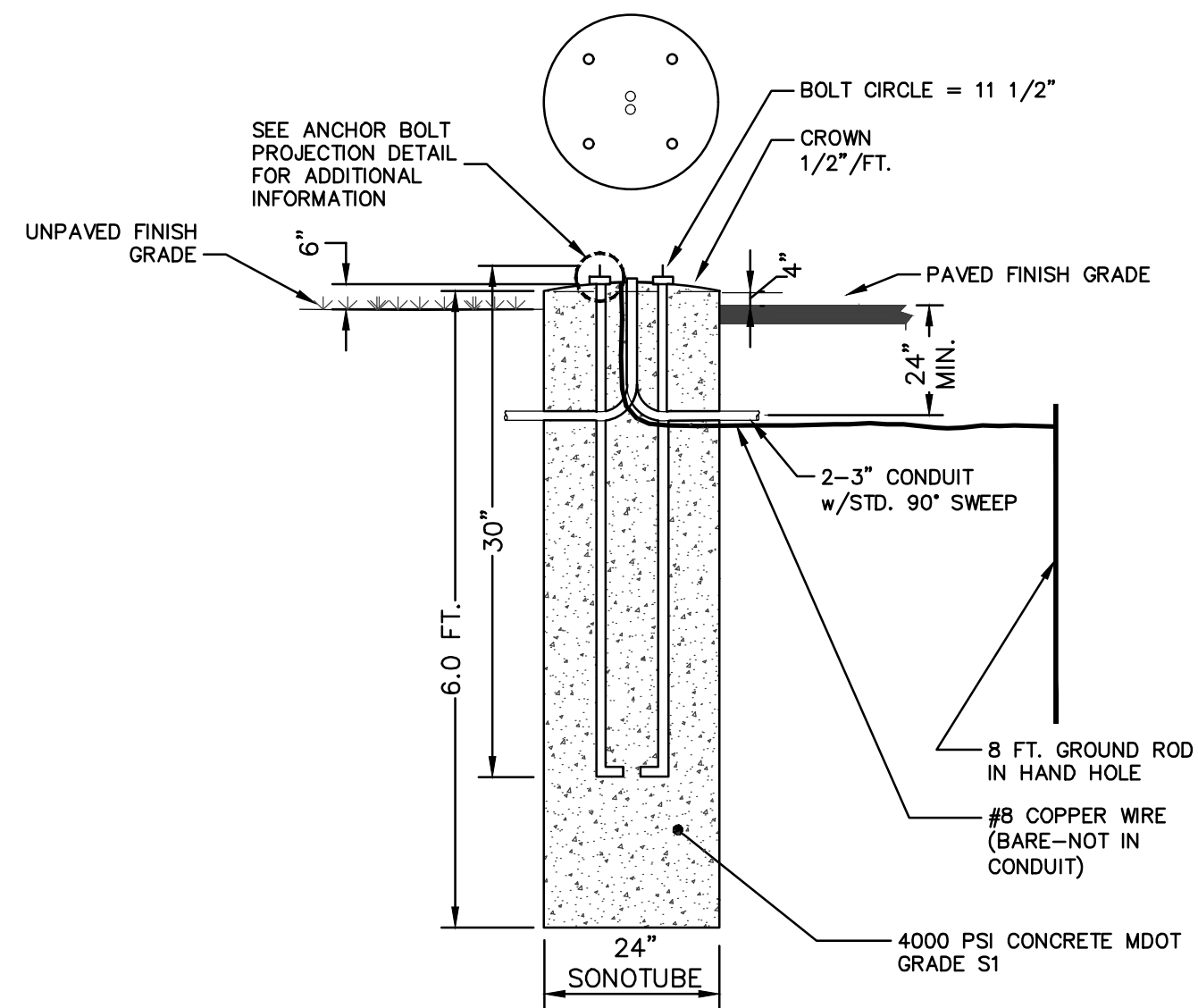
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**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**

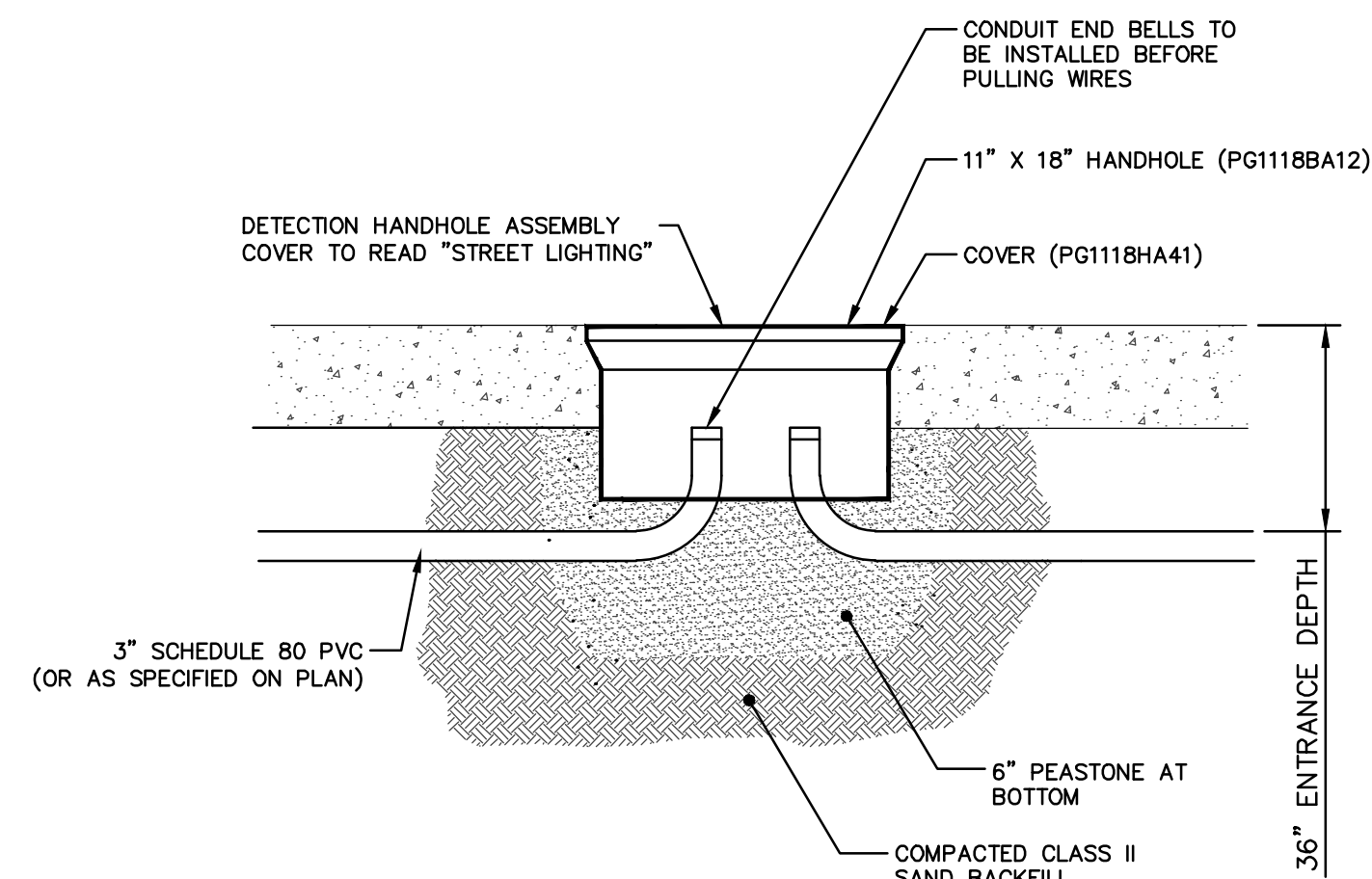
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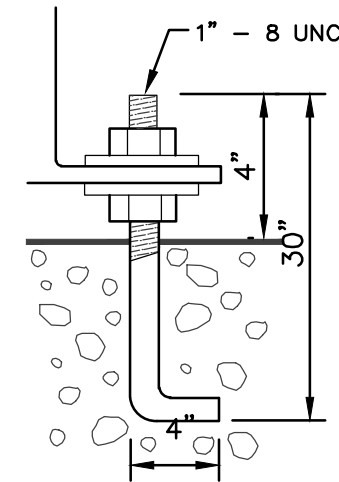
- NOTES:**
- HOLE TO BE AUGERED. MINIMIZE DISTURBANCE OF IN-SITU SOILS DURING AUGERING.
  - CONTRACTOR TO PROVIDE PREFABRICATED ANCHOR BOLT BUILD-UP.
  - THE CITY WILL INSPECT THE AUGERED HOLE AND THE ANCHOR BOLT BUILD-UP AND PROVIDE WRITTEN APPROVAL PRIOR TO THE PLACEMENT OF CONCRETE.
  - NO WATER IS TO BE IN HOLE AT TIME OF CONCRETE PLACEMENT.
  - CONCRETE SHALL BE VIBRATED DURING PLACEMENT.
  - CONTRACTOR WILL PROVIDE NECESSARY CONDUIT FOR CONDUCTOR ENTRY.
  - COPPER CLAD GROUND ROD (1 REQUIRED) TO BE 5/8" DIA. x 8'-0".
  - CONDUIT TO EXTEND 1'-2" ABOVE BASE. CABLES TO EXTEND 6" OUTSIDE OF HAND HOLE.
  - GROUND CABLE SHALL BE #6 SOFT BARE COPPER WIRE WELDED TO GROUND ROD WITH 24" SLACK ABOVE FOUNDATION TOP. THE NEUTRAL AT THE POLE IS TO BE CONNECTED TO THIS GROUND CABLE.

**STREETLIGHT FOUNDATION SD-SL-1**



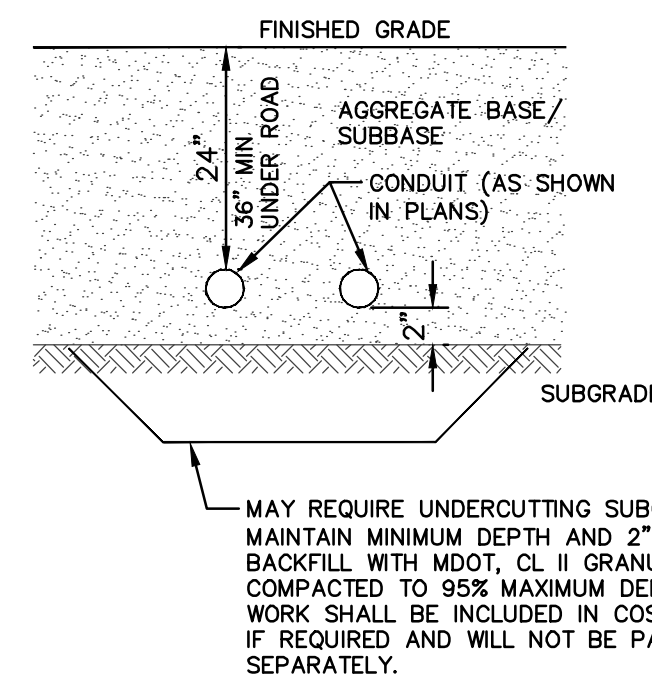
**INSTALLATION NOTE:**  
 THE CONDUIT SHALL BE LOWERED TO A MINIMUM ENTRANCE DEPTH OF 36" BELOW TOP OF THE HANDLE OVER A DISTANCE OF 10 FT. ON EACH SIDE OF THE HANDHOLE ASSEMBLY  
 IF THE CONDUIT HAS TO ENTER THE HANDHOLE AT A DEPTH GREATER THAN 36" DEEP DUE TO CONFLICT, THEN 90 DEGREE SWEEPS SHALL BE PROVIDED.  
 BACKFILL CONDUIT AND HANDHOLE W/CLASS II SAND. IF PLACED IN GREENBELT, PLACE 4" OF TOP SOIL.

**ELECTRICAL HANDHOLE ASSEMBLY SD-E-3**



ALSO PROVIDE SHAKESPEARE OPAB-2035 ANCHOR BOLT KIT FOR EACH LIGHT POLE. ANCHOR BOLTS TO BE HOT-DIPPED GALVANIZED, 30" LONG, WITH 4" HOOK, AND A 4" PROJECTION ABOVE FOUNDATION.

**ANCHOR BOLT PROJECTION DETAIL**



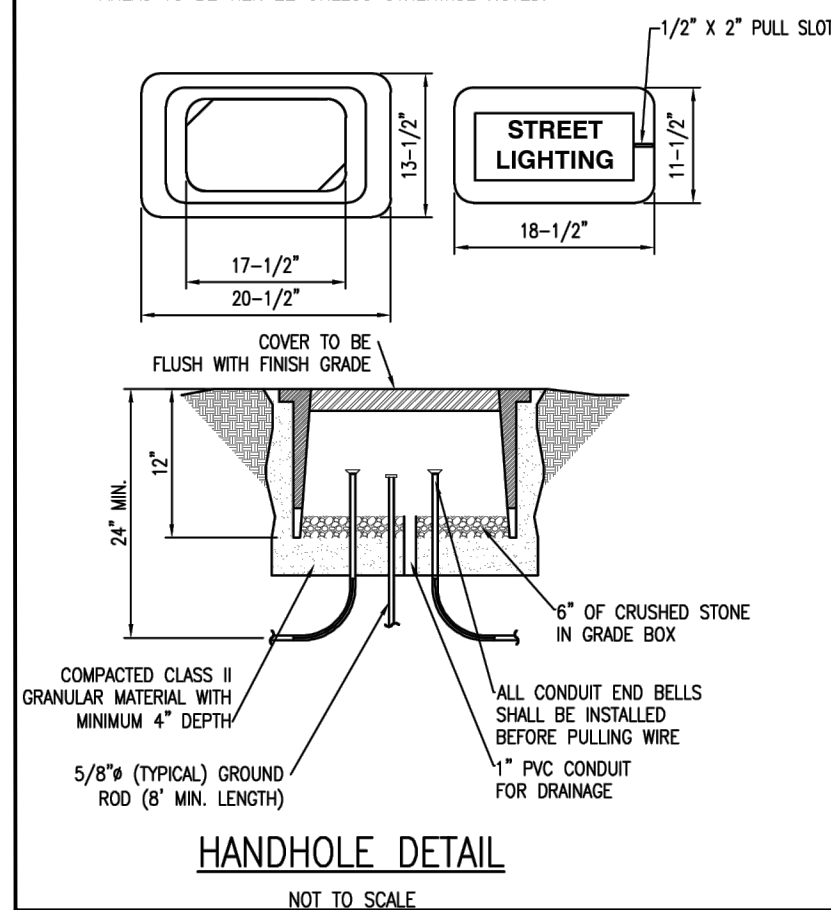
**CONDUIT PLACEMENT DETAIL SD-E-2**

**LIGHT FIXTURE SCHEDULE**

TYPE	DESCRIPTION	MANUFACTURER	CATALOG #	LAMPS	REMARKS
SL1	DECORATIVE GLOBE STREET LIGHT	LUMECON	L35W --1-NW-B-D	LED	ORIENT BANNER ARMS AS DIRECTED BY ENGINEER. ORIENT RECEPTACLE AND POLE HANDHOLE AWAY FROM ROAD. LUMINAIRES ARE FURNISHED AND INSTALLED BY CONTRACTOR. POLES WITH BANNER ARMS, POLE HANDHOLES AND RECEPTABLES ARE CONTRACTOR FURNISHED AND INSTALLED. FIXTURE TO BE MOUNTED ON POLE TYPE LP1.
SL2	DUAL FIXTURE DECORATIVE GLOBE STREET LIGHT	LUMECON	L35W --1-NW-B-D	LED	ORIENT BANNER ARMS AS DIRECTED BY ENGINEER. ORIENT RECEPTACLE AND POLE HANDHOLE AWAY FROM ROAD. ORIENT LUMINAIRES PARALLEL TO THE ROAD. LUMINAIRES ARE FURNISHED AND INSTALLED BY CONTRACTOR. POLES WITH BANNER ARMS, POLE HANDHOLES AND RECEPTABLES ARE CONTRACTOR FURNISHED AND INSTALLED. FIXTURE TO BE MOUNTED ON POLE TYPE LP2.
SL3	COBRA HEAD STREET LIGHT	LEOTEK	GCM2 40H MV NW 3R BK 850 FFA PCR7-CR	LED	FIXTURE TO BE MOUNTED ON POLE TYPE LP3.

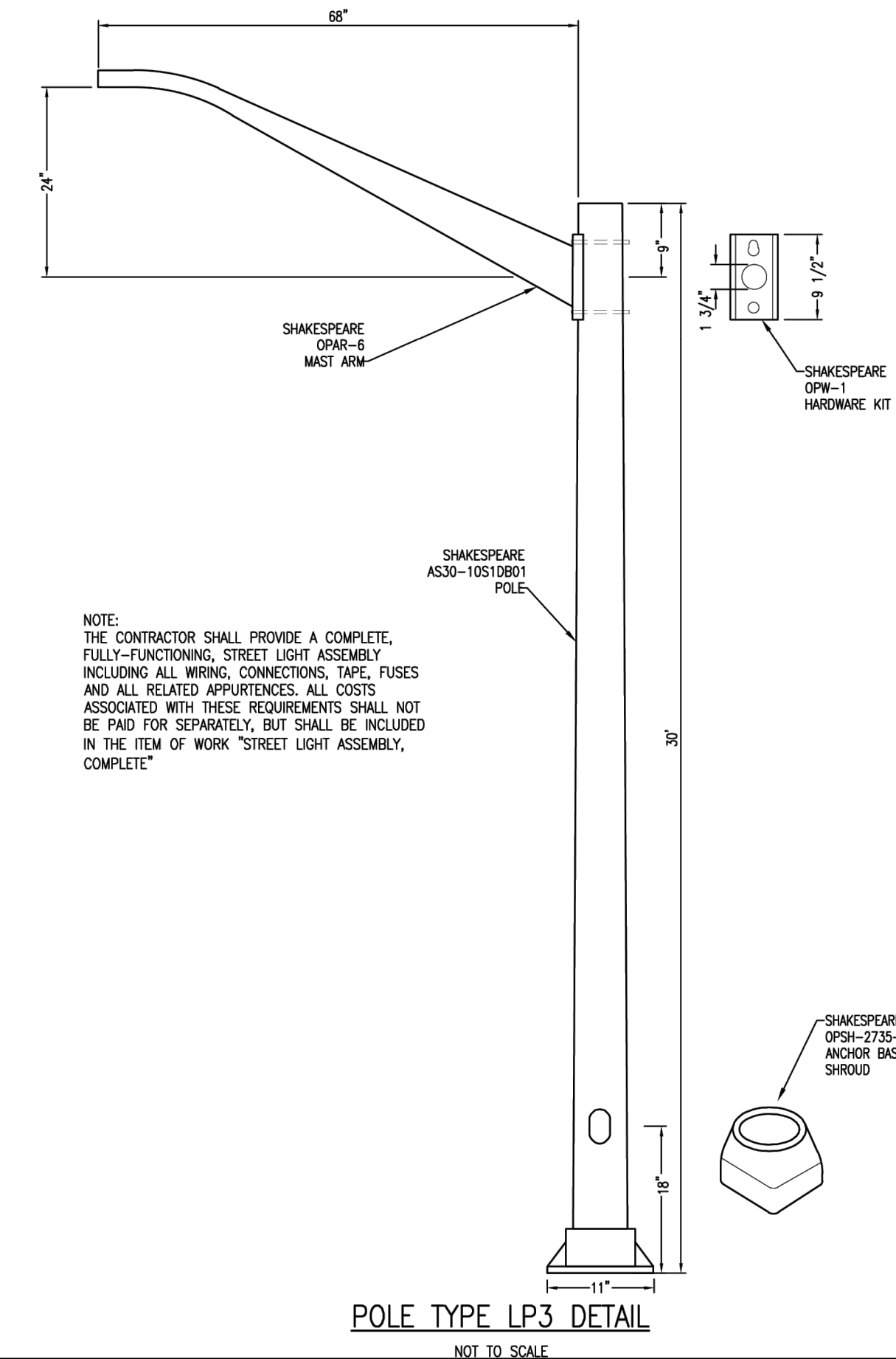
**GENERAL NOTES**

- PROVIDE QUARTZITE MODEL PC1118BA12 WITH COVER PC1118HA40 OR APPROVED EQUIVALENT.
- BOX AND COVER SHALL BE POLYMER CONCRETE.
- COVERS SHALL BE SECURED WITH 3/8" BOLTS, NUTS, AND WASHERS, WHICH SHALL BE BRASS, STAINLESS STEEL, OR OTHER CORROSION RESISTANT MATERIAL. STAINLESS STEEL SHALL HAVE A CHROMIUM CONTENT OF NOT LESS THAN 18% AND A NICKEL CONTENT OF NOT LESS THAN 8%. NUTS SHALL BE RECESSED BELOW TOP SURFACE OF COVER.
- COVER LETTERING SHALL BE 1/2" MINIMUM LETTERS CAST IN STANDARD MARKINGS: (STREET LIGHTING).
- CONDUIT ENTERING THE BOX SHALL HAVE 90° LONG RADIUS BEND (INSIDE THE BOX). THE CONDUITS OPENING INSIDE THE BOX SHALL BE AT LEAST 4" BELOW THE LID, OPENING SHALL HAVE SMOOTH EDGE. IF THE CONDUIT IS P.V.C. A SLP COUPLING MUST BE USED. IF THE CONDUIT IS RIGID PIPE, A PROTECTIVE BUSHING SHALL BE USED.
- CONDUCTORS SHALL HAVE A MINIMUM OF 24" SLACK FROM CONDUIT BELL END.
- BACKFILL WITH EXCAVATED MATERIAL AND THOROUGHLY COMPACT.
- WHERE HANDHOLES ARE INSTALLED IN CONCRETE AREAS, 1/2" PREMOLDED EXPANSION JOINT SHALL BE INSTALLED AROUND THE BOX.
- BOXES IN NON-VEHICLE TRAFFIC AREAS TO BE TIER 8 AND IN VEHICLE TRAFFIC AREAS TO BE TIER 22 UNLESS OTHERWISE NOTED.



**HANDHOLE DETAIL**

NOT TO SCALE



**POLE TYPE LP3 DETAIL**

NOT TO SCALE

**COBRA HEAD STREET LIGHT**



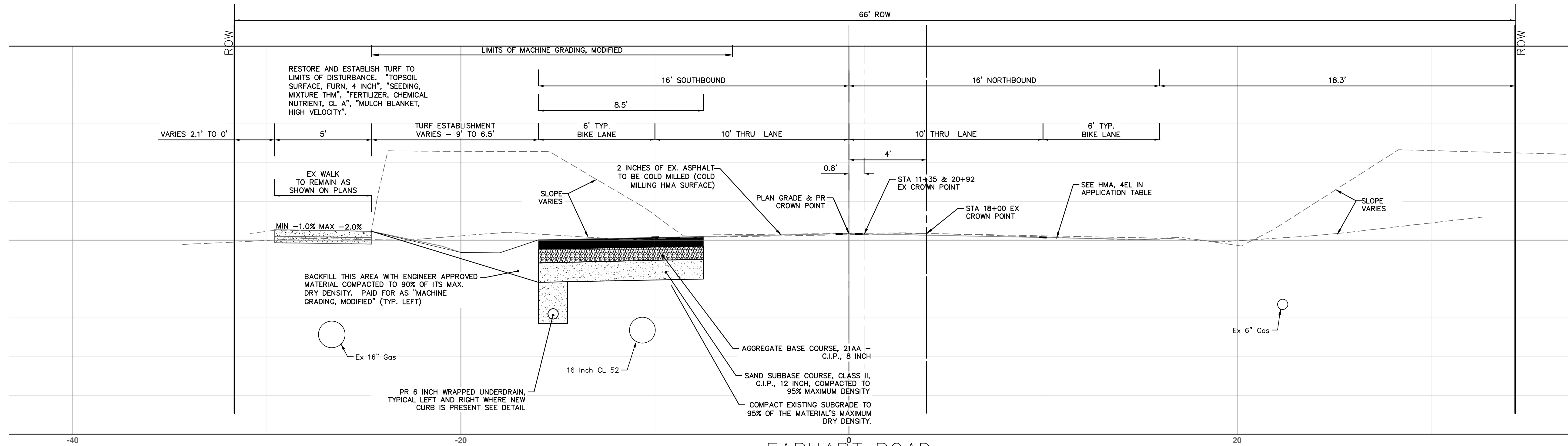
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 SCALE: N/A  
 DRAWING No. 2021023-7  
 SHEET No. 7 OF 74

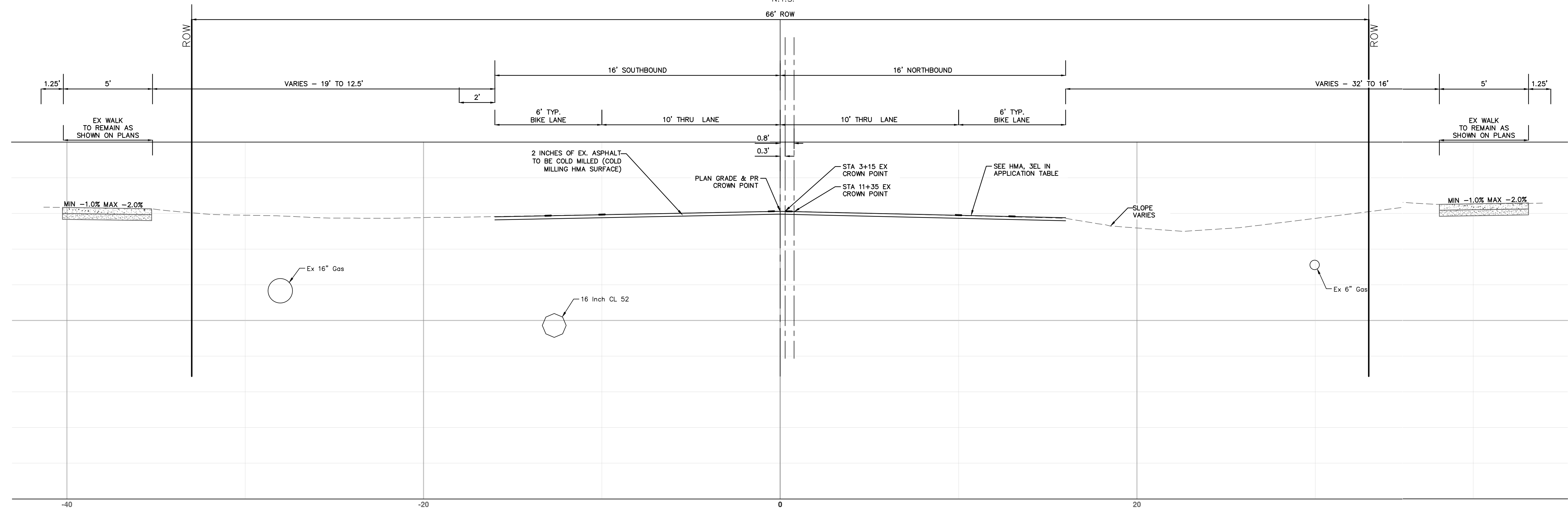
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**EARHART ROAD  
TYPICAL SECTION**

STA. 11+35 TO STA. 20+92

N.T.S.



**EARHART ROAD  
TYPICAL SECTION**

P.O.B. STA. 3+15 TO STA. 11+35

N.T.S.



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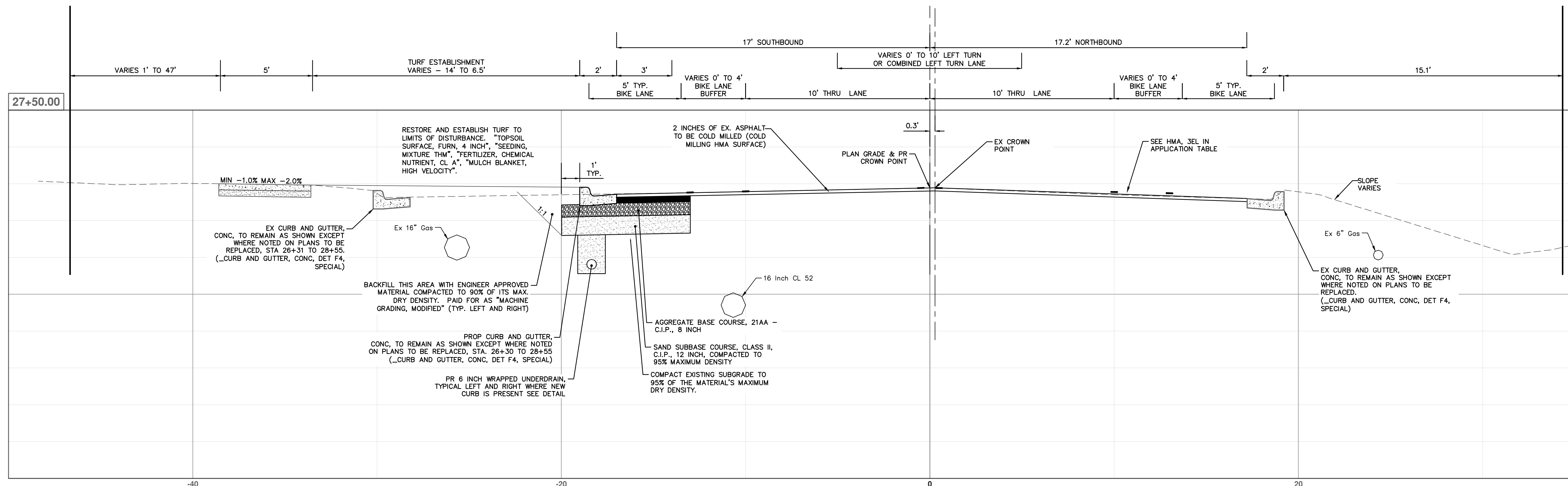
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EARHART ROAD IMPROVEMENTS (GEDDES - US23)

SCALE PLAN: NTS  
DRAWING No. 2021023-8

SHEET No.



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HMA APPLICATION ESTIMATE						
SUPERPAVE MIX	RATE OF APPLICATION	THICKNESS (INCHES)	AWI (MIN.)	BINDER	LOCATION/NOTES	
4EL	220 LB/SYD	2.0	260	58-28	TOP	
4EL	330 LB/SYD	3.0	-	58-28	BASE & LEVELING	
4EL	275 LB/SYD	2.5	260	-	HMA APPROACH	
4EL	275 LB/SYD	2.5	-	-	HAND PATCHING	
Bond Coat SS-1h	0.05 - 0.15 GAL/SYD	-	-	-	INCLUDE IN COST OF HMA ITEM	

**811**  
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**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
 EARHART ROAD TYPICAL SECTIONS

SCALE PLAN: NTS

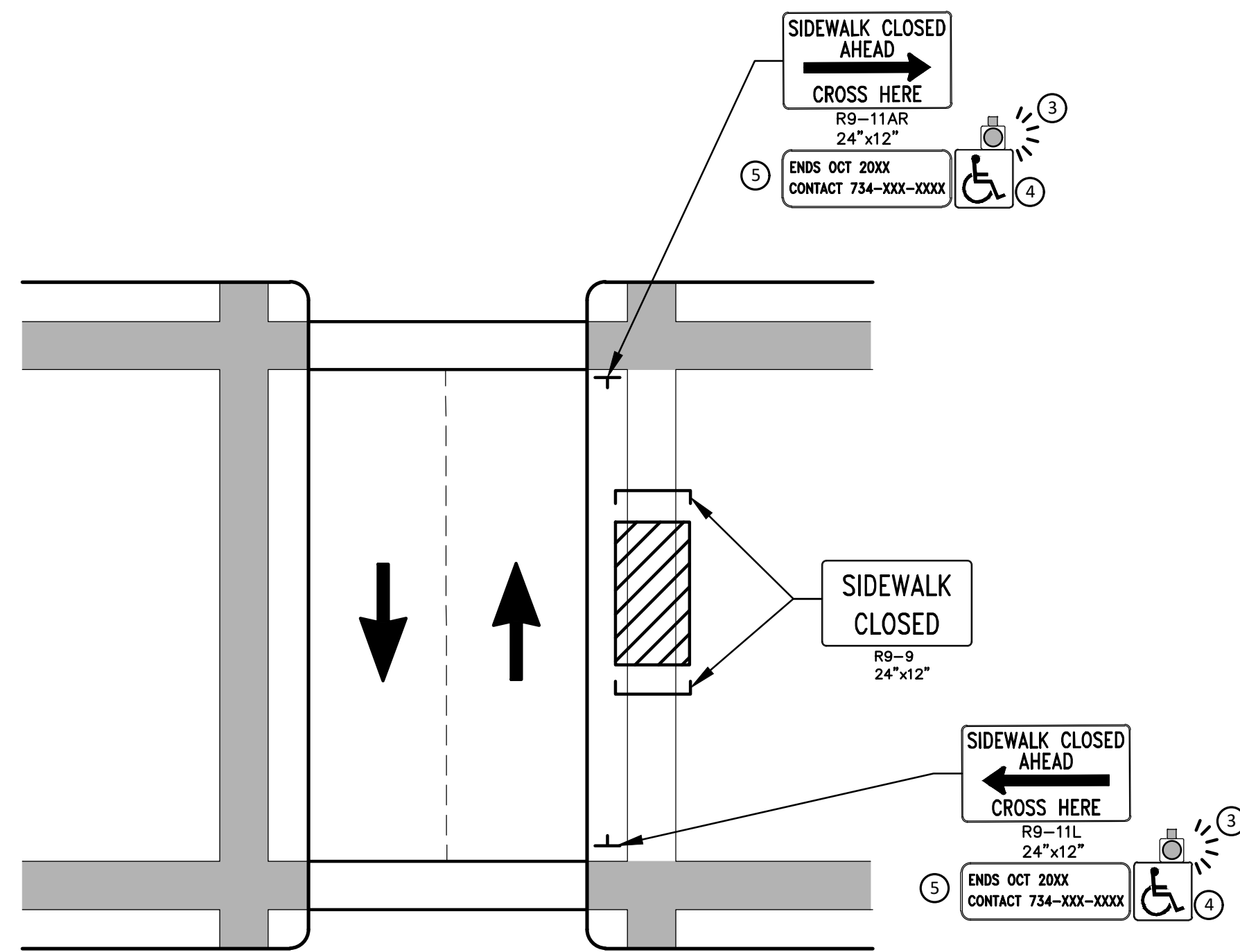
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SHEET No. 9 OF 74

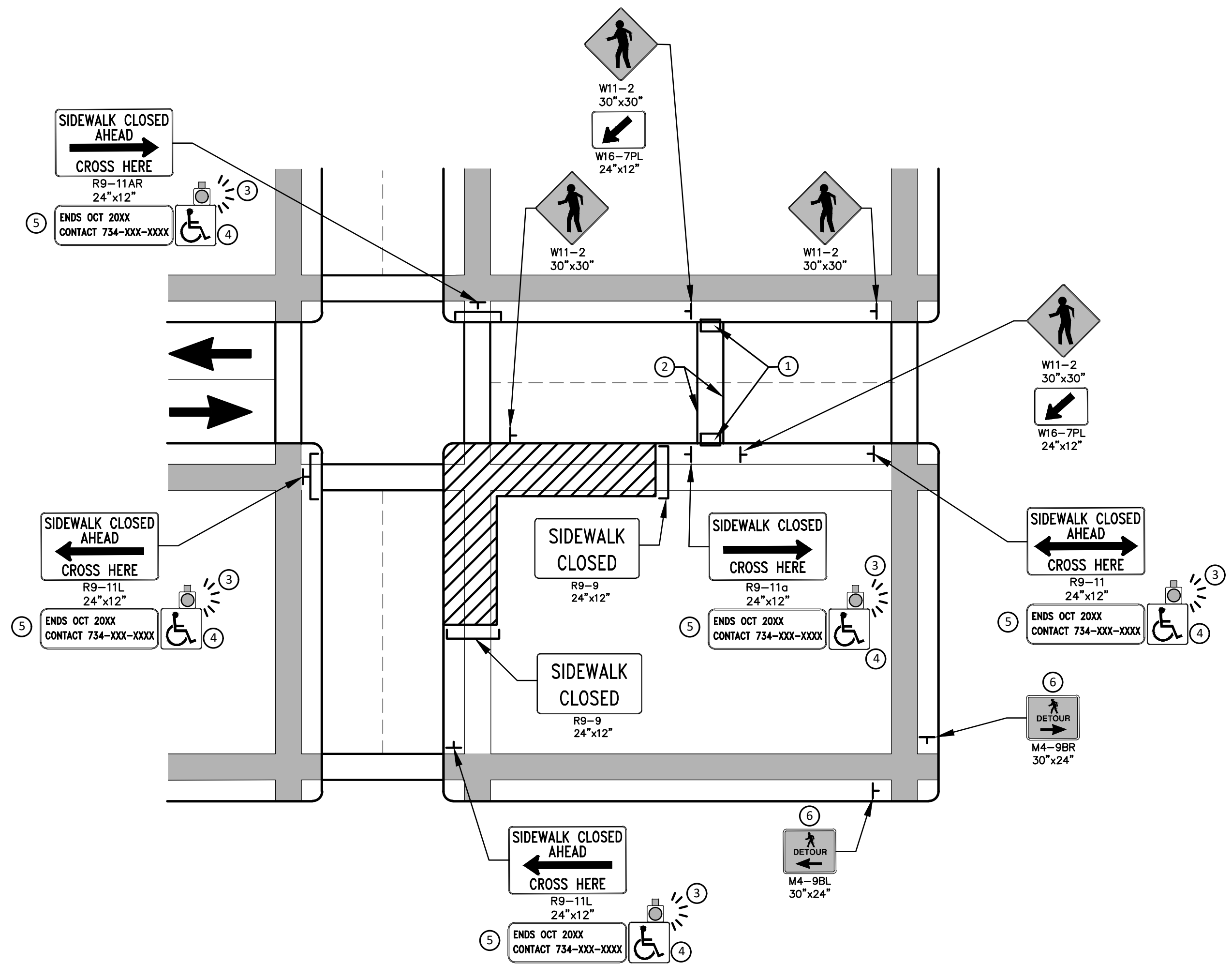
**EARHART ROAD TYPICAL SECTION**  
 STA. 20+92 TO STA. 31+00 P.O.E.  
 N.T.S.

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PEDESTRIAN DETOUR USING OPPOSITE SIDE OF STREET



OTHER SIDE OF STREET DETOUR OR DETOUR WITH TRAILBLAZING SIGNS  
(FOR CORNER SIDEWALK CLOSURE WITH OPTIONAL TEMPORARY CROSSWALK)

**GENERAL NOTES**

WHEN CLOSING OR RELOCATING CROSSWALKS OR SIDEWALKS, THE CONTRACTOR SHALL PROVIDE DETECTABLE TEMPORARY FACILITIES AND INCLUDE ACCESSIBILITY FEATURES CONSISTENT WITH EXISTING PEDESTRIAN FACILITIES.

TEMPORARY TRAFFIC CONTROL DEVICES FOR PEDESTRIANS ARE SHOWN. OTHER DEVICES MAY BE NECESSARY TO CONTROL VEHICULAR TRAFFIC. STAGE WORK, AS NECESSARY, TO PROVIDE AN ALTERNATE PEDESTRIAN ROUTE (APR) AT ALL TIMES. FOR ROADWAYS WITH NO AVAILABLE DETOURS, MAINTAIN ONE OPEN SIDEWALK AT ALL TIMES.

PROVIDE A SMOOTH, CONTINUOUS, HARD SURFACE THROUGH THE LENGTH OF THE APR. COMPACTED GRAVEL, AGGREGATE, OR SLAG MATERIALS ARE NOT ALLOWED. PROVIDE A FIRM, STABLE, AND SLIP RESISTANT TEMPORARY WALKWAY SURFACE TO COVER SHORT SEGMENTS OF ROUGH, SOFT, OR UNEVEN GROUND.

THE PEDESTRIAN TRAFFIC SIGNALS CONTROLLING CLOSED CROSSWALKS SHALL BE COVERED OR DEACTIVATED BY THE CITY OF ANN ARBOR. THE CONTRACTOR SHALL SCHEDULE AND COORDINATE THIS WORK WITH THE ENGINEER A MINIMUM OF 72 HOURS (NOT INCLUDING WEEKENDS & HOLIDAYS) PRIOR TO THE BEGINNING OF WORK THAT REQUIRES A SIDEWALK CLOSURE.

POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

WHEN THE ENGINEER DETERMINES THAT THE CONTRACTOR'S OPERATIONS OR PLACEMENT OF TRAFFIC CONTROL DEVICES HAS CAUSED A SITUATION THAT THE VISIBILITY OF IS REDUCED ENOUGH TO CREATE A HAZARD, THE TRAFFIC CONTROL DEVICES SHALL BE DELINEATED WITH FLAGS OR OTHER ENGINEER-APPROVED DEVICES AT NO ADDITIONAL COST TO THE PROJECT.

MINIMIZE DISRUPTION TO PEDESTRIANS TO THE MAXIMUM EXTENT FEASIBLE BY PROVIDING AN APR IN THE FOLLOWING ORDER OF PREFERENCE:

1. PROVIDE THE APR ON THE SAME SIDE OF THE STREET AS THE DISRUPTED ROUTE UTILIZING BYPASSES.
2. WHERE IT IS NOT FEASIBLE TO PROVIDE A SAME SIDE APR, PROVIDE A DETOUR ON THE OTHER SIDE OF THE STREET.
3. WHERE IT IS NOT FEASIBLE TO PROVIDE AN APR ON THE OTHER SIDE OF THE ROADWAY, PROVIDE AN APR DETOUR WITH TRAILBLAZING SIGNS AS SHOWN ON THE PROJECT PLANS.

**SPECIFIC NOTES**

1. TEMPORARY CURB RAMPS WITH DETECTABLE WARNINGS.
2. TEMPORARY PAVEMENT MARKING FOR CROSSWALK LINES.
3. AN APPROVED AUDIBLE MESSAGE DEVICE OR TACTILE MESSAGE SHALL BE PROVIDED FOR SIGHT-IMPAIRED PEDESTRIANS.
4. THE INTERNATIONAL SYMBOL OF ACCESSIBILITY SHOULD BE DISPLAYED WHEN ANY WALKWAY THROUGH A WORK ZONE HAS BEEN DETERMINED TO BE TPAR COMPLIANT. THE SYMBOL OF ACCESSIBILITY SHALL NOT BE DISPLAYED IF PERSONS WITH DISABILITIES SHOULD NOT USE THE PRIMARY TEMPORARY PEDESTRIAN DETOUR. THE REASON FOR THE NON-COMPLIANCE SHALL BE POSTED AND AN ALTERNATE ROUTE SHALL BE POSTED WHEN THE PRIMARY TEMPORARY PEDESTRIAN DETOUR IS NON-COMPLIANT TO TPAR STANDARDS.
5. TYPICAL SIGN MESSAGE FOR A TEMPORARY PEDESTRIAN DETOUR SHALL INCLUDE INFORMATION SUCH AS THE DURATION OF THE WALKWAY RESTRICTIONS (BEGINNING AND/OR END DATES) AND A PROJECT CONTACT NUMBER FOR 24 / 7 QUESTIONS OR REPORTING HAZARDS.
6. PEDESTRIAN DETOUR TRAILBLAZING SIGNS SHALL BE USED IF THE PEDESTRIAN DETOUR IS IN A LOCATION OTHER THAN ACROSS THE STREET FROM THE SIDEWALK CLOSURE.

**PEDESTRIAN TEMPORARY TRAFFIC CONTROL NOTES**

1. THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN THROUGH MOVEMENTS FROM ONE END OF THE CONSTRUCTION AREA TO THE OTHER, ON AT LEAST ONE SIDE OF THE STREET DURING CONSTRUCTION. ANY SIDEWALK CLOSURES SHALL MEET THE REQUIREMENTS OF THE MMUTCD, PART 6.
2. PEDESTRIAN ACCESS SHALL BE PROVIDED TO ALL ADJACENT PROPERTIES, BUILDINGS, RESIDENCES AND COMMERCIAL PROPERTIES AT ALL TIMES. THIS MAY INCLUDE TEMPORARY WALKWAYS SPANNING THE CONSTRUCTION AREA.
3. IF SIDEWALKS ARE CLOSED, A TEMPORARY PEDESTRIAN ACCESS ROUTE (TPAR) SHALL BE PROVIDED ON THE SAME SIDE OF THE ROAD AS THE CLOSED SIDEWALK, IF POSSIBLE. SIGNS AND BARRICADES SHALL BE USED TO PROVIDE ADVANCE NOTICE OF THE CLOSURE AND THE ROUTE OF ANY PEDESTRIAN DETOURS. THE TPAR SHALL HAVE A MINIMUM UNOBSTRUCTED WIDTH OF 4 FEET. IF THE TPAR IS LESS THAN 5 FEET IN WIDTH, A 5 FOOT BY 5 FOOT PASSING SPACE SHALL BE PROVIDED AT LEAST EVERY 200 FEET. THE SURFACE OF THE TPAR SHALL BE SMOOTH AND CONTINUOUS FOR THE LENGTH OF THE TPAR. THE TPAR SHALL MAINTAIN THE SAME LEVEL OF ACCESSIBILITY AND DETECTABILITY AS THE FACILITY THAT IS BEING CLOSED. THE TPAR SHALL NOT LEAD PEDESTRIANS INTO CONFLICTS WITH VEHICLES, EQUIPMENT, OR CONSTRUCTION OPERATIONS.
4. IF THE TPAR IS ADJACENT TO MOVING TRAFFIC, CONSTRUCTION OPERATIONS/EQUIPMENT, OR DROP-OFFS, THEN CRASH WORTHY CHANNELIZING DEVICES THAT MEET THE REQUIREMENTS OF NCHRP 350 AND THE MMUTCD SHALL BE USED.
5. THE CONTRACTOR SHALL NOT STORE OR PLACE ANY CONSTRUCTION MATERIALS, EQUIPMENT OR SIGNS IN THE PEDESTRIAN PATH OF TRAVEL.
6. THE CONTRACTOR'S OPERATIONS SHALL NOT OCCUPY SIDEWALKS EXCEPT WHERE PROPER PROTECTION AND A TPAR HAVE BEEN PROVIDED.
7. WHEN DIRECTED BY THE ENGINEER, OR STATED ON THE PLANS, THE CONTRACTOR SHALL PROVIDE A TEMPORARY PEDESTRIAN TRAFFIC CONTROL PLAN FOR REVIEW AND WRITTEN APPROVAL BY THE ENGINEER A MINIMUM OF THREE WEEKS BEFORE SUCH PLAN IS IMPLEMENTED. THIS PLAN SHALL DETAIL THE CONSTRUCTION PHASING AND SCHEDULE AND THE SPECIFIC METHODS OF MAINTAINING SAFE PEDESTRIAN ACCESS THROUGHOUT THE CONSTRUCTION AREA. THIS PLAN SHALL PROVIDE THE LOCATION AND DETAILS OF TEMPORARY CONSTRUCTION SIGNING, MARKINGS, BARRICADES, CHANNELIZING DEVICES, TPARS AND METHODS TO MAINTAIN ACCESS TO ADJACENT PROPERTIES, BUSINESSES, RESIDENCES, ETC. NO WORK SHALL BE ALLOWED TO BEGIN UNTIL THIS PLAN IS APPROVED BY THE ENGINEER IN WRITING.

**LEGEND**

- SIGN
- EXISTING PEDESTRIAN SURFACE
- WORK AREA
- PEDESTRIAN CHANNELIZATION DEVICE
- BARRIER
- SIDEWALK BARRICADE
- DIRECTION OF TRAFFIC
- TRAFFIC CONTROL DEVICE



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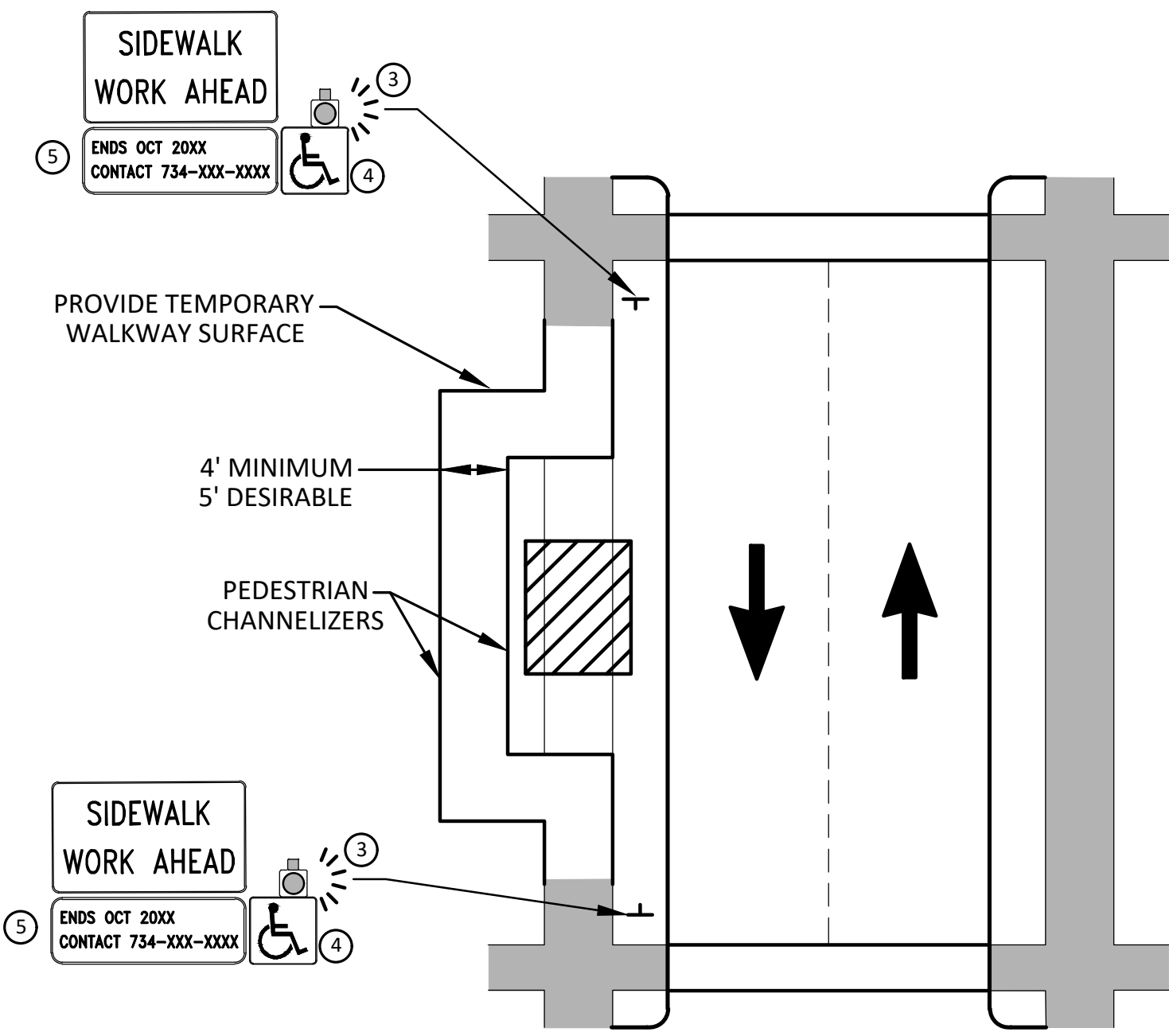
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**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
ALTERNATE PEDESTRIAN ROUTE (APR) DETOUR

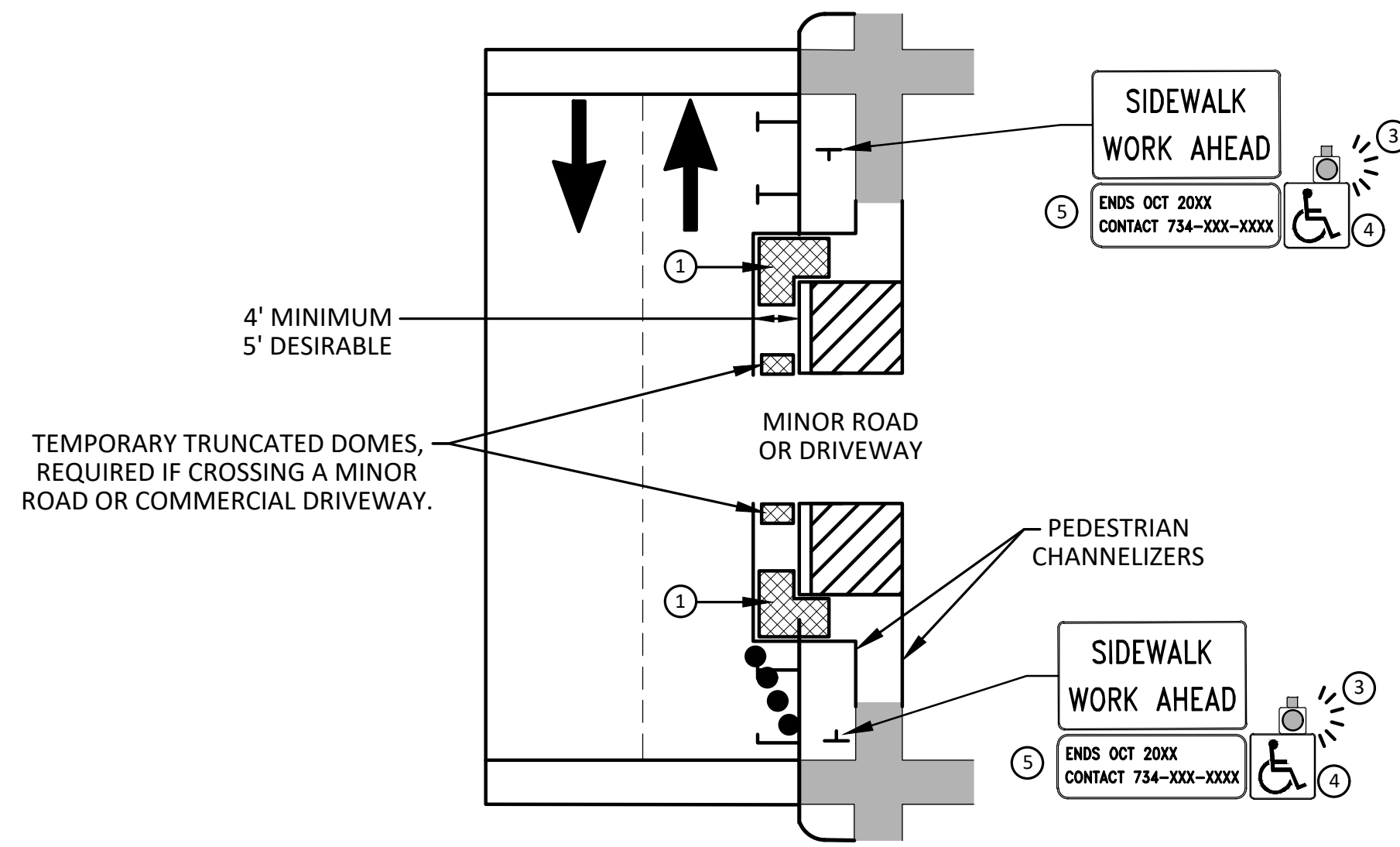
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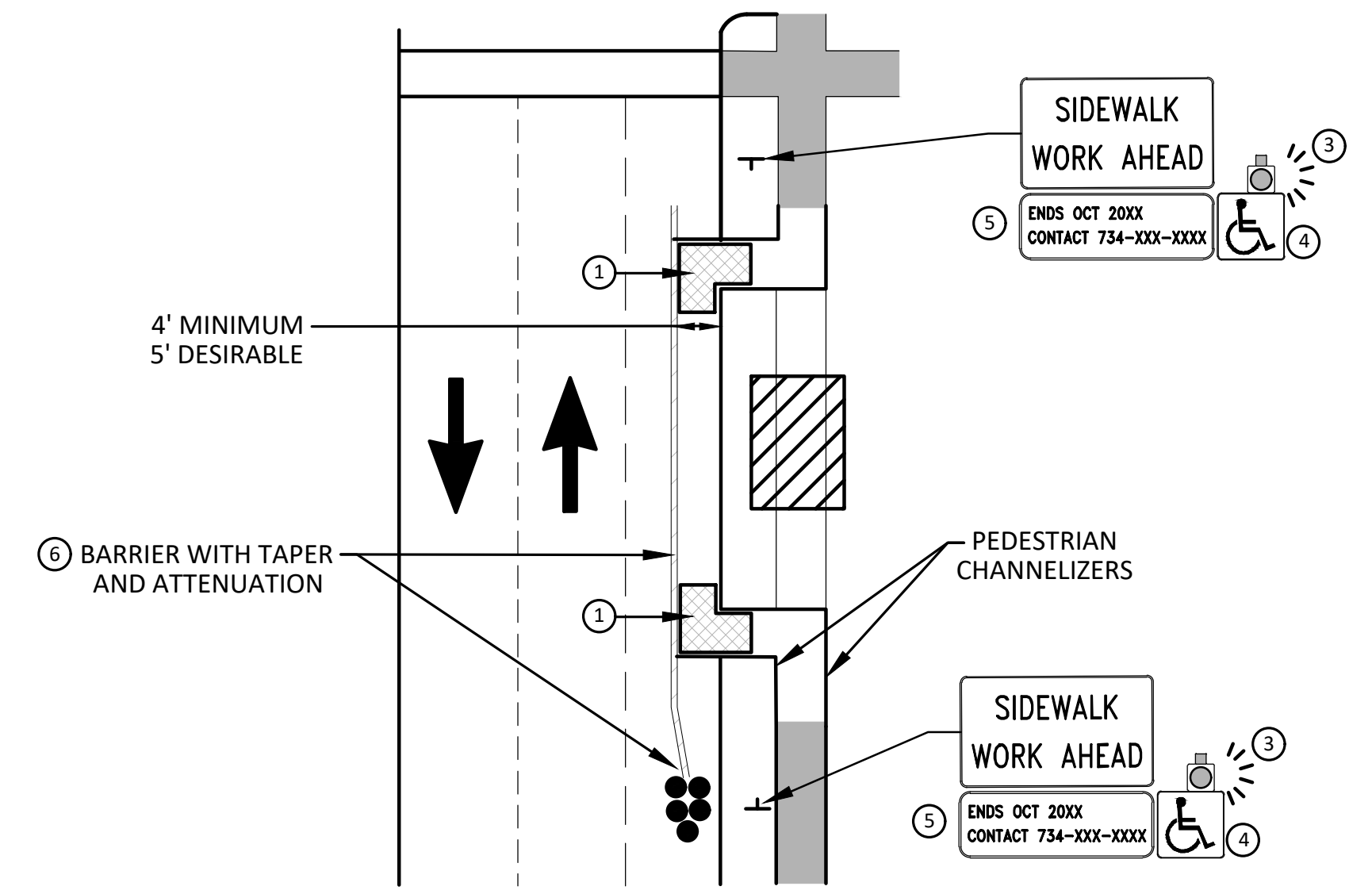


**BYPASS ON ADJACENT AVAILABLE  
RIGHT OF WAY  
BYPASS TYPE A**

NOTE: MAY ONLY BE USED ON ROADWAY WITH POSTED  
SPEED OF 45 MPH OR LESS.



**SIDEWALK BYPASS USING PARKING OR  
SHOULDER ON LOW SPEED ROADWAY  
BYPASS TYPE B**



**SIDEWALK BYPASS USING  
SHOULDER OR PARKING LANE ON  
HIGH SPEED ROADWAY  
BYPASS TYPE C**

**GENERAL NOTES**

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PROVIDE A SMOOTH, CONTINUOUS, HARD SURFACE THROUGH THE LENGTH OF THE APR. COMPACTED GRAVEL, AGGREGATE, OR SLAG MATERIALS ARE NOT ALLOWED. PROVIDE A FIRM, STABLE, AND SLIP RESISTANT TEMPORARY WALKWAY SURFACE TO COVER SHORT SEGMENTS OF ROUGH, SOFT, OR UNEVEN GROUND.

THE PEDESTRIAN TRAFFIC SIGNALS CONTROLLING CLOSED CROSSWALKS SHALL BE COVERED OR DEACTIVATED BY THE CITY OF ANN ARBOR. THE CONTRACTOR SHALL SCHEDULE AND COORDINATE THIS WORK WITH THE ENGINEER A MINIMUM OF 72 HOURS (NOT INCLUDING WEEKENDS & HOLIDAYS) PRIOR TO THE BEGINNING OF WORK THAT REQUIRES A SIDEWALK CLOSURE.

POST MOUNTED SIGNS LOCATED ADJACENT TO A SIDEWALK SHALL HAVE A 7 FOOT MINIMUM CLEARANCE FROM THE BOTTOM OF THE SIGN TO THE SIDEWALK SURFACE.

WHEN THE ENGINEER DETERMINES THAT THE CONTRACTOR'S OPERATIONS OR PLACEMENT OF TRAFFIC CONTROL DEVICES HAS CAUSED A SITUATION THAT THE VISIBILITY OF A TRAFFIC CONTROL DEVICE IS REDUCED ENOUGH TO CREATE A HAZARD, THE TRAFFIC CONTROL DEVICES SHALL BE DELINEATED WITH FLAGS OR OTHER ENGINEER-APPROVED DEVICES AT NO ADDITIONAL COST TO THE PROJECT.

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1. PROVIDE THE APR ON THE SAME SIDE OF THE STREET AS THE DISRUPTED ROUTE UTILIZING BYPASSES.
2. WHERE IT IS NOT FEASIBLE TO PROVIDE A SAME SIDE APR, PROVIDE A DETOUR ON THE OTHER SIDE OF THE STREET.
3. WHERE IT IS NOT FEASIBLE TO PROVIDE AN APR ON THE OTHER SIDE OF THE ROADWAY, PROVIDE AN APR DETOUR WITH TRAILBLAZING SIGNS AS SHOWN ON THE PROJECT PLANS.

**SPECIFIC NOTES**

1. TEMPORARY CURB RAMP WITH DETECTABLE WARNINGS.
2. S DEVICE TAPER 25 FEET LONG, RECOMMENDED WHEN THE CLOSED AREA WAS USED AS AN INTERMITTENT TRAFFIC LANE OR BYPASS LANE. STREET PARKING SHALL BE PROHIBITED FOR AT LEAST 50 FEET IN ADVANCE OF THE MID-BLOCK CROSSWALK.
3. AN APPROVED AUDIBLE MESSAGE DEVICE OR TACTILE MESSAGE SHOULD BE PROVIDED FOR SIGHT-IMPAIRED PEDESTRIANS.
4. THE INTERNATIONAL SYMBOL OF ACCESSIBILITY SHALL BE DISPLAYED WHEN ANY WALKWAY THROUGH A WORK ZONE HAS BEEN DETERMINED TO BE TPAR COMPLIANT. THE SYMBOL OF ACCESSIBILITY SHALL NOT BE DISPLAYED IF PERSONS WITH DISABILITIES SHOULD NOT USE THE PRIMARY TEMPORARY PEDESTRIAN DETOUR. THE REASON FOR THE NON-COMPLIANCE SHALL BE POSTED AND AN ALTERNATE ROUTE SHALL BE POSTED WHEN THE PRIMARY TEMPORARY PEDESTRIAN DETOUR IS NON-COMPLIANT TO TPAR STANDARDS.
5. TYPICAL SIGN MESSAGE FOR A TEMPORARY PEDESTRIAN DETOUR SHALL INCLUDE INFORMATION SUCH AS THE DURATION OF THE WALKWAY RESTRICTIONS (BEGINNING AND/OR END DATES) AND A PROJECT CONTACT NUMBER FOR 24 / 7 QUESTIONS OR REPORTING HAZARDS.
6. SEE MMUTCD FOR GUIDANCE ON PLACEMENT AND USAGE OF BARRIER.

**LEGEND**

- SIGN
- EXISTING PEDESTRIAN SURFACE
- WORK AREA
- PEDESTRIAN CHANNELIZATION DEVICE
- BARRIER
- SIDEWALK BARRICADE
- DIRECTION OF TRAFFIC
- TRAFFIC CONTROL DEVICE



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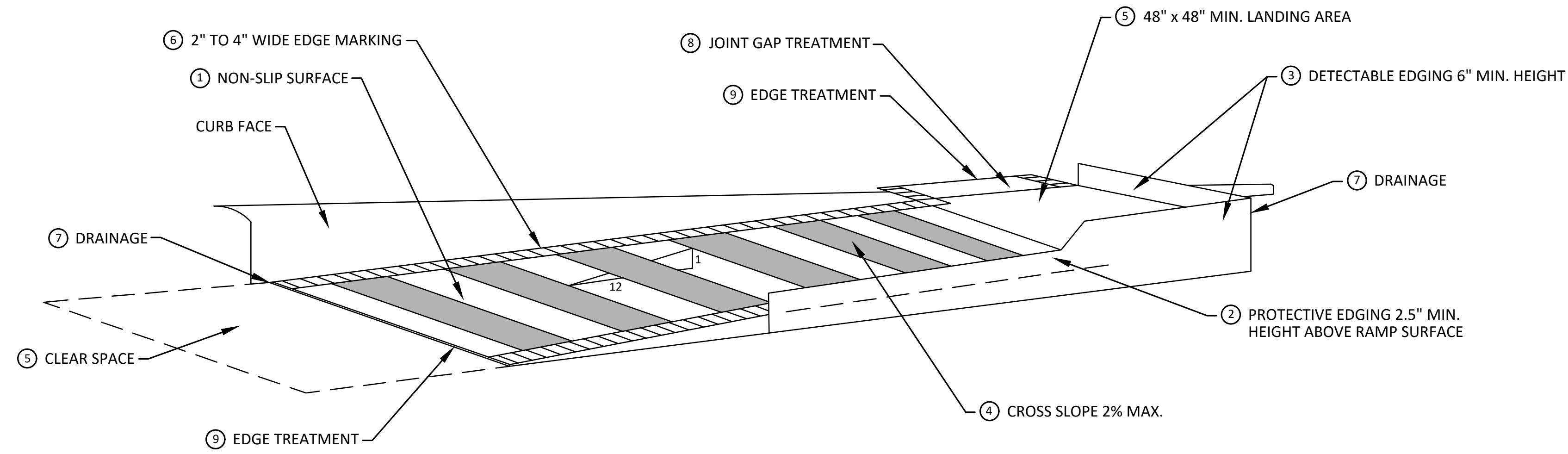
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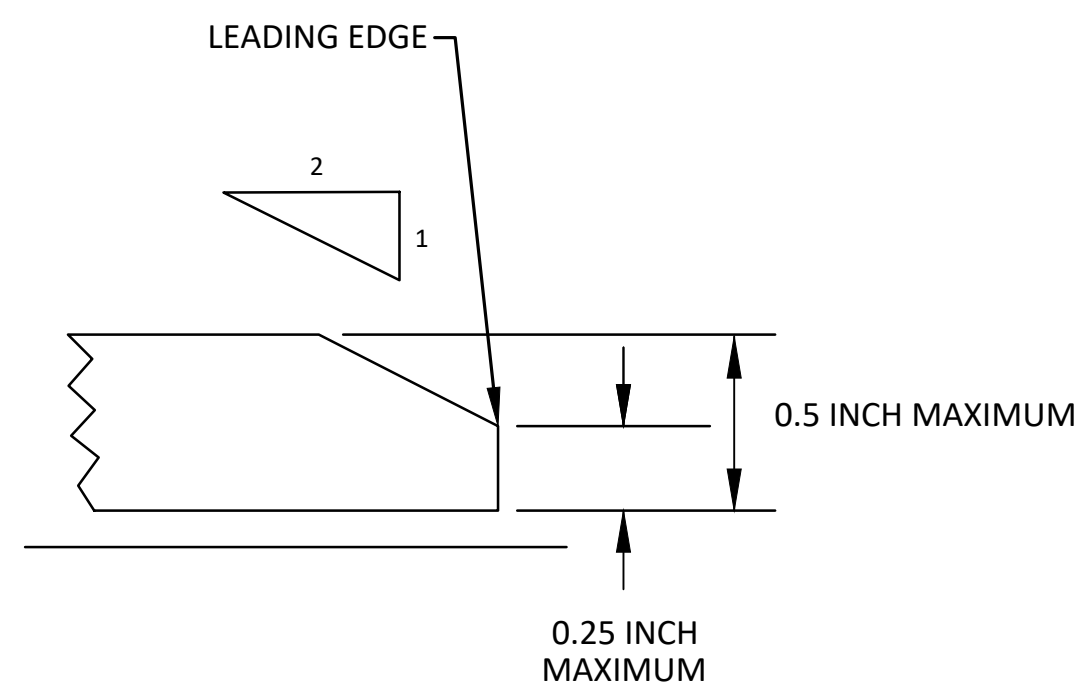
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**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
ALTERNATE PEDESTRIAN ROUTE (APR) BYPASS

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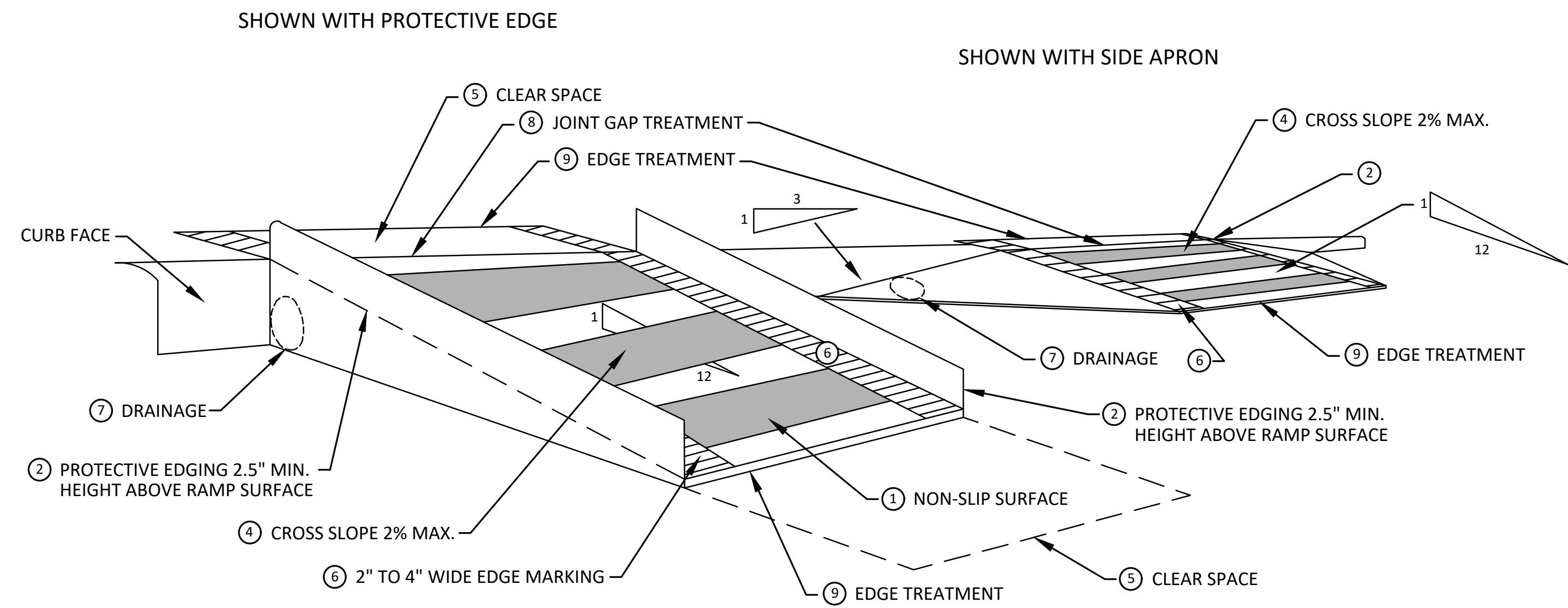
**TEMPORARY CURB RAMP  
PARALLEL TO CURB**



**9 EDGE TREATMENT**

**SPECIFIC NOTES**

- 1 CURB RAMPS SHALL BE 48" MIN. WIDTH WITH A FIRM, STABLE AND SLIP RESISTANT SURFACE. PROTECTIVE EDGING WITH A 2.5" MIN. HEIGHT ABOVE THE RAMP SHALL BE PLACED WHEN A CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6" OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3. PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3" OR MORE.
- 2 DETECTABLE EDGING ANYTIME A HANDRAIL IS REQUIRED, AND ANYTIME THE PATH CHANGES DIRECTION. THIS INCLUDES A TURN ONTO THE RAMP FROM THE PATH. DETECTABLE EDGING MUST BEGIN A MAXIMUM OF 2.5" ABOVE THE RAMP SURFACE, AND EXTEND AT LEAST 6" ABOVE THE RAMP SURFACE. CONTRASTING COLOR SHALL BE PLACED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
- 3 CURB RAMPS AND LANDINGS SHALL HAVE A 2% MAX. CROSS SLOPE.
- 4 CLEAR SPACE OF 48" x 48" MIN. SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
- 5 THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A CONTRASTING COLOR, 2" TO 4" WIDE MARKING. THE MARKING IS OPTIONAL WHERE COLOR CONTRASTING EDGING IS USED.
- 6 WATER FLOW IN THE GUTTER SYSTEM SHALL NOT BE IMPEDED.
- 7 LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 1/2" WIDTH.
- 8 CHANGES BETWEEN SURFACE HEIGHTS SHALL NOT EXCEED 1/2". LATERAL EDGES SHOULD BE VERTICAL UP TO 1/4" HIGH, AND BEVELED AT 1:2 BETWEEN 1/4" AND 1/2" HEIGHT.



**TEMPORARY CURB RAMP  
PERPENDICULAR TO CURB**



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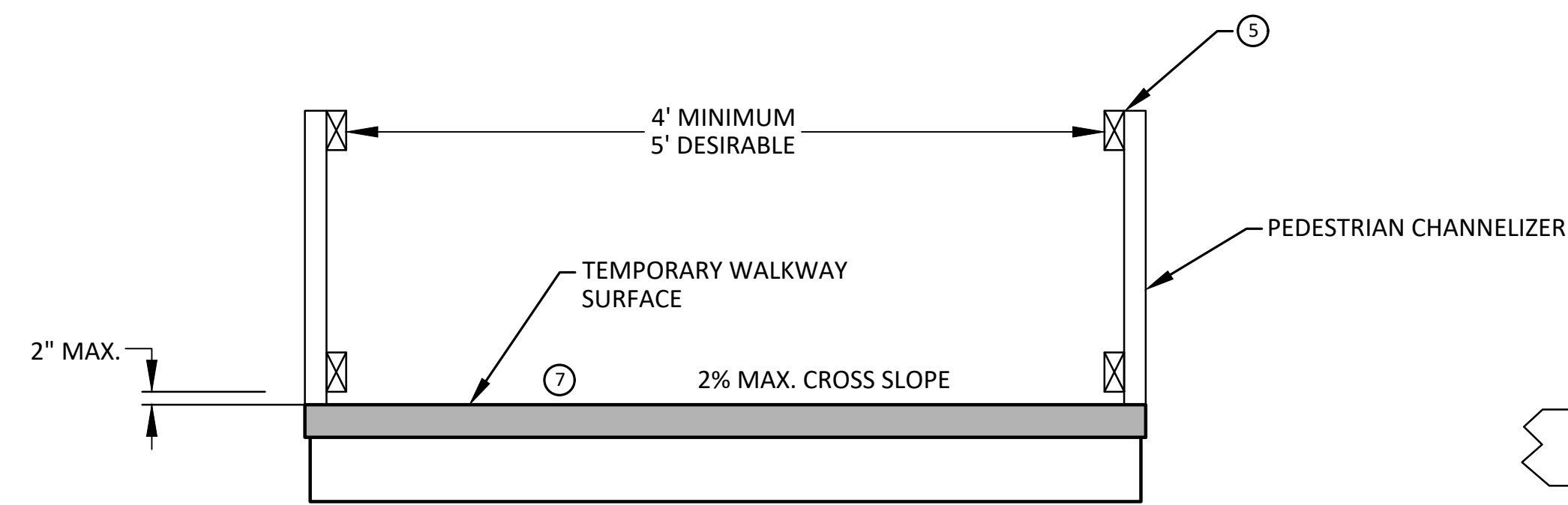
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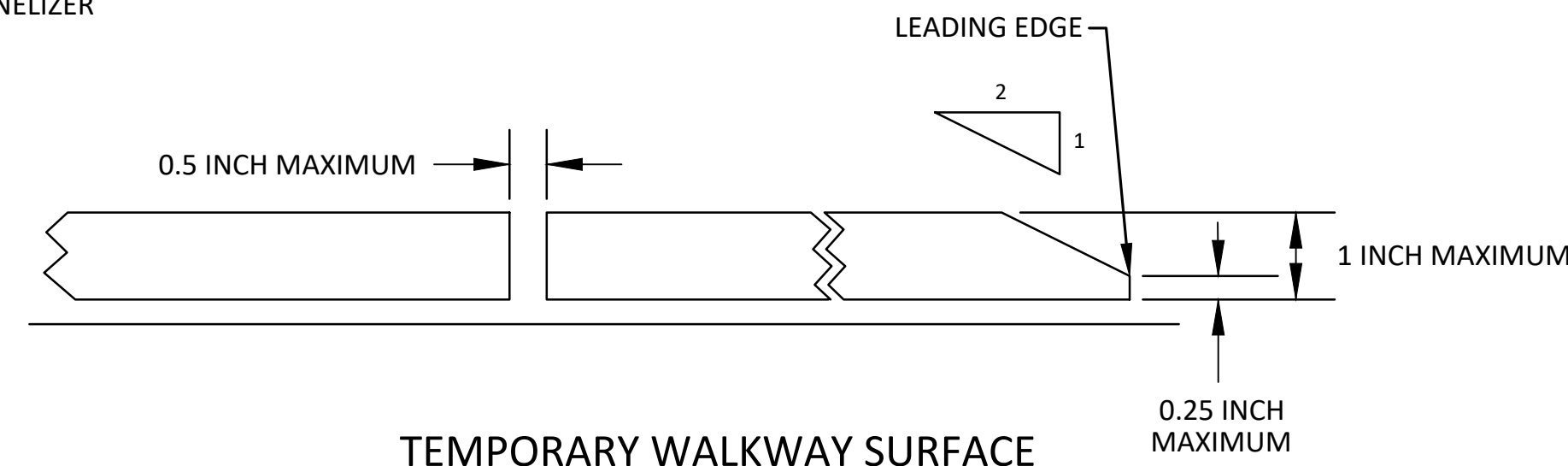
**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
TPAR RAMPS

SCALE: N.T.S.  
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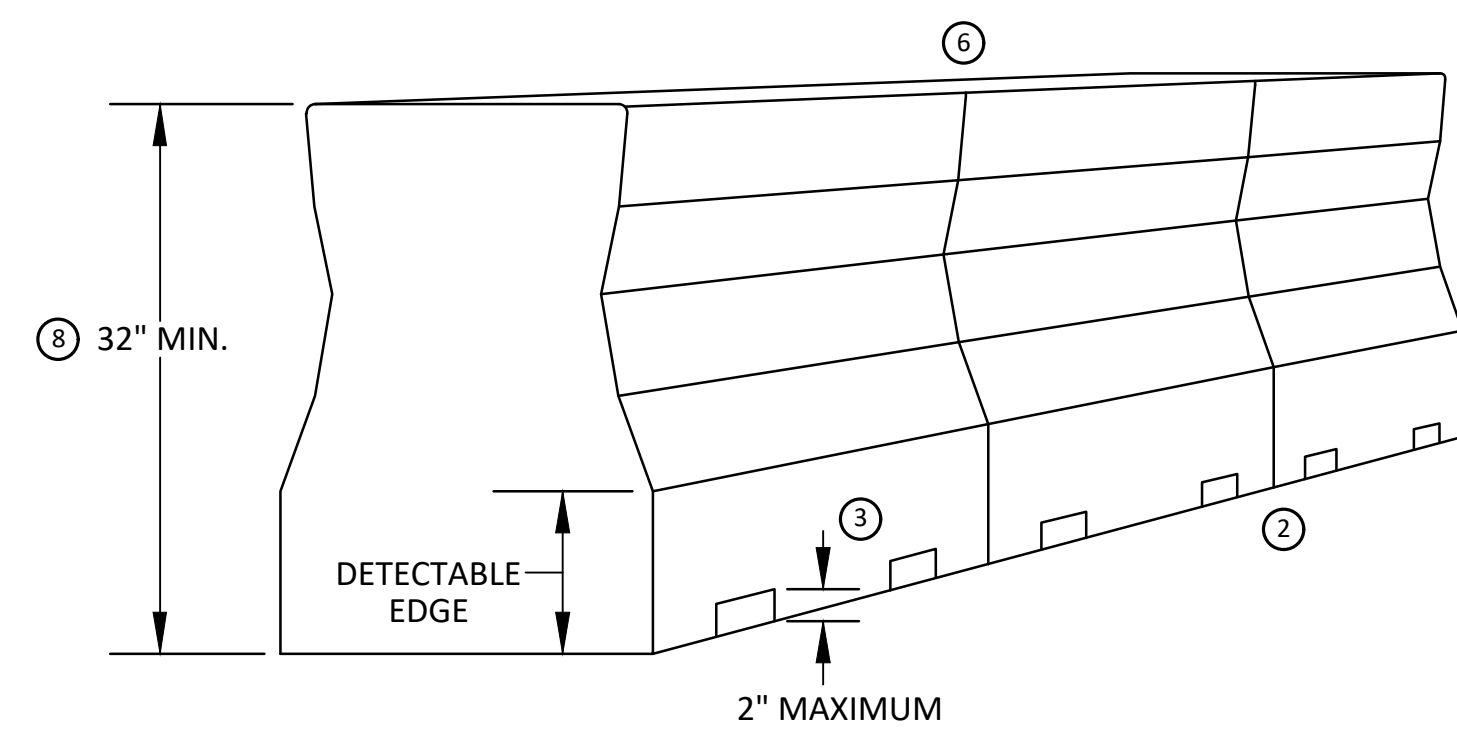
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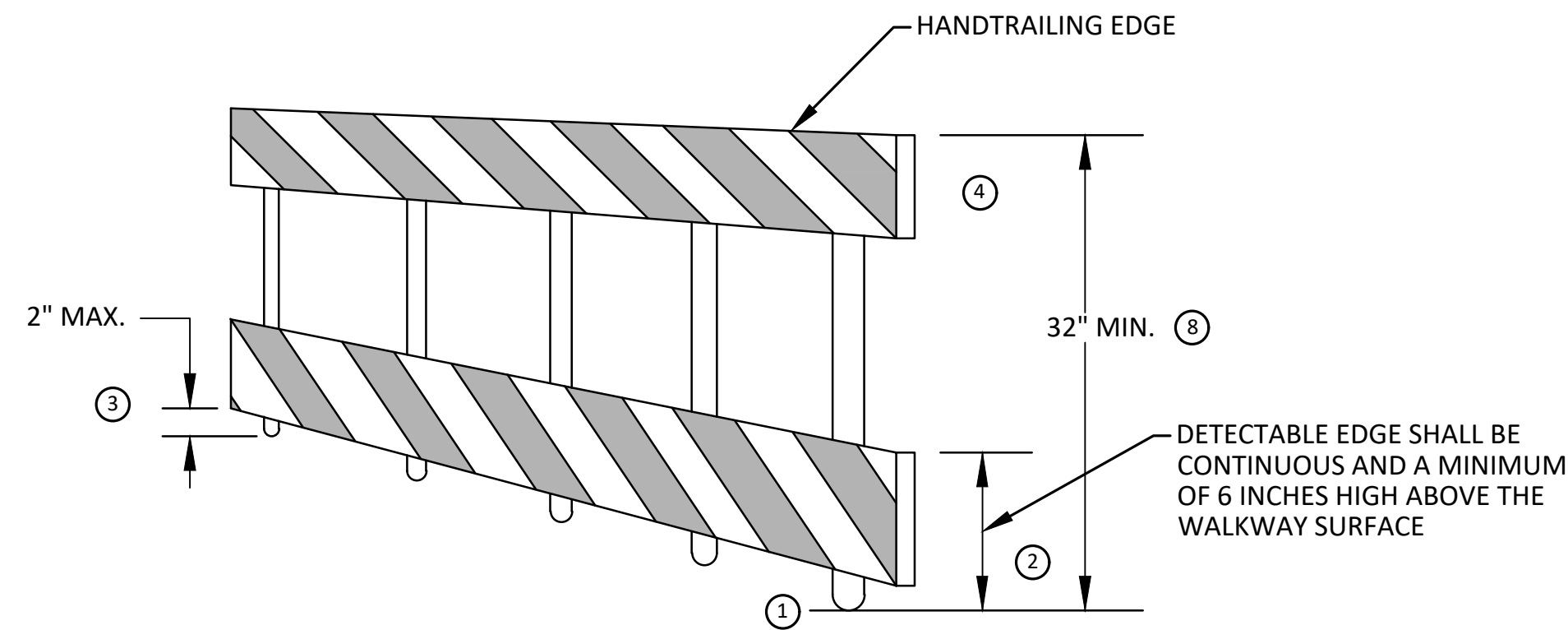
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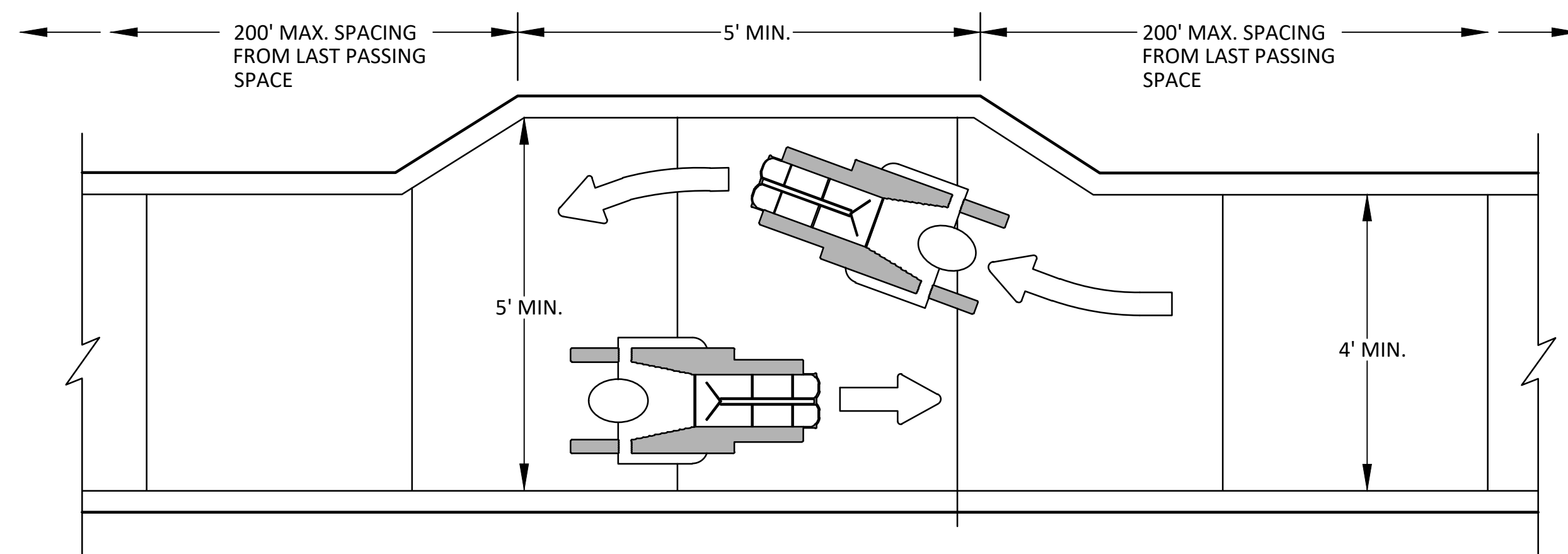
TEMPORARY WALKWAY SURFACE



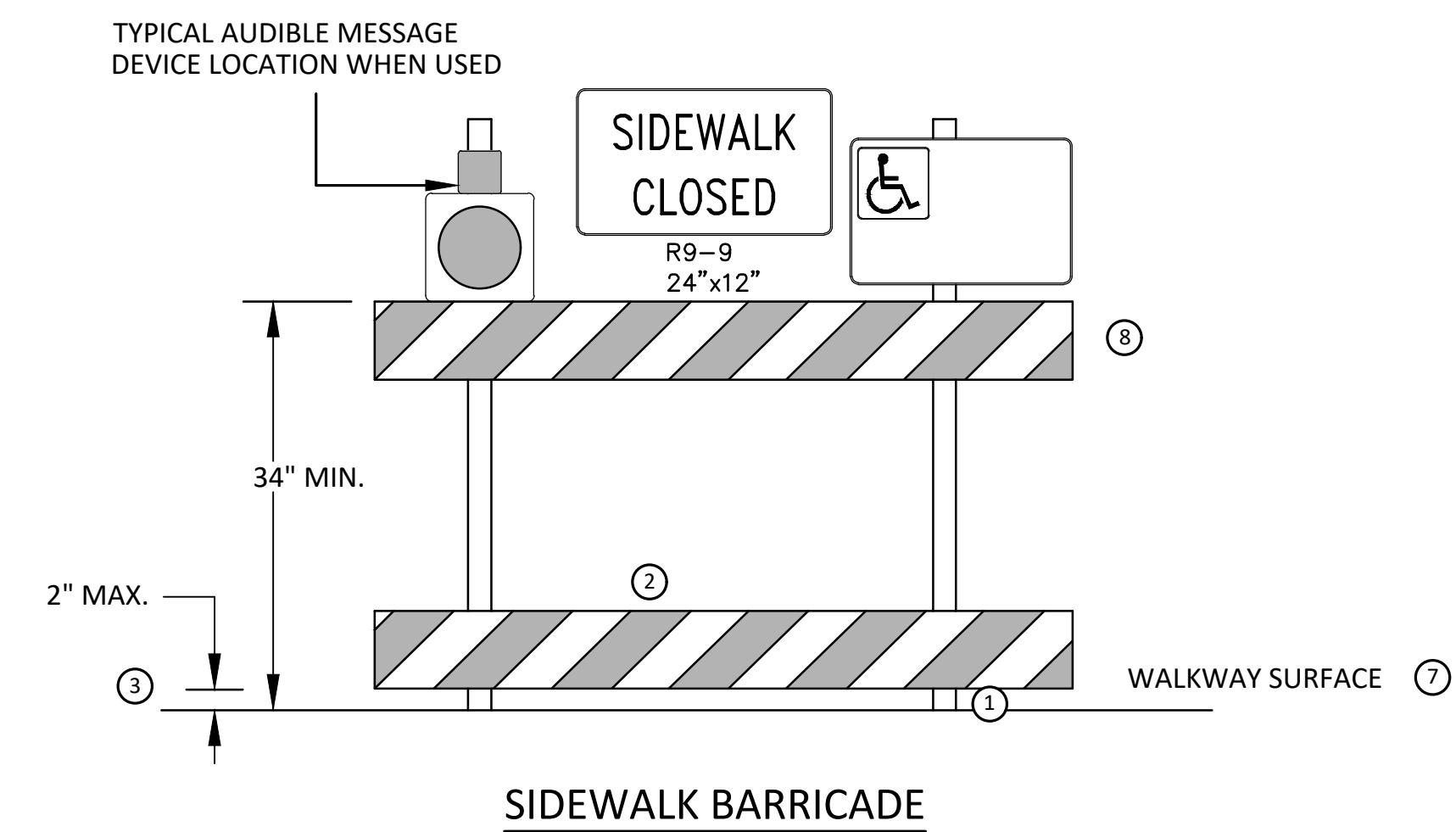
PEDESTRIAN CHANNELIZER USING A BARRIER  
(MINIMUM REQUIREMENTS)



PEDESTRIAN CHANNELIZER  
(MINIMUM REQUIREMENTS)



NARROW TEMPORARY PEDESTRIAN ACCESS ROUTE PASSING DETAIL



SIDEWALK BARRICADE

**GENERAL NOTES**

RAILINGS OR OTHER OBJECTS MAY PROTRUDE A MAXIMUM OF 4 INCHES INTO THE WALKWAY CLEAR SPACE WHEN LOCATED A MINIMUM OF 27 INCHES ABOVE THE WALKWAY SURFACE.

ANY PEDESTRIAN DEVICES USED TO PROVIDE POSITIVE PROTECTION FOR PEDESTRIANS OR WORKERS SHALL MEET NCHRP 350 CRASHWORTHY REQUIREMENTS APPROPRIATE FOR THE BARRIER'S APPLICATION.

BARRICADES SHALL BE PLACED CONTINUOUSLY ACROSS THE ENTIRE WIDTH OF THE WALKWAY SURFACE BEING CLOSED.

**SPECIFIC NOTES**

1 ANY TRIPPING HAZARD IN THE WALKWAY NEEDS A DETECTABLE EDGE. BALLAST SHALL BE LOCATED BEHIND OR INTERNAL TO THE DEVICE. ANY SUPPORT ON THE FRONT OF THE DEVICE SHALL NOT EXTEND INTO THE 48 INCH MINIMUM WALKWAY CLEAR SPACE AND SHALL NOT EXCEED 0.5 INCHES IN HEIGHT ABOVE THE WALKWAY SURFACE.

2 DETECTABLE EDGES SHALL BE CONTINUOUS AND A MINIMUM OF 6 INCHES IN HEIGHT ABOVE WALKWAY SURFACE AND HAVE COLOR MARKINGS CONTRASTING WITH THE WALKWAY SURFACE.

3 DEVICES SHALL NOT BLOCK WATER DRAINAGE FROM THE WALKWAY. A GAP HEIGHT OR OPENING FROM THE WALKWAY SURFACE UP TO A MAXIMUM OF 2 INCHES IS ALLOWED FOR DRAINAGE PURPOSES.

4 PROVIDE A HANDRAIL ON BOTH SIDES OF THE RAMP IF THE RAMP IS NOT EXPOSED TO VEHICLE TRAFFIC AND HAS A TOTAL RISE GREATER THAN 6 INCHES, AND A LENGTH GREATER THAN 72 INCHES.

- ENSURE THE HANDRAIL IS 1.25 AND 1.5 INCHES WIDE AND CONFIGURED TO BE A "GRASPABLE" CROSS-SECTION.

WHEN THE RAMP IS EXPOSED TO TRAFFIC, IN LIEU OF HANDRAILS, USE A PROTECTIVE EDGE 2.5 INCHES MINIMUM HEIGHT ABOVE THE RAMP SURFACE OR 1:10 FLARE ON BOTH SIDES OF THE RAMP.

5 ALL DEVICES SHALL BE FREE OF SHARP OR ROUGH EDGES, AND FASTENERS (BOLTS) SHALL BE ROUNDED TO PREVENT HARM TO HANDS, ARMS OR CLOTHING OF PEDESTRIANS.

6 ALL DEVICES USED TO CHANNELIZE PEDESTRIAN FLOW SHOULD INTERLOCK SUCH THAT GAPS DO NOT ALLOW PEDESTRIANS TO STRAY FROM THE INTENDED CHANNELIZED PATH.

7 A WALKWAY SURFACE SHALL BE FIRM, STABLE, AND SLIP RESISTANT. COMPACTED GRAVEL, AGGREGATE, OR SLAG MATERIALS ARE NOT ALLOWED.

8 LONGITUDINAL CHANNELIZING DEVICES FOR PEDESTRIANS SHALL BE 32 INCHES IN HEIGHT OR GREATER.



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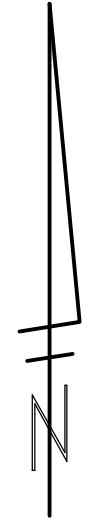
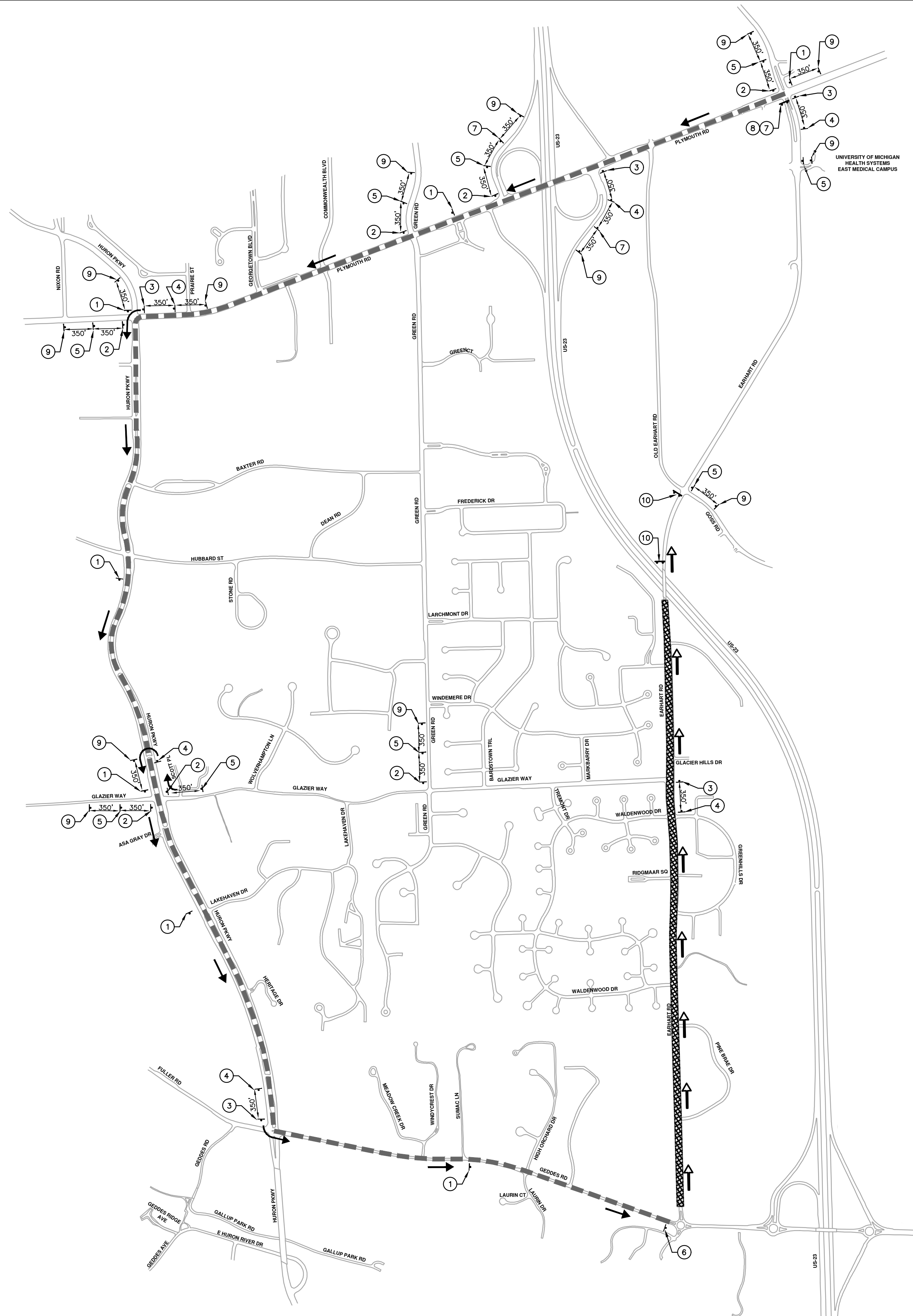
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CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING  
EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
TPAR WALKWAY DEVICES

SCALE: N.T.S.  
DRAWING No. 2021023-13

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**LEGEND:**

DETOUR ROUTE - SOUTH BOUND EARHART ROAD

NORTH BOUND EARHART ROAD

- DETOUR CONSTRUCTION NOTES:**
1. DEPENDING ON THE DETOUR ROUTE THAT IS PUT IN PLACE, CONFLICTING SIGNS MAY BE PRESENT. PRIOR TO THE ORDERING OR PLACEMENT OF ANY SIGNS, MEET WITH THE ENGINEER TO VERIFY THE EXACT NUMBER AND LOCATION OF THE SIGNS TO BE PLACED. THE ENGINEER AND THE CONTRACTOR SHALL WORK TOGETHER TO ELIMINATE ALL CONFLICTS.
  2. THE CITY RESERVES THE RIGHT TO ORDER ADDITIONAL SIGNS AND PLACE THEM INTO SERVICE IN ORDER TO SAFELY MAINTAIN TRAFFIC. ADJUSTMENTS IN THE UNIT PRICE FOR THESE ITEMS OF WORK SHALL NOT BE ALLOWED FOR THESE OCCURRENCES.
  3. THE CONTRACTOR SHALL COORDINATE HIS OPERATIONS WITH THOSE OF THE CITY OF ANN ARBOR'S SIGNS AND SIGNALS UNIT DURING THE COURSE OF THE DETOUR ROUTE ESTABLISHMENT TO EFFECTIVELY AND SAFELY MAINTAIN TRAFFIC. DO NOT ERECT ANY SIGNS UNTIL ITS LOCATION AND PROPOSED DATE OF INSTALLATION IS APPROVED BY THE ENGINEER.
  4. THE DETOUR IS TO BE IN PLACE DURING EARHART ROAD IMPROVEMENTS, AS DIRECTED BY THE ENGINEER. THE CONTRACTOR WILL CLOSE AND DETOUR SOUTH BOUND EARHART ROAD TRAFFIC. SEE "SPECIAL PROVISION FOR MAINTAINING TRAFFIC AND CONSTRUCTION SEQUENCING" FOR DETAILS ON CONSTRUCTION STAGING, SEQUENCING, CLOSURE AND DETOUR LIMITATIONS, AND OTHER DETAILS.
  5. REFERENCE THE "SPECIAL PROVISION FOR MAINTAINING TRAFFIC AND CONSTRUCTION SEQUENCING" FOR ADDITIONAL PROJECT REQUIREMENTS. THE CONTRACTOR'S ATTENTION IS SPECIFICALLY DIRECTED TO THE SECTION OF SAME SPECIAL PROVISION REGARDING COORDINATION WITH THE CITY SIGNS AND SIGNALS UNIT FOR MODIFICATIONS TO TRAFFIC SIGNALS FOR THE DETOUR.
  6. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN SIGNS AS SHOWN ON THE PLANS OR OTHERWISE DIRECTED BY THE ENGINEER.
  7. CONSTRUCTION WARNING SIGNS SHALL HAVE AN ORANGE, HIGH-INTENSITY, REFLECTORIZED BACKGROUND.
  8. SIGNS SHALL CONFORM TO THE 2011 MICHIGAN MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
  9. THE CONTRACTOR SHALL DRIVE ALL SIGNS INTO EXPOSED GROUND OR INSERT INTO A BORED HOLE IN PAVEMENT AS NECESSARY TO PERMANENTLY SECURE. ALL HOLES IN PAVEMENT SHALL BE FILLED WITH ENGINEER-APPROVED MORTAR WHEN THE SIGN IS REMOVED AND NO LONGER NEEDED. COSTS FOR THIS WORK SHALL BE INCLUDED IN THE PAY ITEM "TEMPORARY SIGN, TYPE B, FURNISH AND OPERATE"
  10. ADVANCE WARNING SIGNS SHALL BE PROVIDED WITH TWO (2) TYPE A FLASHING LIGHTS AND ONE (1) DAY-GLOW ORANGE FLAG.
  11. CITY TO BE NOTIFIED A MINIMUM OF 7 DAYS PRIOR TO IMPLEMENTING SOUTH BOUND EARHART ROAD DETOUR. MESSAGE BOARDS TO BE PLACED 7 DAYS PRIOR TO IMPLEMENTATION OF DETOUR.

SOUTH BOUND EARHART ROAD			
	SIGN	NUMBER	QUANTITY
1	SB EARHART ROAD DETOUR ↑	D3-1 M4-9S 30"x24"	7 7
2	SB EARHART ROAD DETOUR →	D3-1 M4-9R 30"x24"	7 7
3	SB EARHART ROAD DETOUR ←	D3-1 M4-9L 30"x24"	5 5
4	SB EARHART ROAD DETOUR ↙	D3-1 M4-9L (MOD) 30"x24"	6 6
5	SB EARHART ROAD DETOUR ↘	D3-1 M4-9R (MOD) 30"x24"	9 9
6	SB EARHART ROAD END DETOUR	D3-1 M4-8A 24"x18"	1 1
7	EARHART RD CLOSED AT US-23	SPECIAL 48x66	3
8	DETOUR →	M4-10R 48"x18"	1
9	DETOUR AHEAD	D3-1 W20-2 36"x36"	13 13
10	ROAD CLOSED	R11-2 48"x30"	2

BARRICADE, TYPE III, HIGH INTENSITY, DOUBLE SIDED, LIGHTED - 2 EACH

SIGN, PORTABLE, CHANGEABLE MESSAGE, NTCIP-COMPLIANT - 2 EACH

VERIFY EXACT QUANTITY AND LOCATION WITH ENGINEER PRIOR TO ORDERING

PRCMS MESSAGE PHASING	
PRIOR TO CONSTRUCTION	DURING CONSTRUCTION
EARHART ROAD CONSTRUCTION	SB GEDDES ROAD CLSD AT US-23
MONTH DATE *	FOLLOW POSTED DETOUR

NOTE: PRCMS LOCATIONS AND MESSAGES WILL BE AS DIRECTED BY THE ENGINEER. TO BE PLACED ONE WEEK PRIOR TO CONSTRUCTION.

\*THE CONTRACTOR SHALL PLACE THE APPROPRIATE DATE AS APPROVED BY THE ENGINEER.

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**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**

SCALE PLAN: NTS

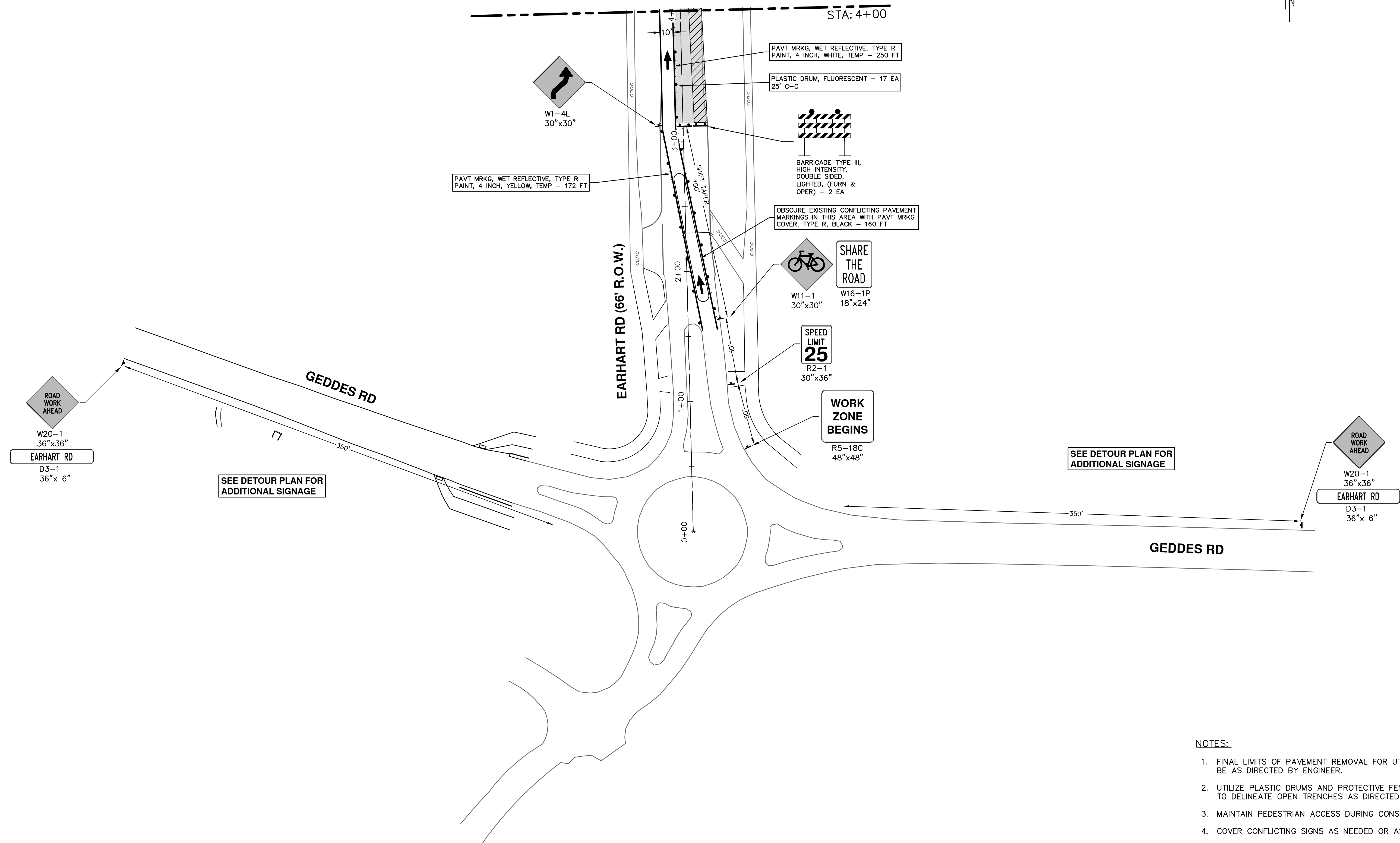
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SHEET No. 14 OF 74

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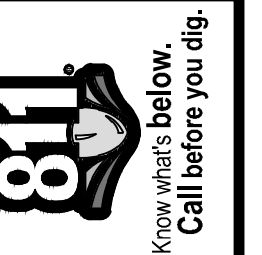
**DETOUR PLAN**

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**NOTES:**

1. FINAL LIMITS OF PAVEMENT REMOVAL FOR UTILITY INSTALLATION WILL BE AS DIRECTED BY ENGINEER.
2. UTILIZE PLASTIC DRUMS AND PROTECTIVE FENCING WITHIN WORK ZONE TO DELINEATE OPEN TRENCHES AS DIRECTED BY ENGINEER.
3. MAINTAIN PEDESTRIAN ACCESS DURING CONSTRUCTION.
4. COVER CONFLICTING SIGNS AS NEEDED OR AS DIRECTED BY ENGINEER.
5. REMOVE CONFLICTING PAVEMENT MARKINGS AS DIRECTED BY THE ENGINEER.
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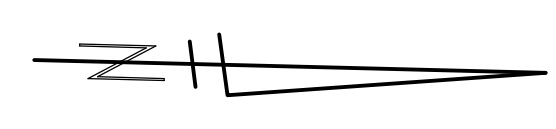
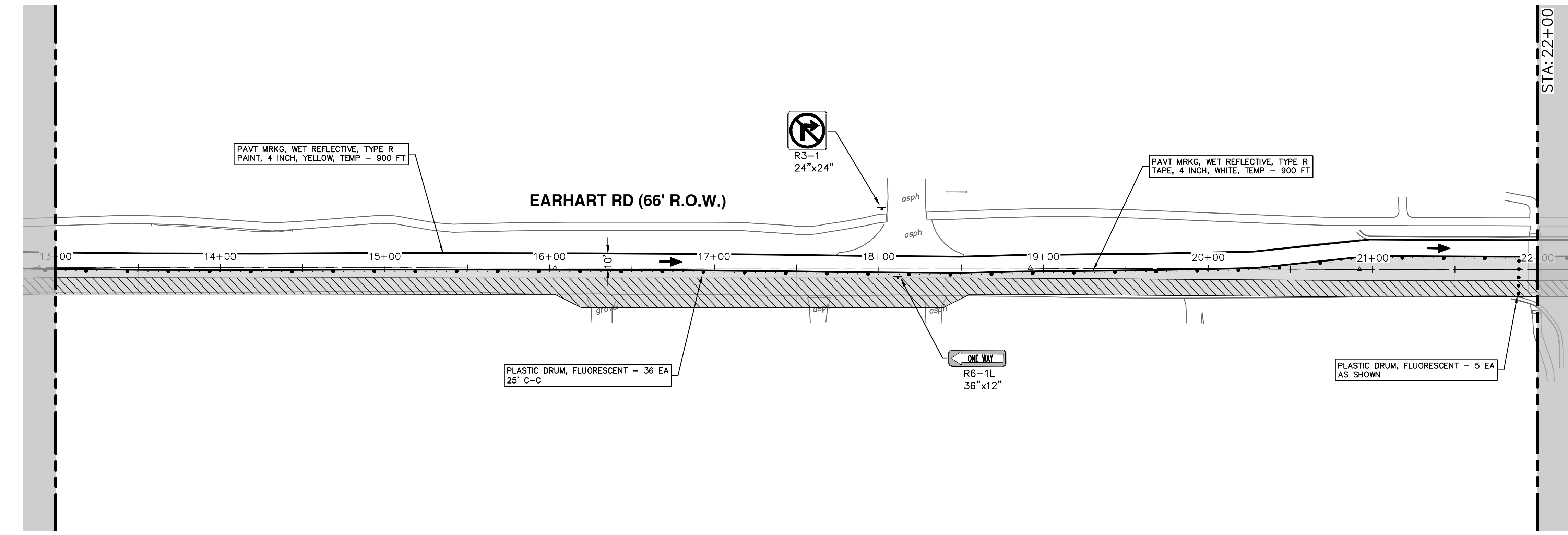
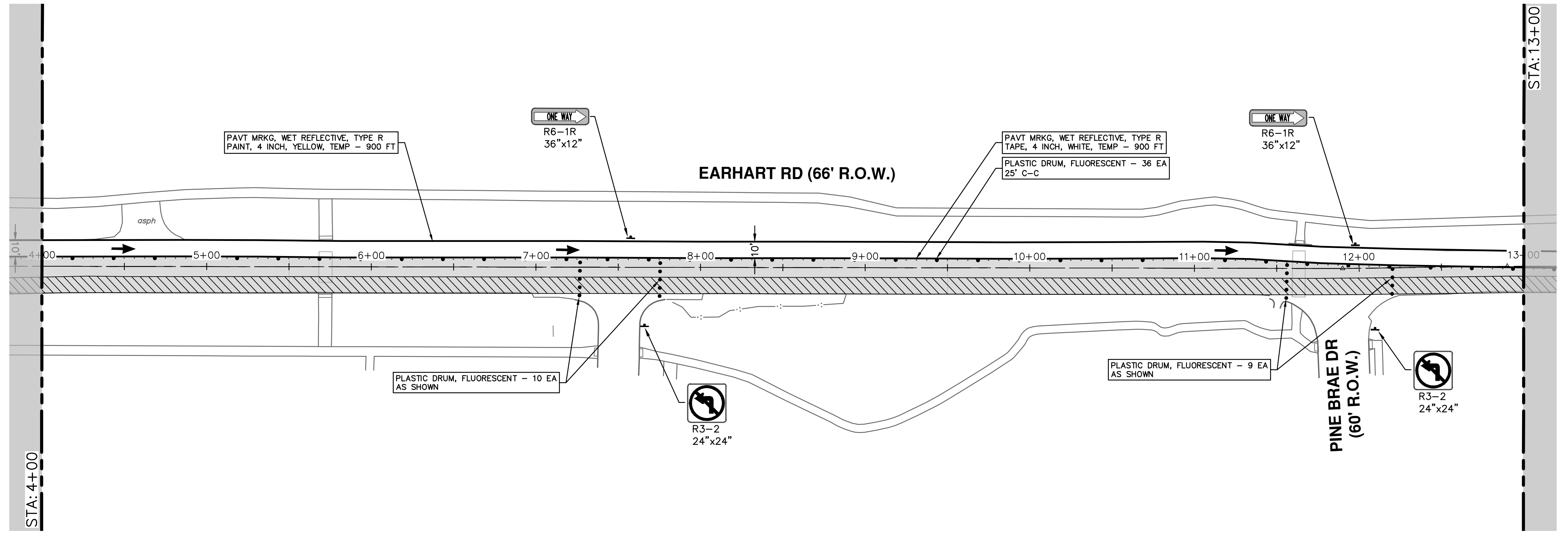
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**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
 MAINTENANCE OF TRAFFIC - PH I  
 STA. 0+00 STA. 4+00

SCALE: 1" = 40'  
 DRAWING No. 2021023-15  
 SHEET No. 15 OF 74

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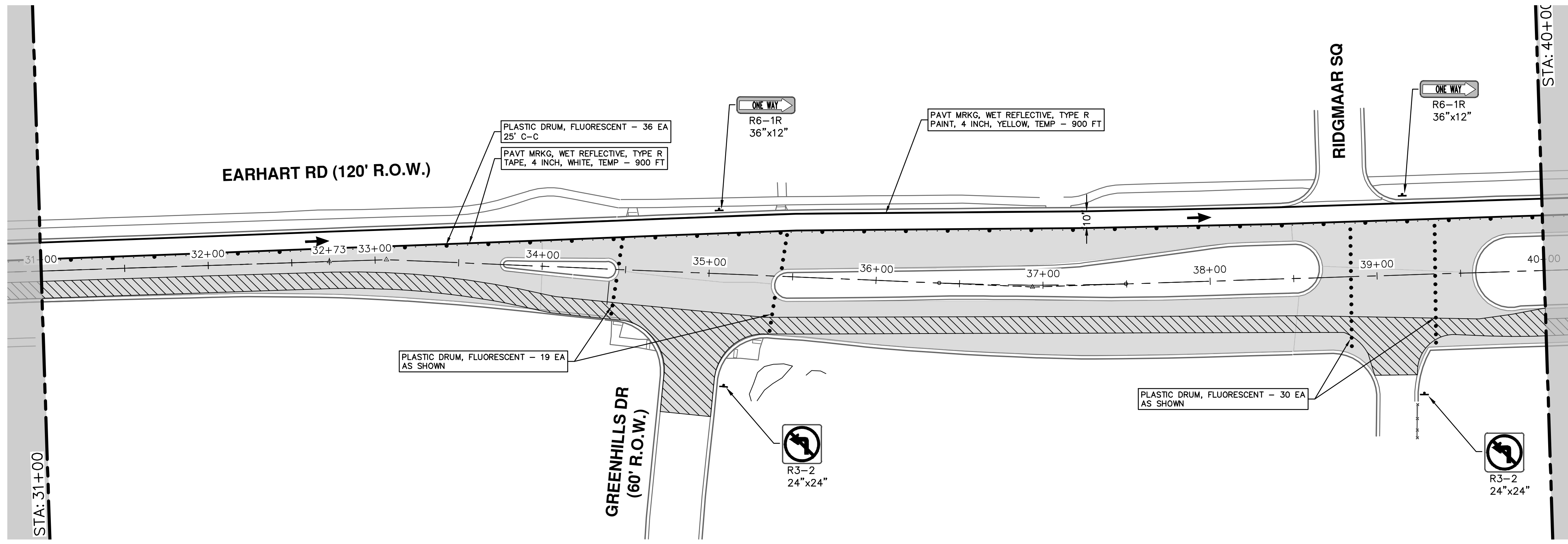
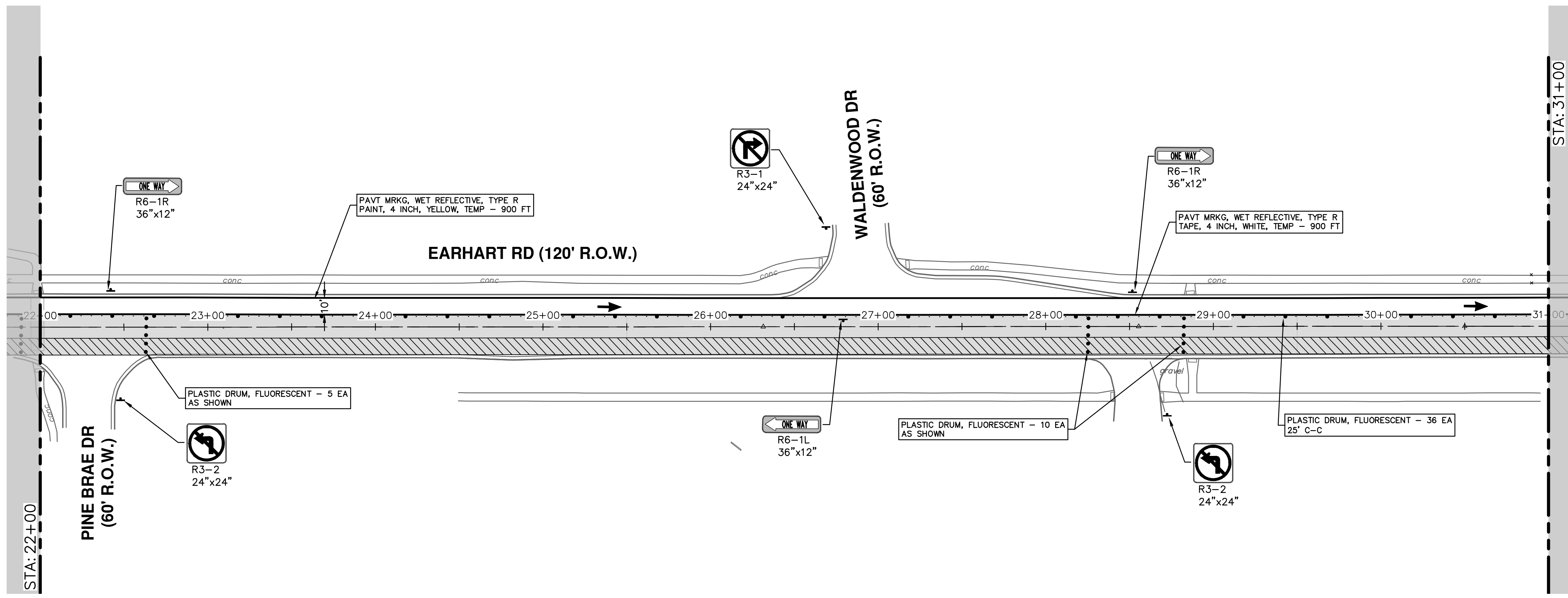
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**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
 MAINTENANCE OF TRAFFIC - PH I  
 STA. 4+00 - STA. 22+00  
 SCALE: 1" = 40'  
 DRAWING No. 2021023-16  
 SHEET No. 16 OF 74

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17 OF 74

SCALE: 1" = 40'  
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**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
 MAINTENANCE OF TRAFFIC - PH I  
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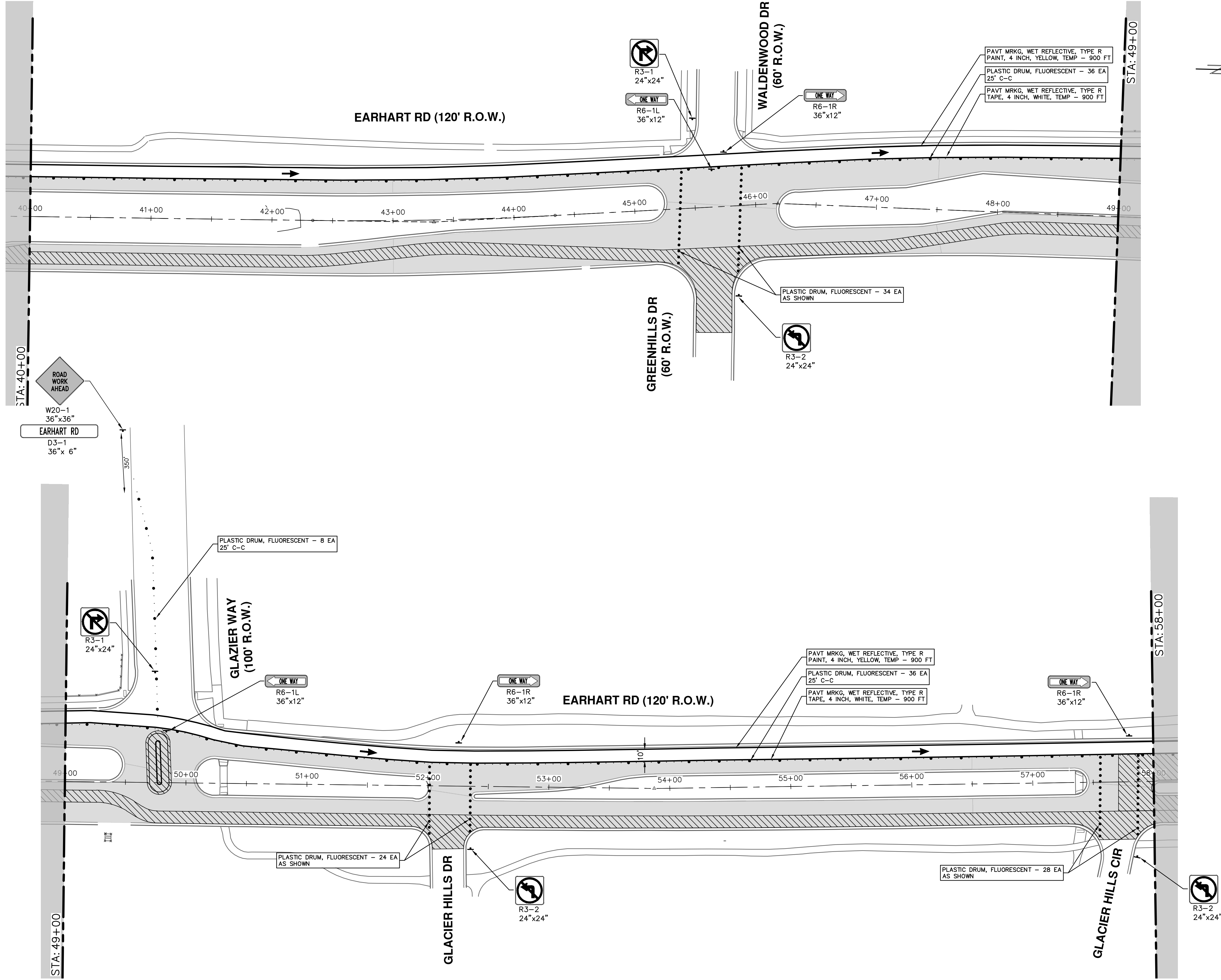


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**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**

MAINTENANCE OF TRAFFIC - PH I

SCALE: 1" = 40'

DRAWING No. 2021023-1B

SHEET No. 18 OF 74

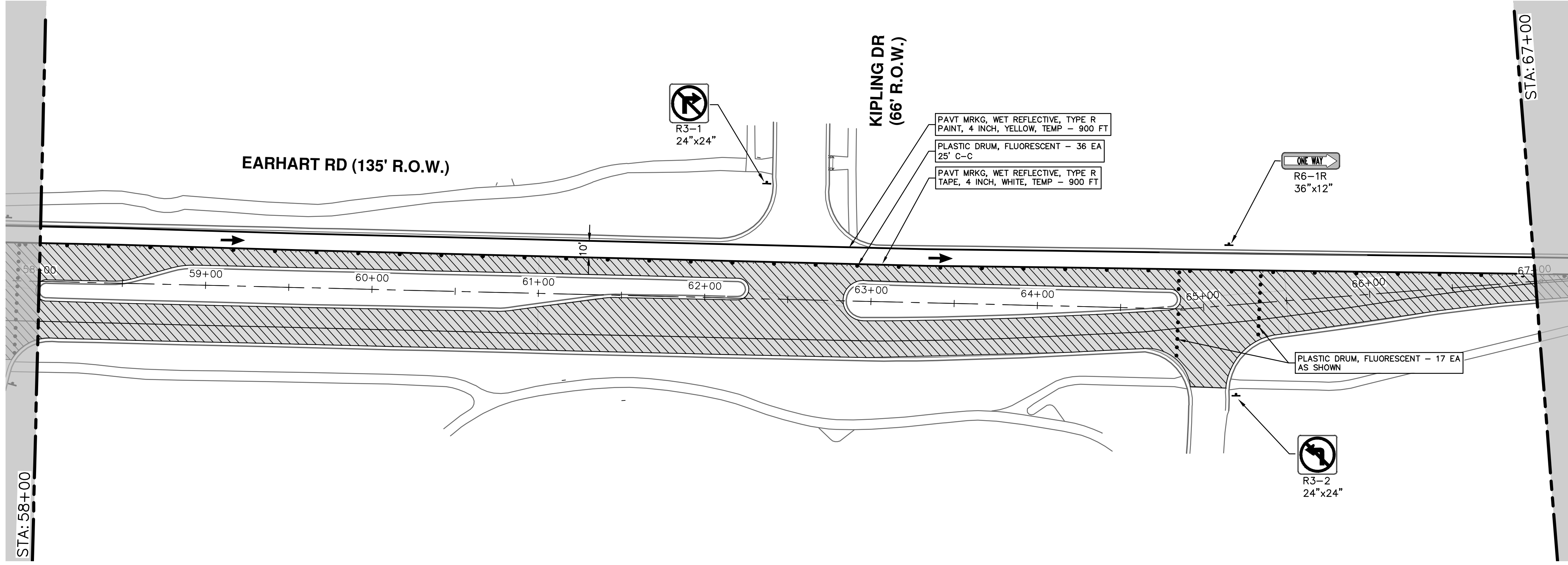
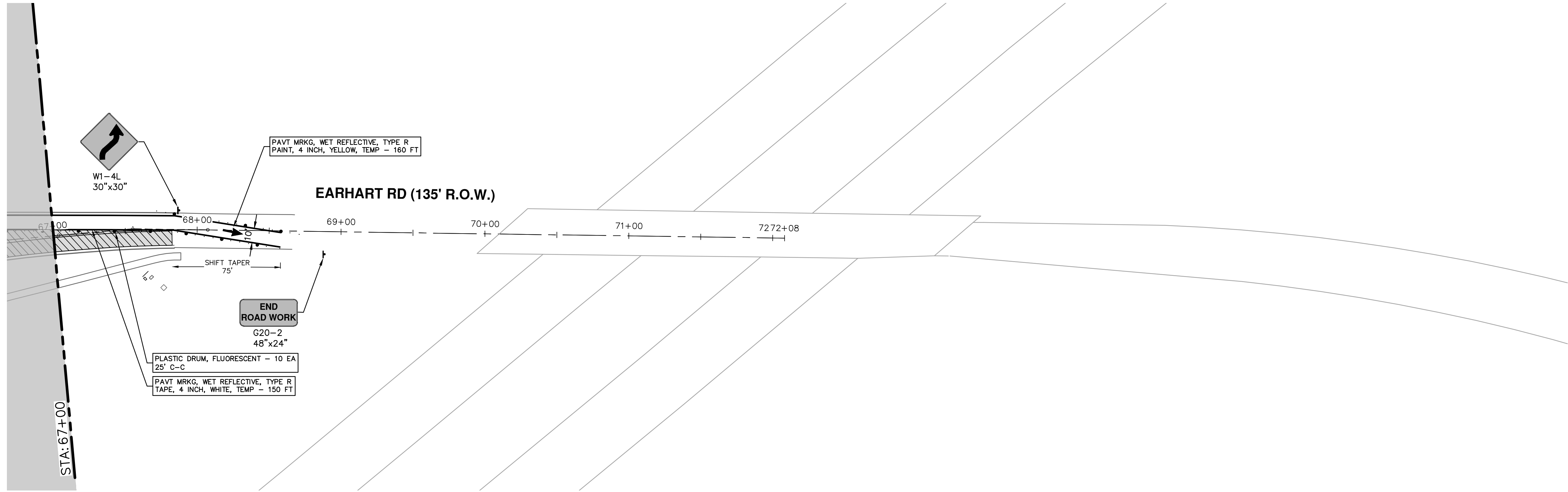
STA. 40+00 - STA. 58+00

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**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
 MAINTENANCE OF TRAFFIC - PH I  
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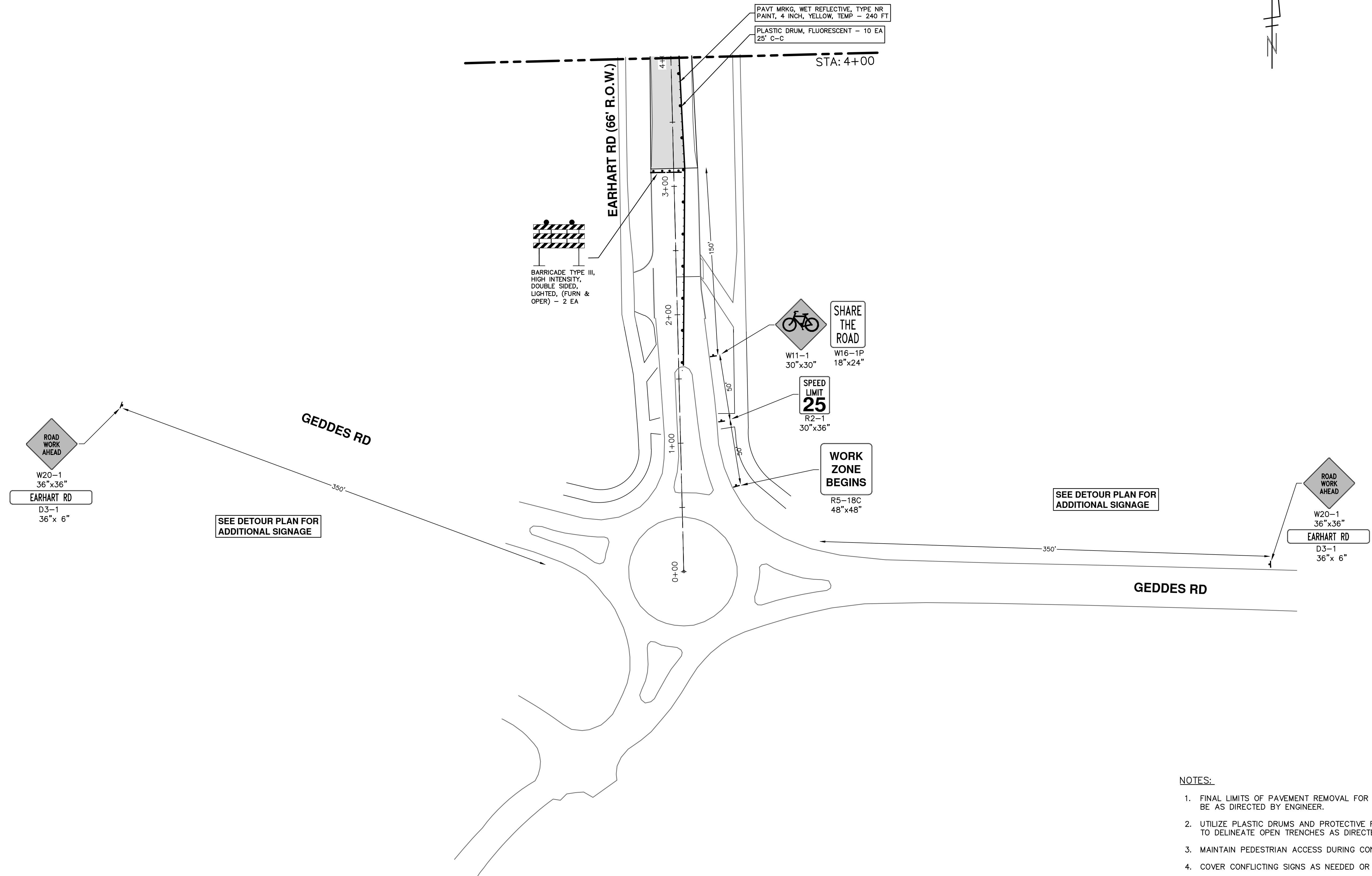


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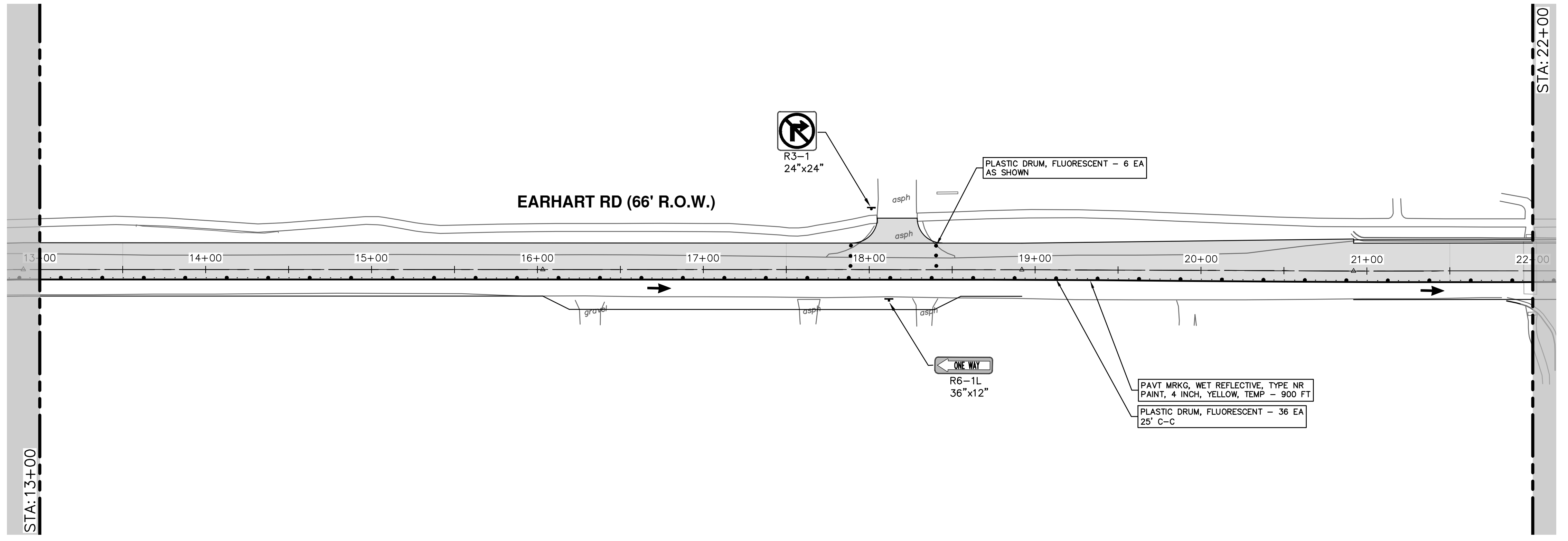
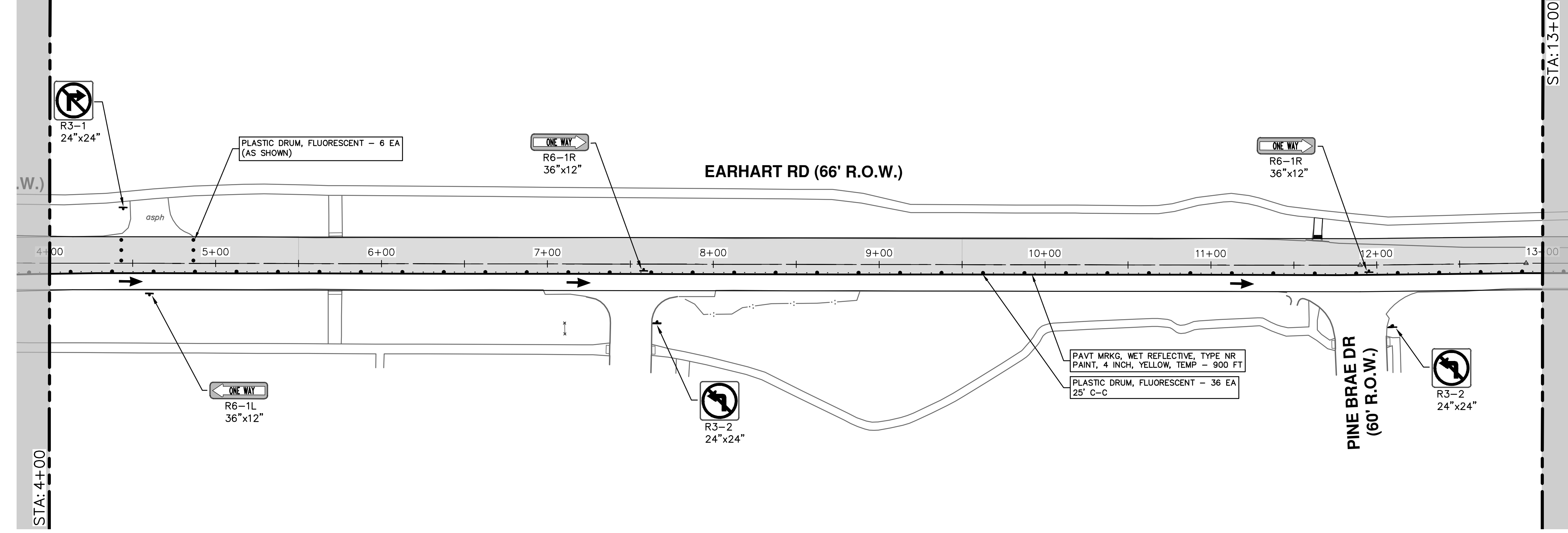


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SCALE: 1" = 40' DRAWING No. <b>2021023-20</b>	STA. 0+00 STA. 4+00 SHEET No. <b>20 OF 74</b>
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**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**

MAINTENANCE OF TRAFFIC - PH II

SCALE: 1" = 40'

DRAWING No. 2021023-21

SHEET No. 21 OF 74

STA. 4+00 - STA. 22+00

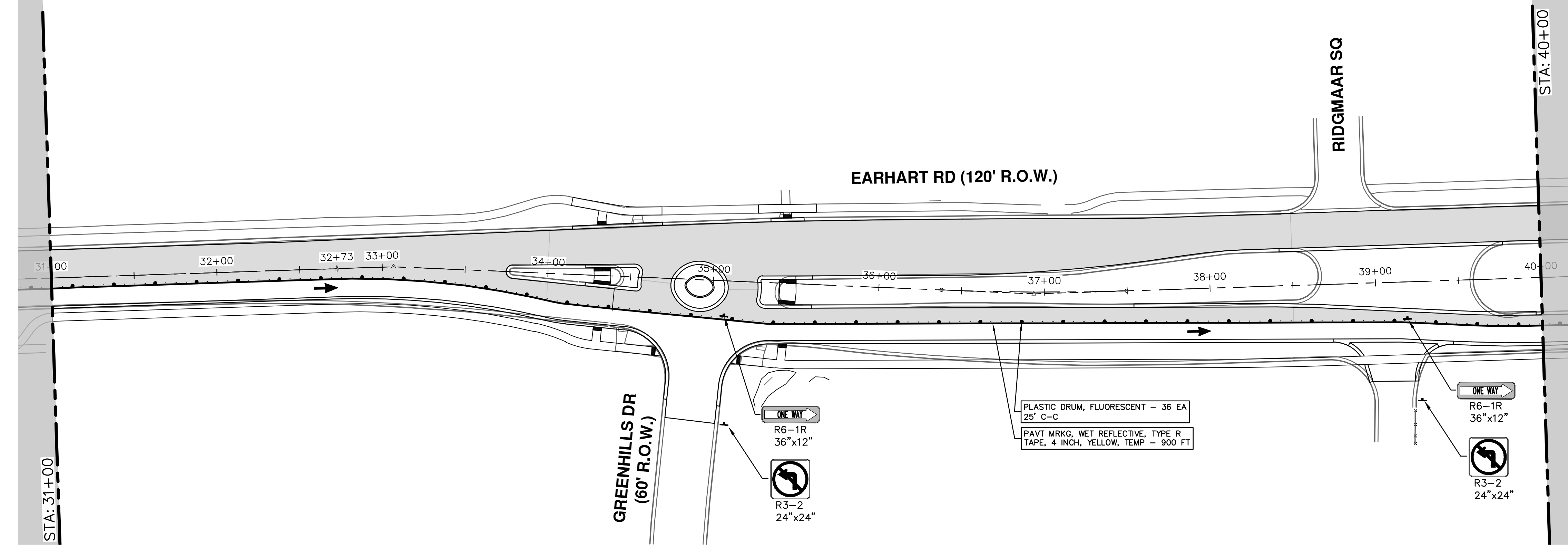
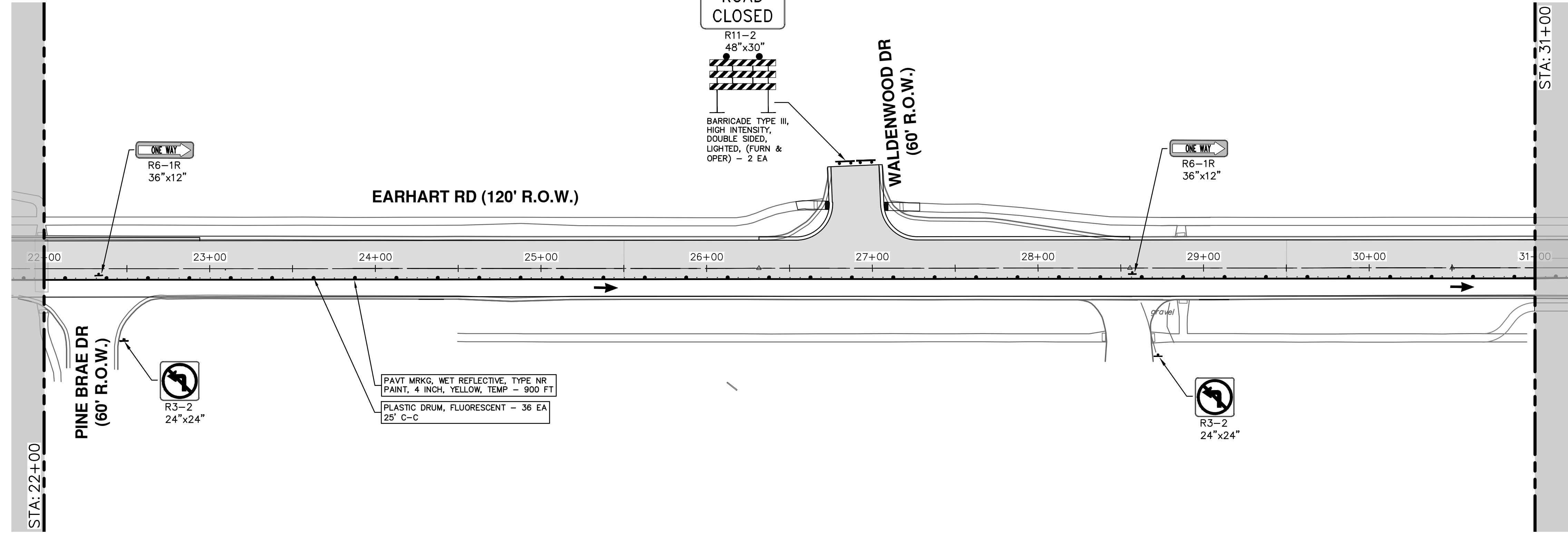
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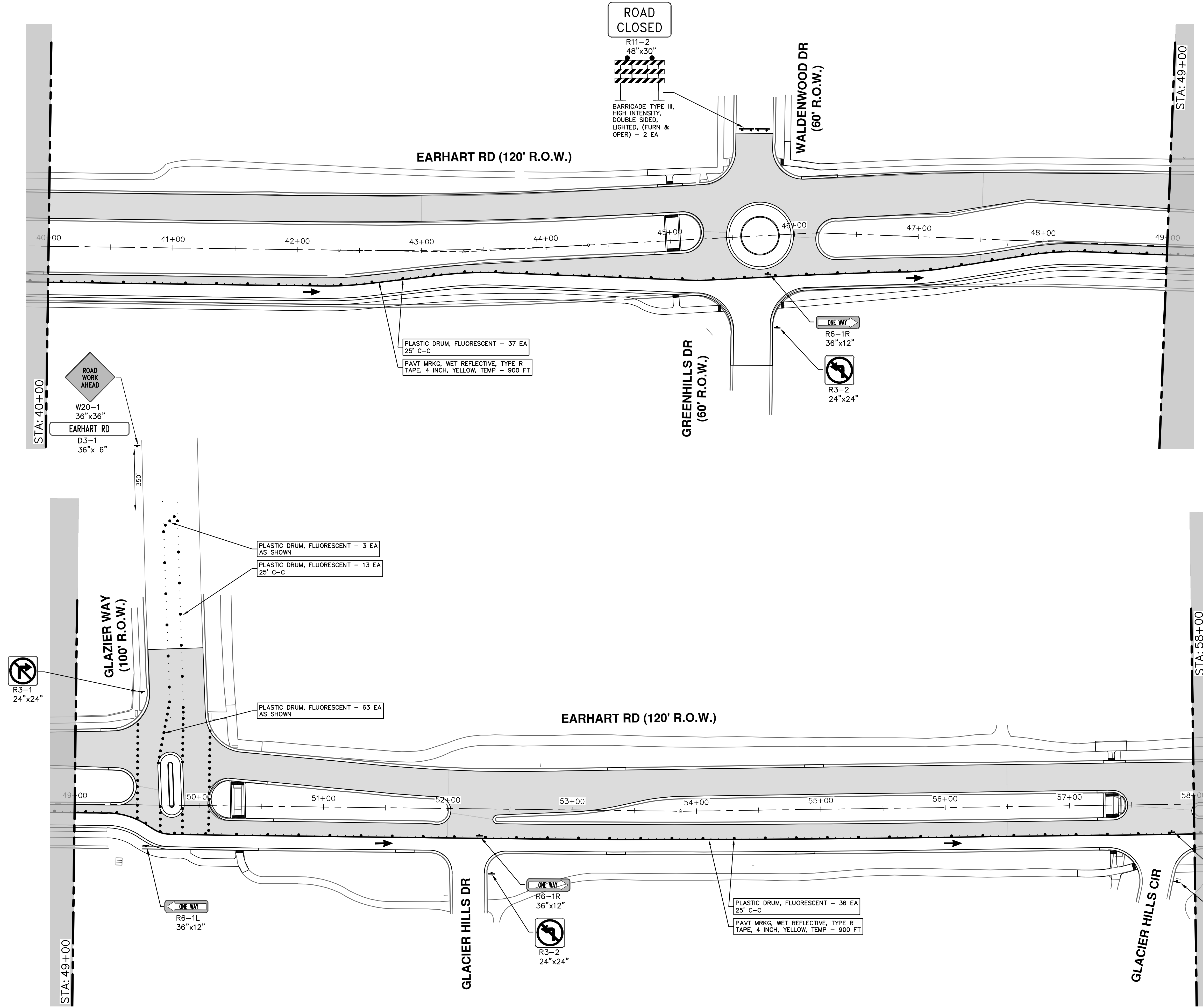


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EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
MAINTENANCE OF TRAFFIC - PH II  
STA. 22+00 - STA. 40+00

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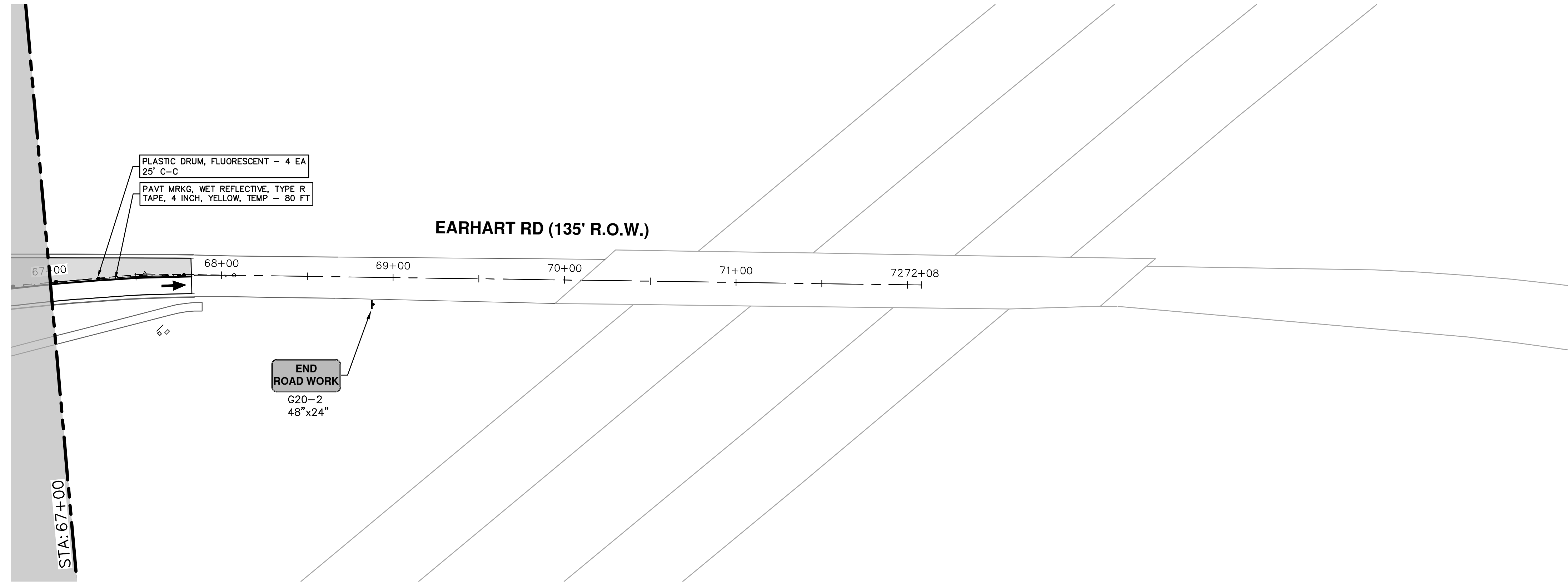
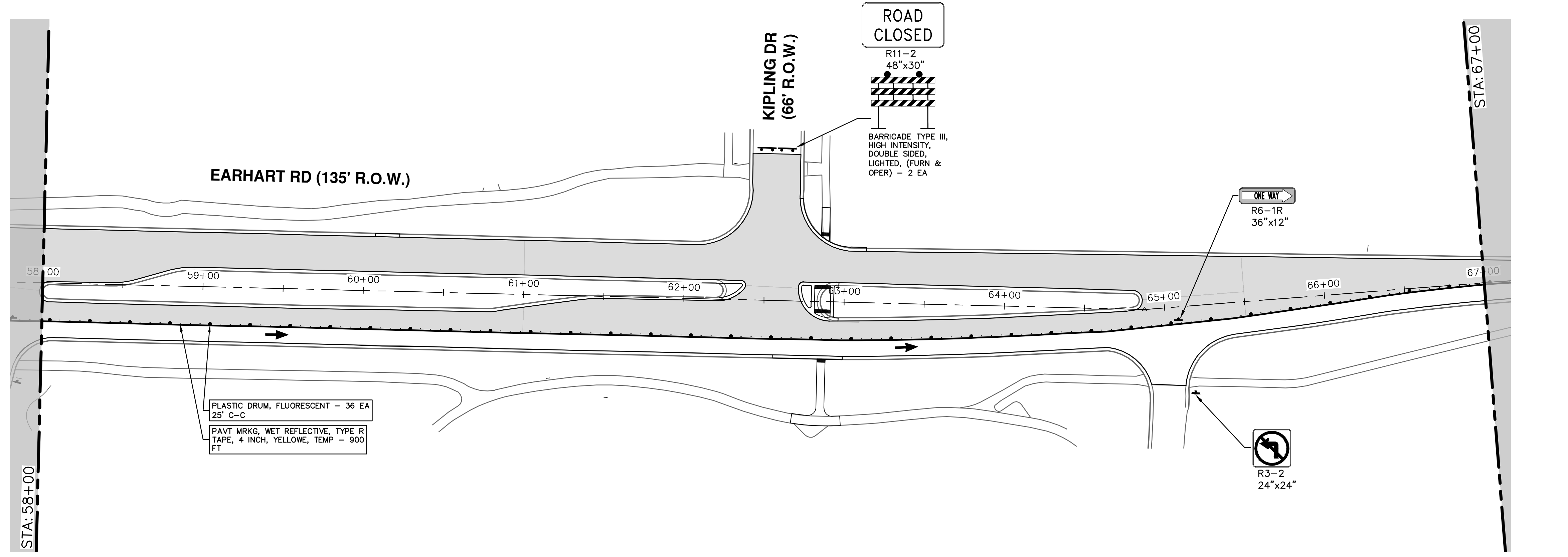
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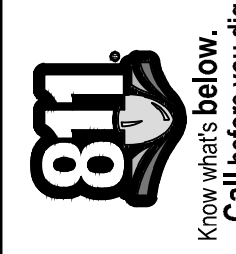
	
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SCALE: 1" = 40' 	DRAWING No. <b>2021023-23</b>
SHEET No. <b>23 OF 74</b>	STA. 40+00 - STA. 58+00
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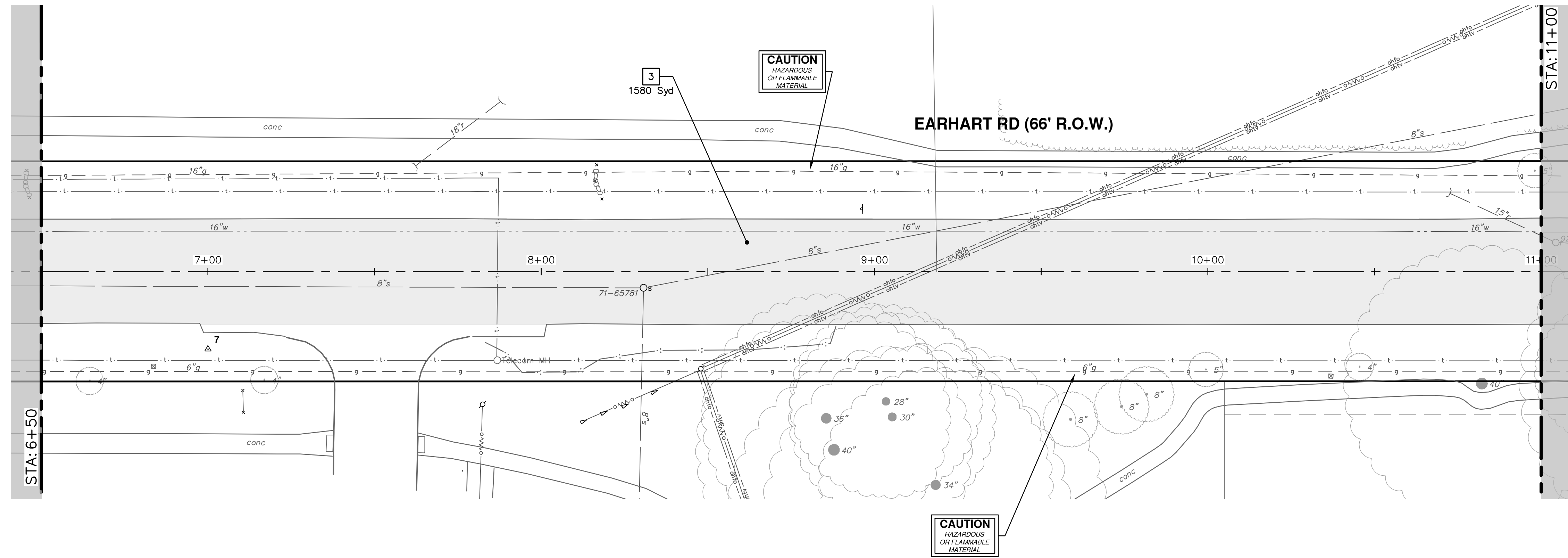
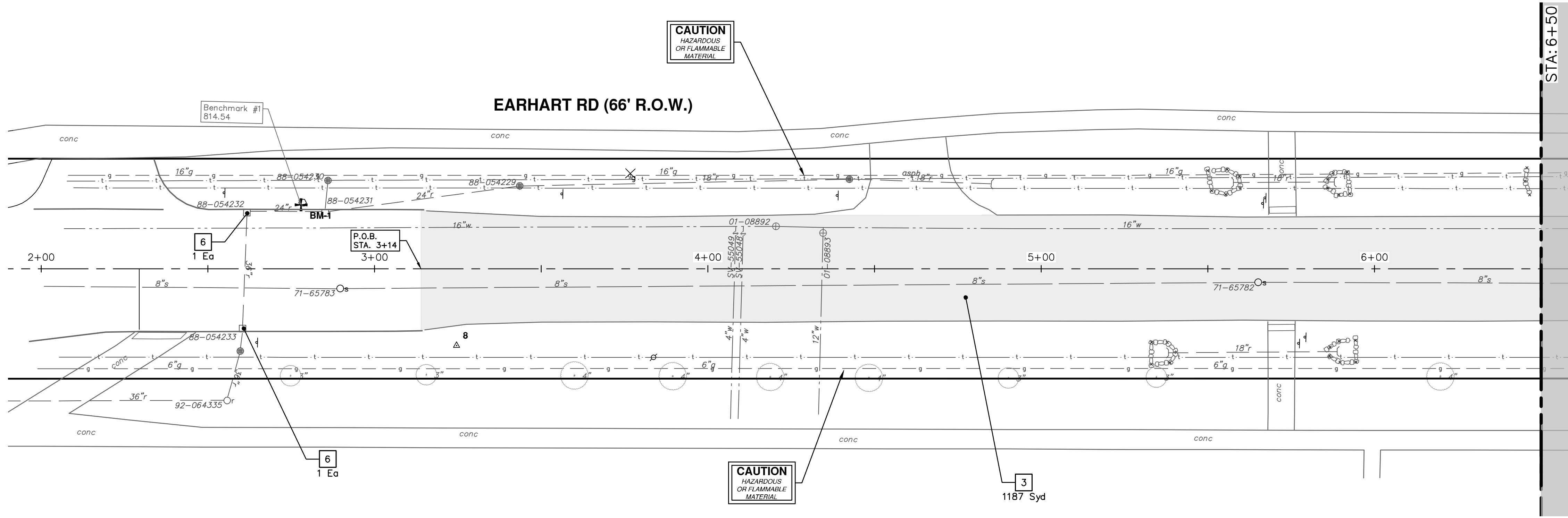
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REMOVAL KEY	
KEY	DESCRIPTION
1	CURB AND GUTTER, REM, MODIFIED
2	SIDEWALK, REM, MODIFIED
3	COLD MILLING HMA SURFACE
4	HMA SURFACE, REM
5	DRIVE APPROACH, REM, MODIFIED
6	SOIL EROSION CONTROL, INLET FILTER
7	TREE REMOVAL, 6 INCH TO 12 INCH
8	TREE REMOVAL, 13 INCH TO 24 INCH
9	TREE REMOVAL, GREATER THAN 24 INCH
10	TREE PROTECTION

**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
 REMOVALS

SCALE: 1" = 20'

DRAWING No. 2021023-25

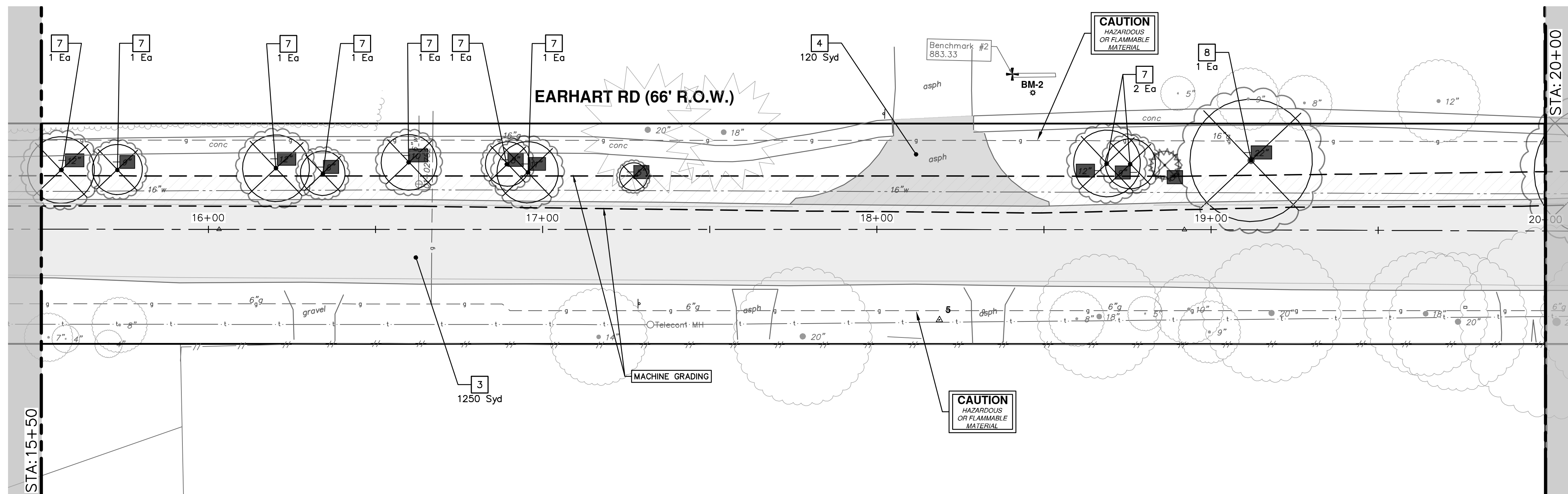
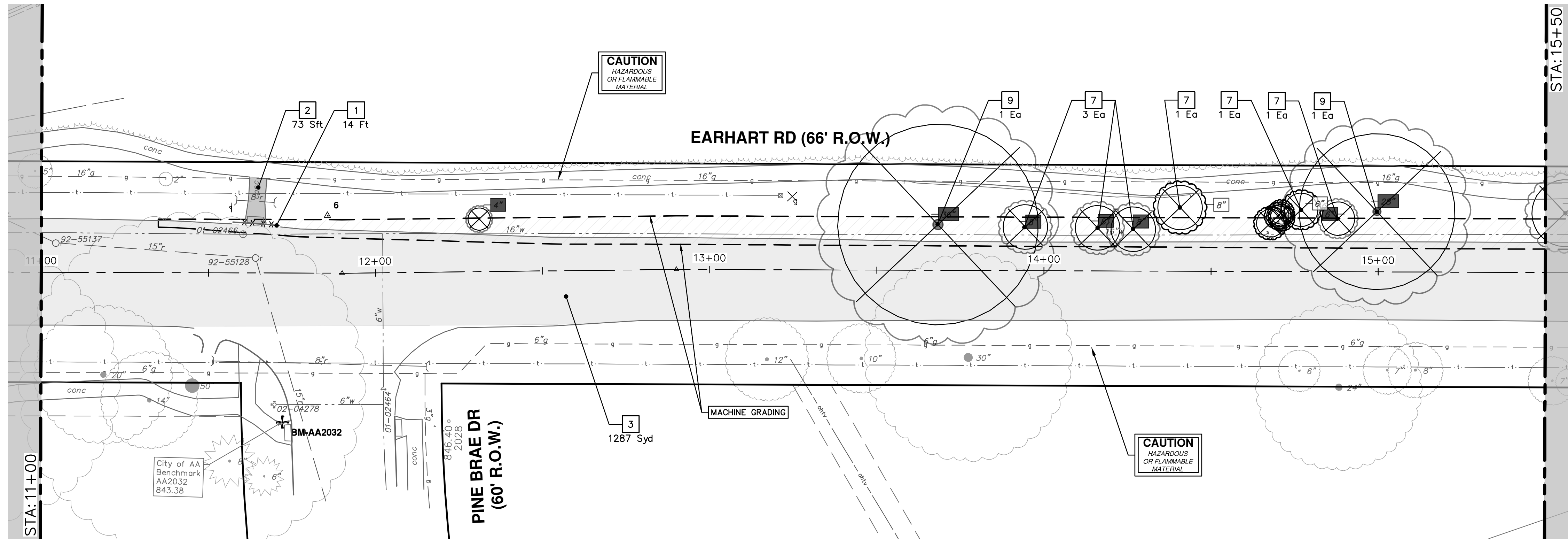
SHEET No. 25 OF 74

STA. 3+14 - STA. 11+00

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**811**  
 Know what's below.  
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REV.	DESCRIPTION	DATE	CC/DF	NB	CHECKED
00	OUT TO BID	4-27-23			



TREE REMOVAL TABLE				
Tag No.	Size	Species	Northing	Easting
	36"		28414.7151	13308565.9089
	8"		284440.8826	13308565.8666
	8"		284473.1677	13308565.6274
	8"		284462.4934	13308565.5318
	6"		284534.3033	13308560.7022
	8"		284486.9813	13308558.7283
	6"		284523.2160	13308558.3802
	28"		284545.8926	13308558.2864
	12"		284602.2434	13308557.1213
	9"		284619.0833	13308556.5501
	8"		284680.5390	13308556.0928
	12"		284666.5199	13308554.9597
	11"		284741.8037	13308554.2470
	10"		284706.2605	13308552.1806
	8"		284735.7861	13308551.9235
	22"		284958.0876	13308545.2038

TREE REMOVAL TABLE				
Tag No.	Size	Species	Northing	Easting
	12"		284915.0050	13308547.2100
	9"		284922.0220	13308547.1310

REMOVAL KEY	
KEY	DESCRIPTION
1	CURB AND GUTTER, REM, MODIFIED
2	SIDEWALK, REM, MODIFIED
3	COLD MILLING HMA SURFACE
4	HMA SURFACE, REM
5	DRIVE APPROACH, REM, MODIFIED
6	SOIL EROSION CONTROL, INLET FILTER
7	TREE REMOVAL, 6 INCH TO 12 INCH
8	TREE REMOVAL, 13 INCH TO 24 INCH
9	TREE REMOVAL, GREATER THAN 24 INCH
10	TREE PROTECTION



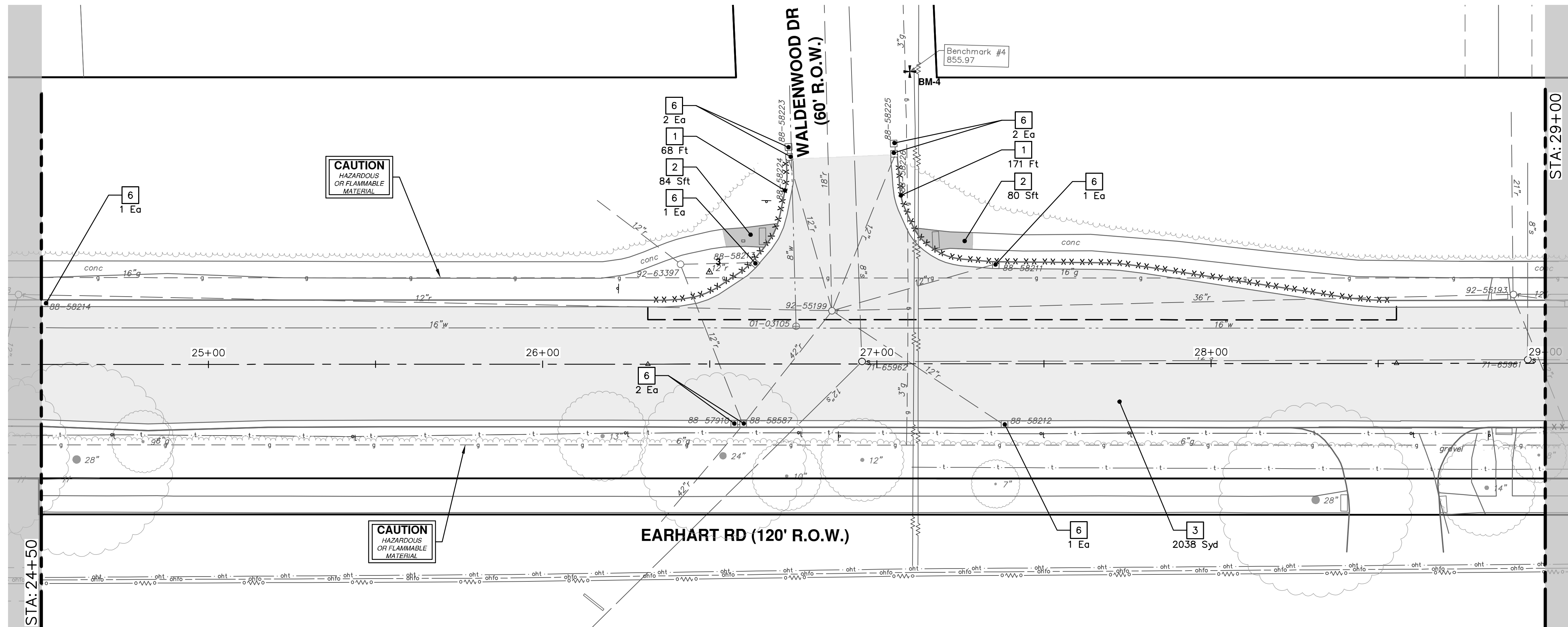
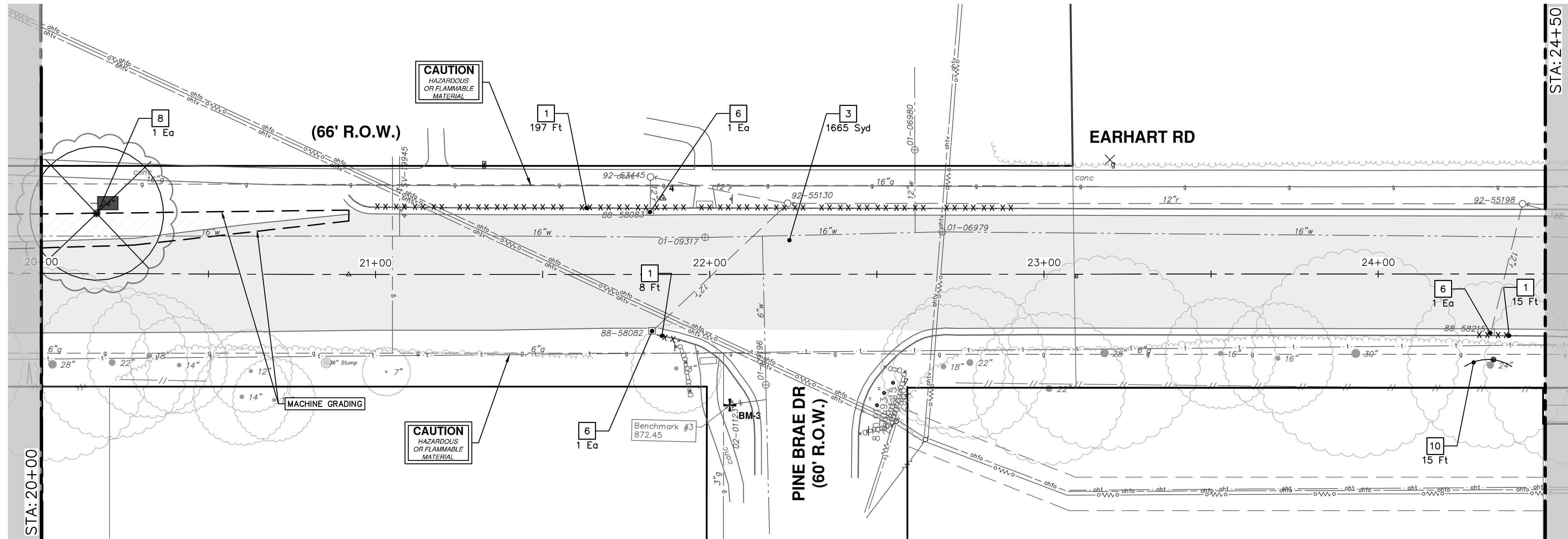
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CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING  
REMOVALS  
EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
STA. 11+00 - STA. 20+00

SCALE: 1" = 20'  
DRAWING No. 2021023-26

R:\2021023 Earhart (geddes to US23)\Plan Production\2021023Rem.dwg Dwg Created: 26-Apr-23 - \_a2 standard bw.stb - Plot Date: 27-Apr-23



REMOVAL KEY	
KEY	DESCRIPTION
1	CURB AND GUTTER, REM, MODIFIED
2	SIDEWALK, REM, MODIFIED
3	COLD MILLING HMA SURFACE
4	HMA SURFACE, REM
5	DRIVE APPROACH, REM, MODIFIED
6	SOIL EROSION CONTROL, INLET FILTER
7	TREE REMOVAL, 6 INCH TO 12 INCH
8	TREE REMOVAL, 13 INCH TO 24 INCH
9	TREE REMOVAL, GREATER THAN 24 INCH
10	TREE PROTECTION

TREE REMOVAL TABLE				
Tag No.	Size	Species	Northing	Easting
	24"		285062.7729	13308545.7864

**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**REMOVALS**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**

SCALE: 1" = 20'  
 DRAWING No. 2021023-27  
 SHEET No. 27 OF 74

STA. 20+00 - STA. 29+00

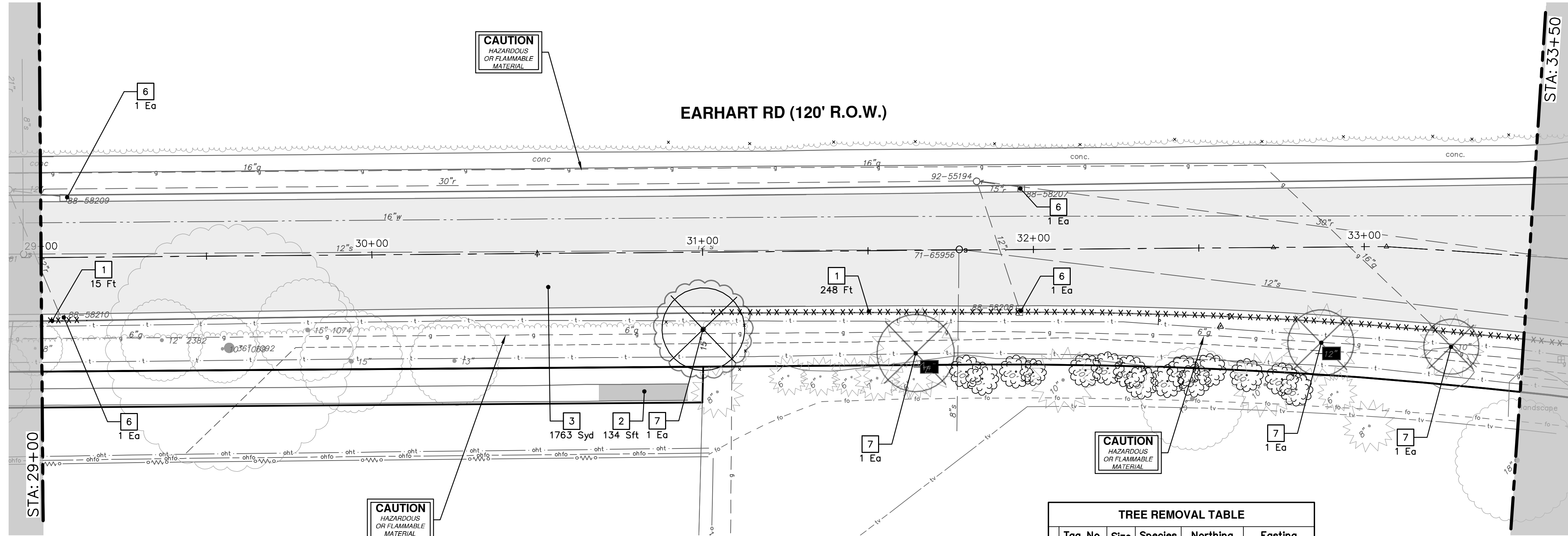
ANN ARBOR MICHIGAN

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 ANN ARBOR, MI 48106-8647  
 ANN ARBOR: 734-784-4410  
 WWW.ANARBOR.MI.GOV

REV.	DATE	DESCRIPTION
00	4-27-23	OUT TO BID
01		DRAWN
02		CHECKED

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 Know what's below.  
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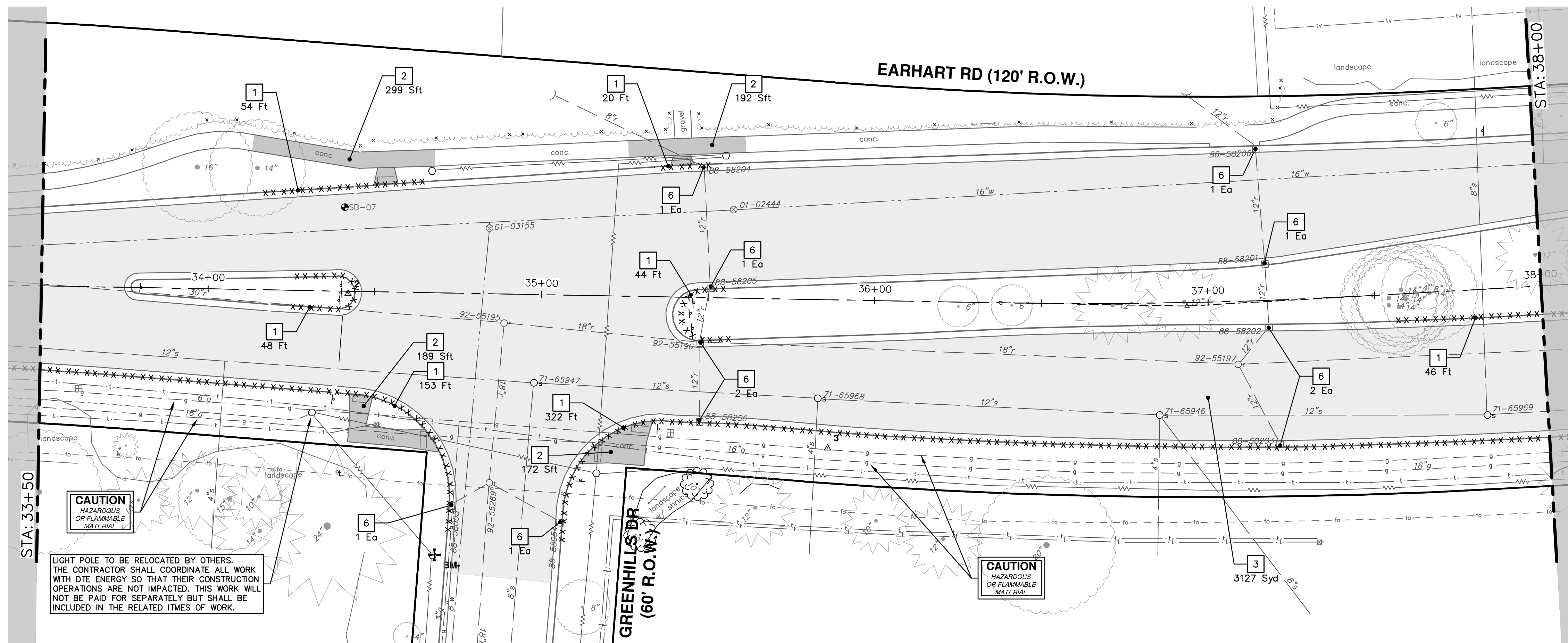
R:\2021023 Earhart (Geddes to US23)\Plan Production\2021023Rem.dwg Dwg Created: 26-Apr-23 - \_a2 standard bw.stb - Plot Date: 27-Apr-23



Tag No.	Size	Species	Northing	Easting
15"			286146.8980	13308559.2360

Tag No.	Size	Species	Northing	Easting
	14"		286211.1963	13308565.3055
	10"		286373.0583	13308560.5098
	12"		286333.6174	13308560.0656

KEY	DESCRIPTION
1	CURB AND GUTTER, REM, MODIFIED
2	SIDEWALK, REM, MODIFIED
3	COLD MILLING HMA SURFACE
4	HMA SURFACE, REM
5	DRIVE APPROACH, REM, MODIFIED
6	SOIL EROSION CONTROL, INLET FILTER
7	TREE REMOVAL, 6 INCH TO 12 INCH
8	TREE REMOVAL, 13 INCH TO 24 INCH
9	TREE REMOVAL, GREATER THAN 24 INCH
10	TREE PROTECTION



LIGHT POLE TO BE RELOCATED BY OTHERS. THE CONTRACTOR SHALL COORDINATE ALL WORK WITH DTE ENERGY SO THAT THEIR CONSTRUCTION OPERATIONS ARE NOT IMPACTED. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE RELATED TIMES OF WORK.



**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
 REMOVALS

SHEET No. **28 OF 74**

Scale: 1" = 20'

DRAWING No. **2021023-28**

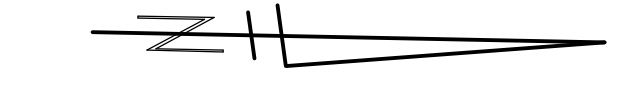
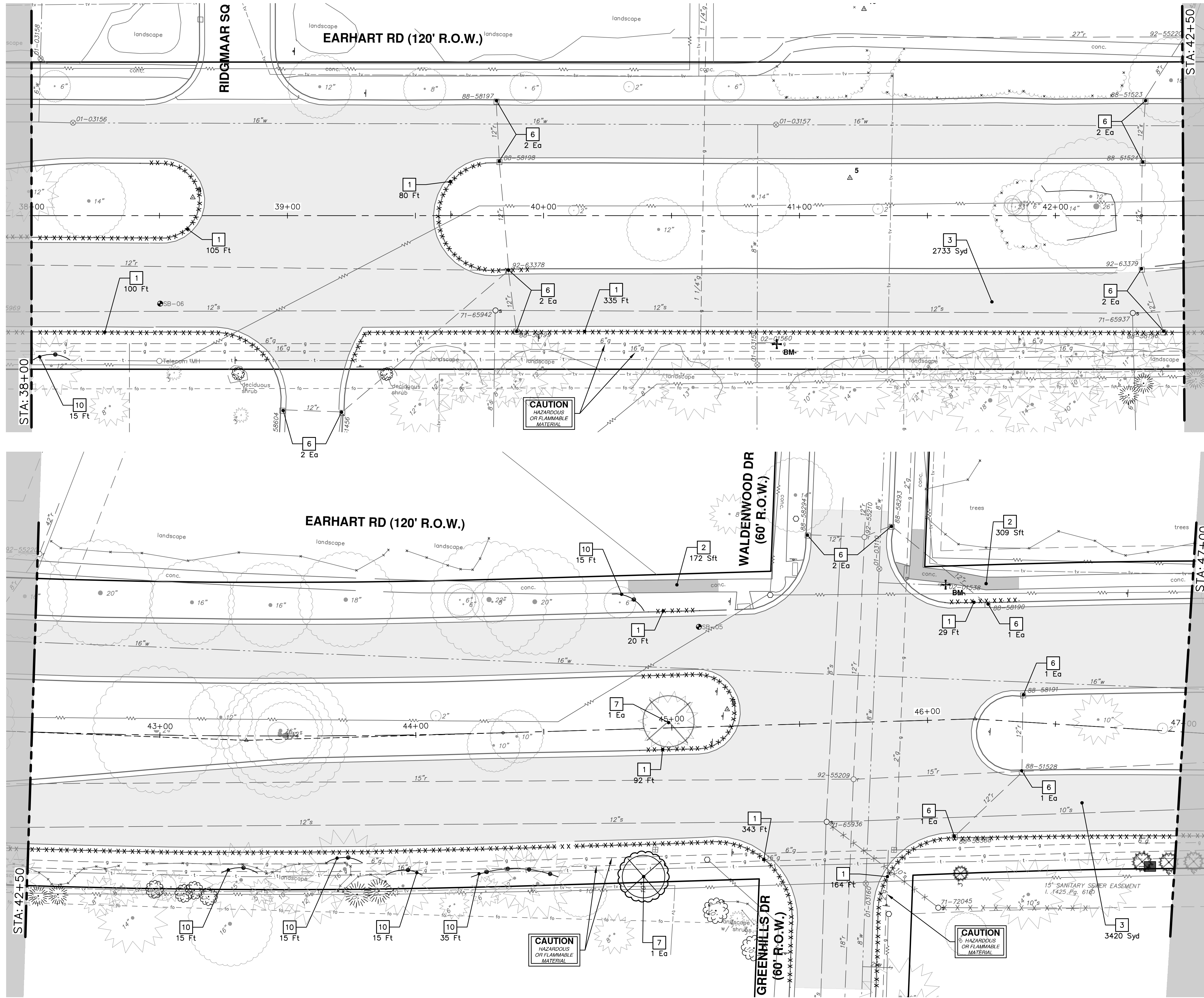
STA. 29+00 - STA. 38+00

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NO.	DESCRIPTION	DATE	DRAWN	CHECKED
1		4-27-23		
2				
3				
4				
5				
6				
7				
8				
9				
10				

811  
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R:\2021023 Earhart (Geddes to US23)\Plan Production\2021023Rem.dwg Dwg Created: 26-Apr-23 - \_a2 standard bw.stb - Plot Date: 27-Apr-23



REMOVAL KEY	
KEY	DESCRIPTION
1	CURB AND GUTTER, REM, MODIFIED
2	SIDEWALK, REM, MODIFIED
3	COLD MILLING HMA SURFACE
4	HMA SURFACE, REM
5	DRIVE APPROACH, REM, MODIFIED
6	SOIL EROSION CONTROL, INLET FILTER
7	TREE REMOVAL, 6 INCH TO 12 INCH
8	TREE REMOVAL, 13 INCH TO 24 INCH
9	TREE REMOVAL, GREATER THAN 24 INCH
10	TREE PROTECTION

TREE REMOVAL TABLE				
Tag No.	Size	Species	Northing	Easting
	12"		287543.5632	13308515.7878

TREE REMOVAL TABLE				
Tag No.	Size	Species	Northing	Easting
	10"		287537.6440	13308576.9080

**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
 REMOVALS

SCALE: 1" = 20'  
 DRAWING No. 2021023-29  
 SHEET No. 29 OF 74

STATION: STA. 38+00 - STA. 47+00

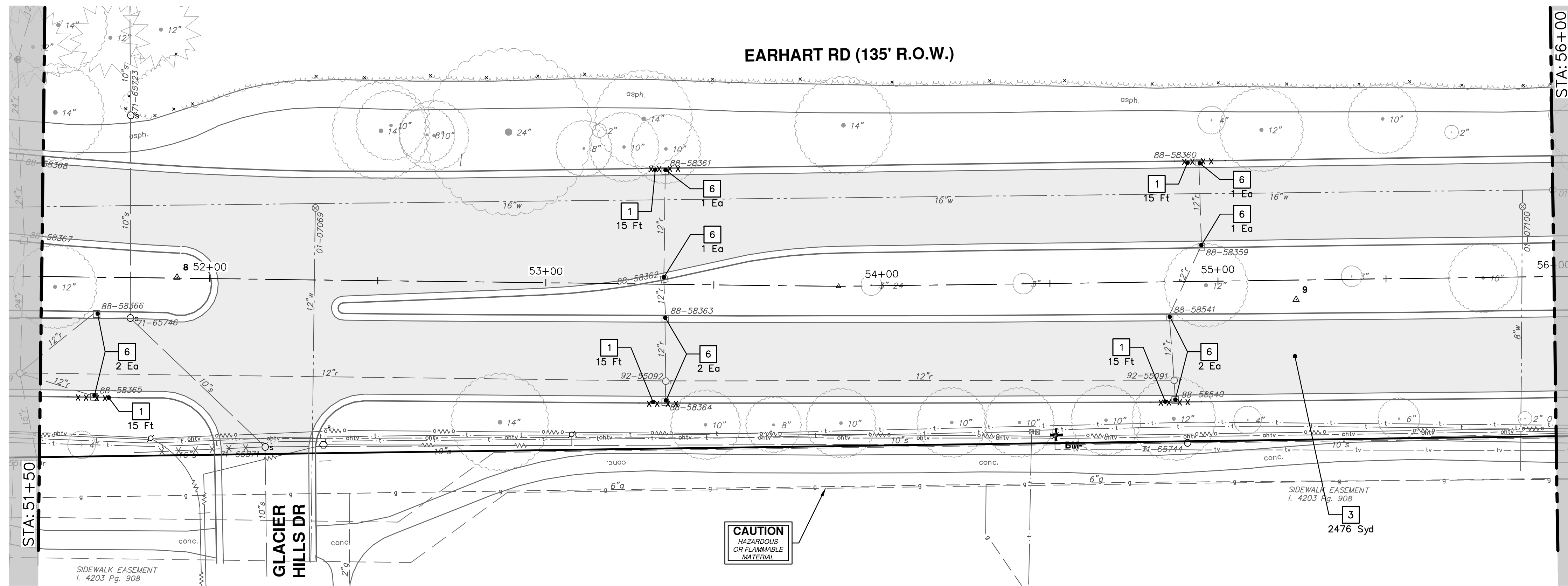
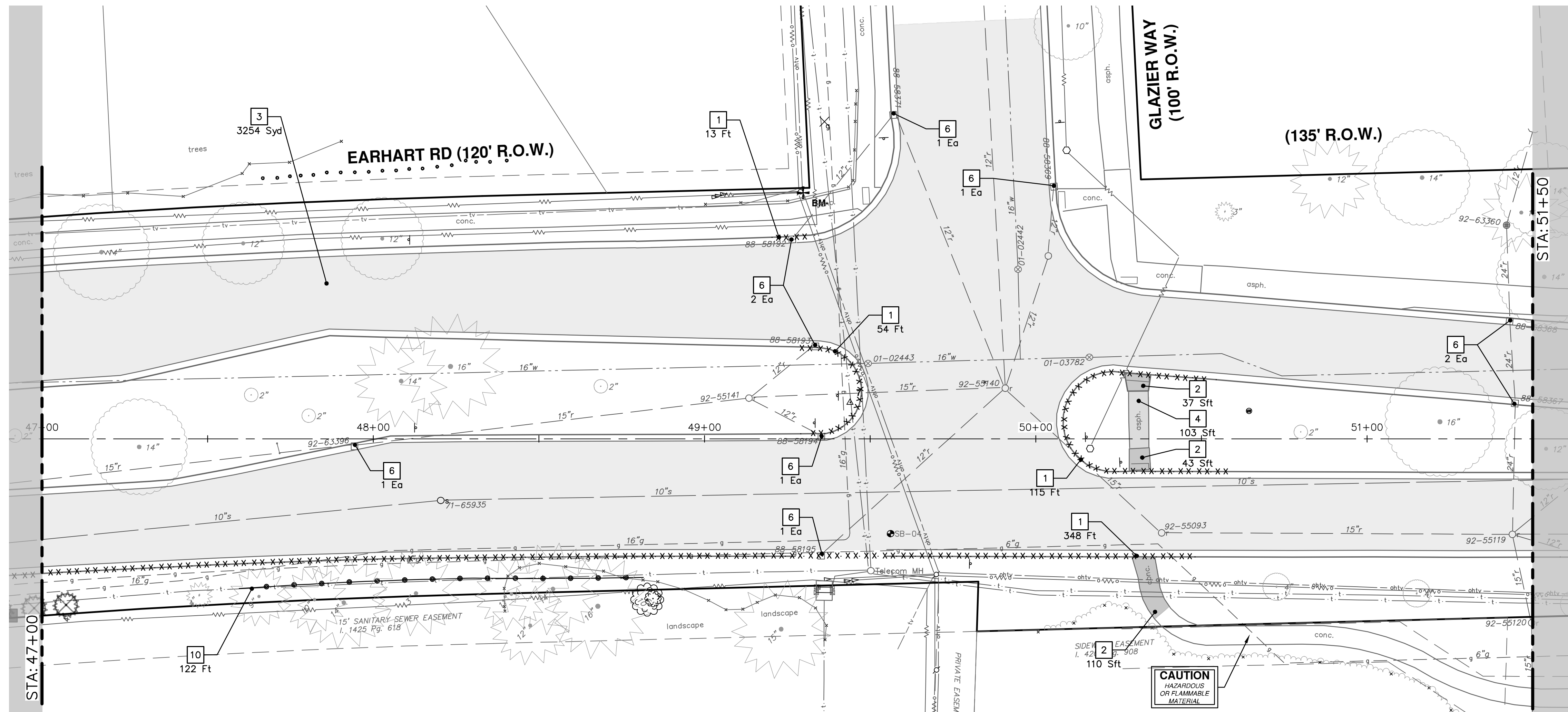
DATE: 4-27-23  
 DRAWN: CC/DF  
 CHECKED: NB

DESCRIPTION: REV. 00  
 OUT TO BID

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R:\2021023 Earhart (Geddes to US23)\Plan Production\2021023Rem.dwg Dwg Created: 26-Apr-23 - \_o2 standard bw.stb - Plot Date: 27-Apr-23



REMOVAL KEY	
KEY	DESCRIPTION
1	CURB AND GUTTER, REM, MODIFIED
2	SIDEWALK, REM, MODIFIED
3	COLD MILLING HMA SURFACE
4	HMA SURFACE, REM
5	DRIVE APPROACH, REM, MODIFIED
6	SOIL EROSION CONTROL, INLET FILTER
7	TREE REMOVAL, 6 INCH TO 12 INCH
8	TREE REMOVAL, 13 INCH TO 24 INCH
9	TREE REMOVAL, GREATER THAN 24 INCH
10	TREE PROTECTION

**811**  
Know what's below.  
Call Before you dig.

DATE: 4-27-23  
DRAWN: CC/DF  
CHECKED: NB

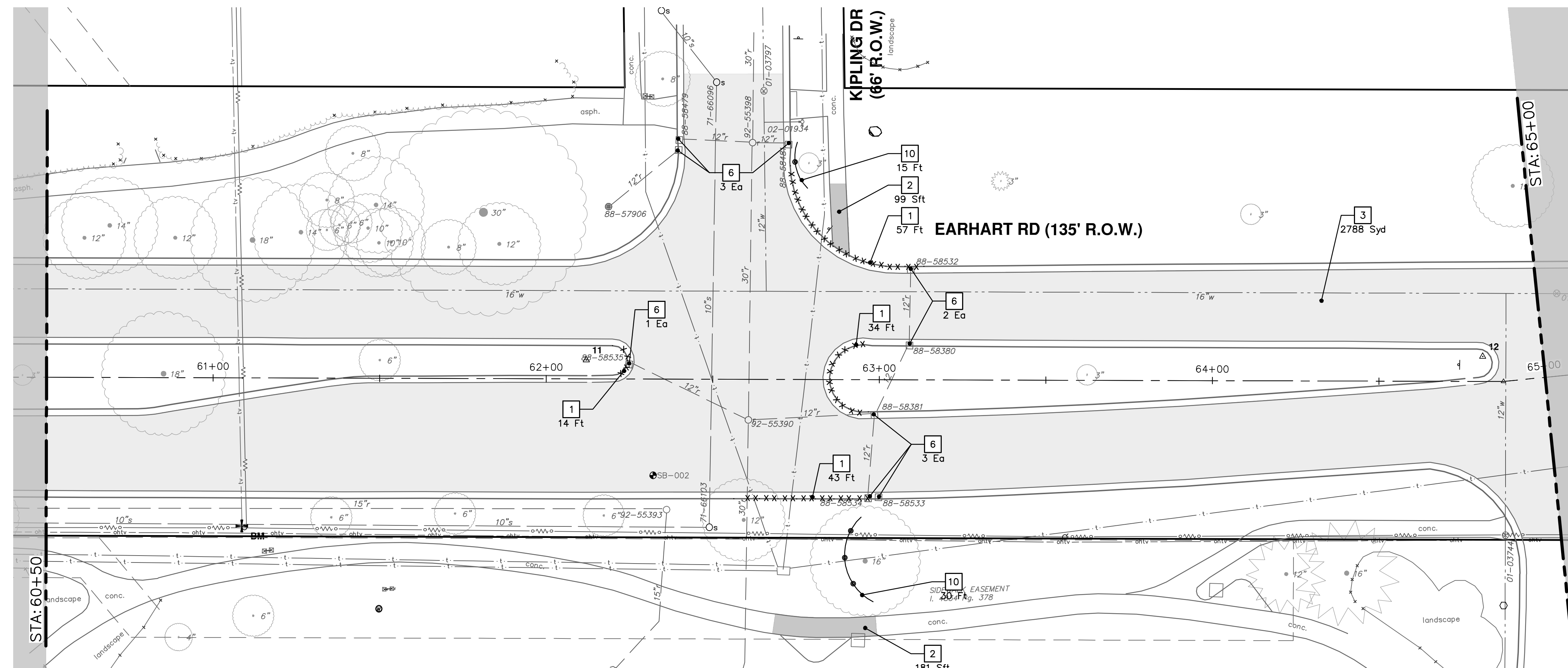
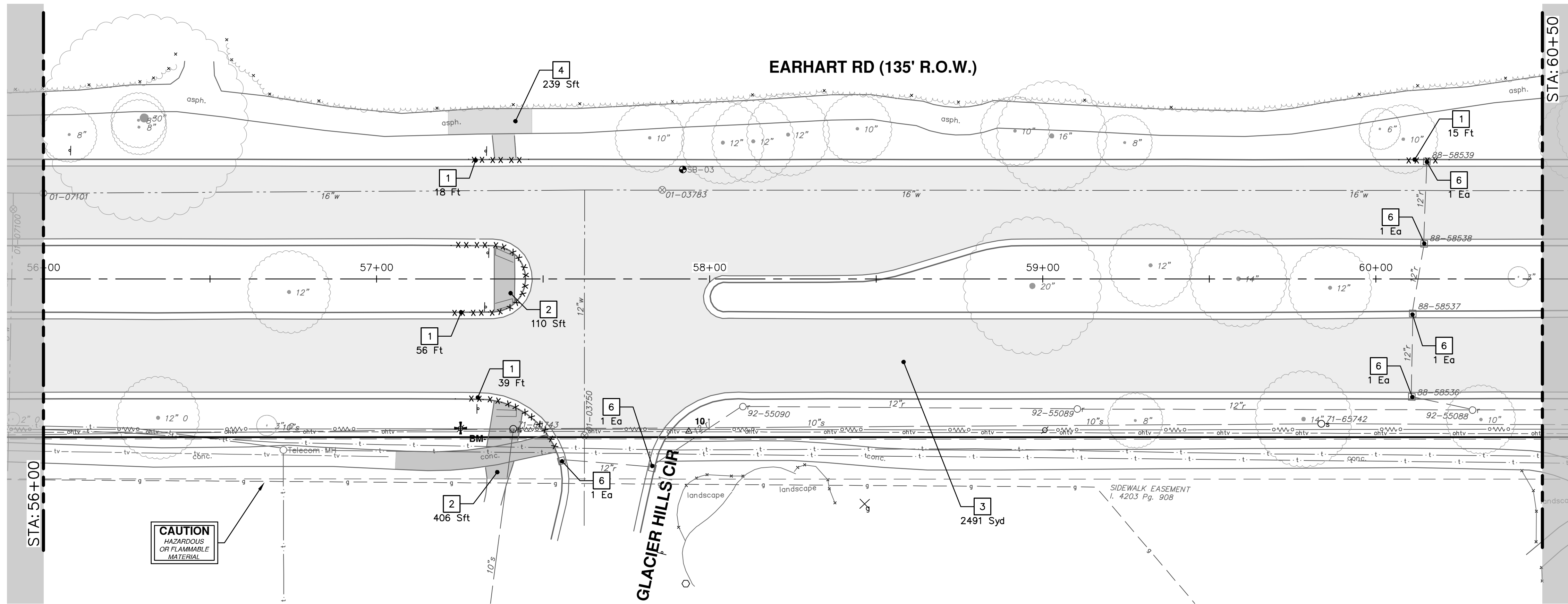
**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
REMOVALS  
EARHART ROAD IMPROVEMENTS (GEDDES - US23)

DESCRIPTION: REV.  
OUT TO BID: 4-27-23  
DATE: 4-27-23  
DRAWN: CC/DF  
CHECKED: NB

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STATION: STA. 47+00 - STA. 56+00  
DRAWING No. 2021023-30  
SHEET No. 30 OF 74

R:\2021023 Earhart (Geddes to US23)\Plan Production\2021023Rem.dwg Dwg Created: 26-Apr-23 - \_o2 standard bw.stb - Plot Date: 27-Apr-23



REMOVAL KEY	
KEY	DESCRIPTION
1	CURB AND GUTTER, REM, MODIFIED
2	SIDEWALK, REM, MODIFIED
3	COLD MILLING HMA SURFACE
4	HMA SURFACE, REM
5	DRIVE APPROACH, REM, MODIFIED
6	SOIL EROSION CONTROL, INLET FILTER
7	TREE REMOVAL, 6 INCH TO 12 INCH
8	TREE REMOVAL, 13 INCH TO 24 INCH
9	TREE REMOVAL, GREATER THAN 24 INCH
10	TREE PROTECTION

**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
 REMOVALS  
 STA. 56+00 - STA. 65+00

SHEET No. **31 OF 74**

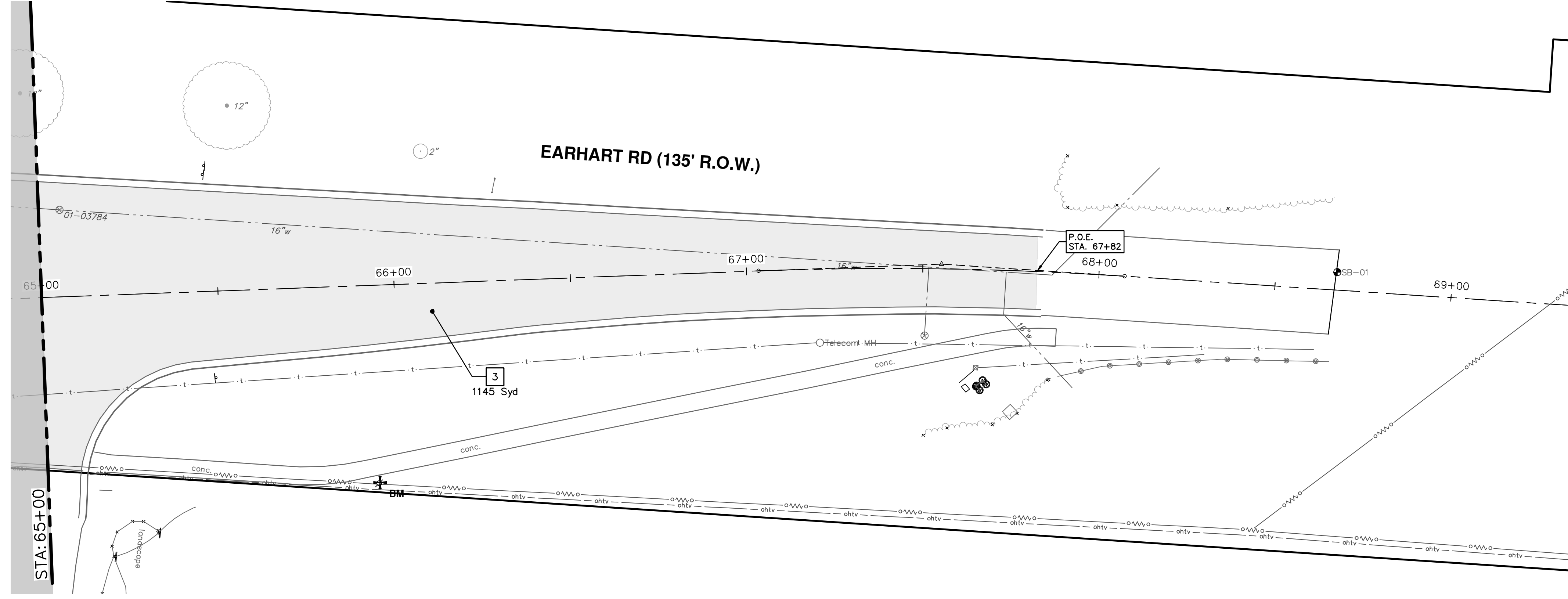
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**ANN ARBOR MICHIGAN**

DRAWING No. **2021023-31**

REV.	DESCRIPTION	DATE	BY	CHECKED
00	OUT TO BID	4-27-23	CC/DF	NB



REMOVAL KEY	
KEY	DESCRIPTION
1	CURB AND GUTTER, REM, MODIFIED
2	SIDEWALK, REM, MODIFIED
3	COLD MILLING HMA SURFACE
4	HMA SURFACE, REM
5	DRIVE APPROACH, REM, MODIFIED
6	SOIL EROSION CONTROL, INLET FILTER
7	TREE REMOVAL, 6 INCH TO 12 INCH
8	TREE REMOVAL, 13 INCH TO 24 INCH
9	TREE REMOVAL, GREATER THAN 24 INCH
10	TREE PROTECTION

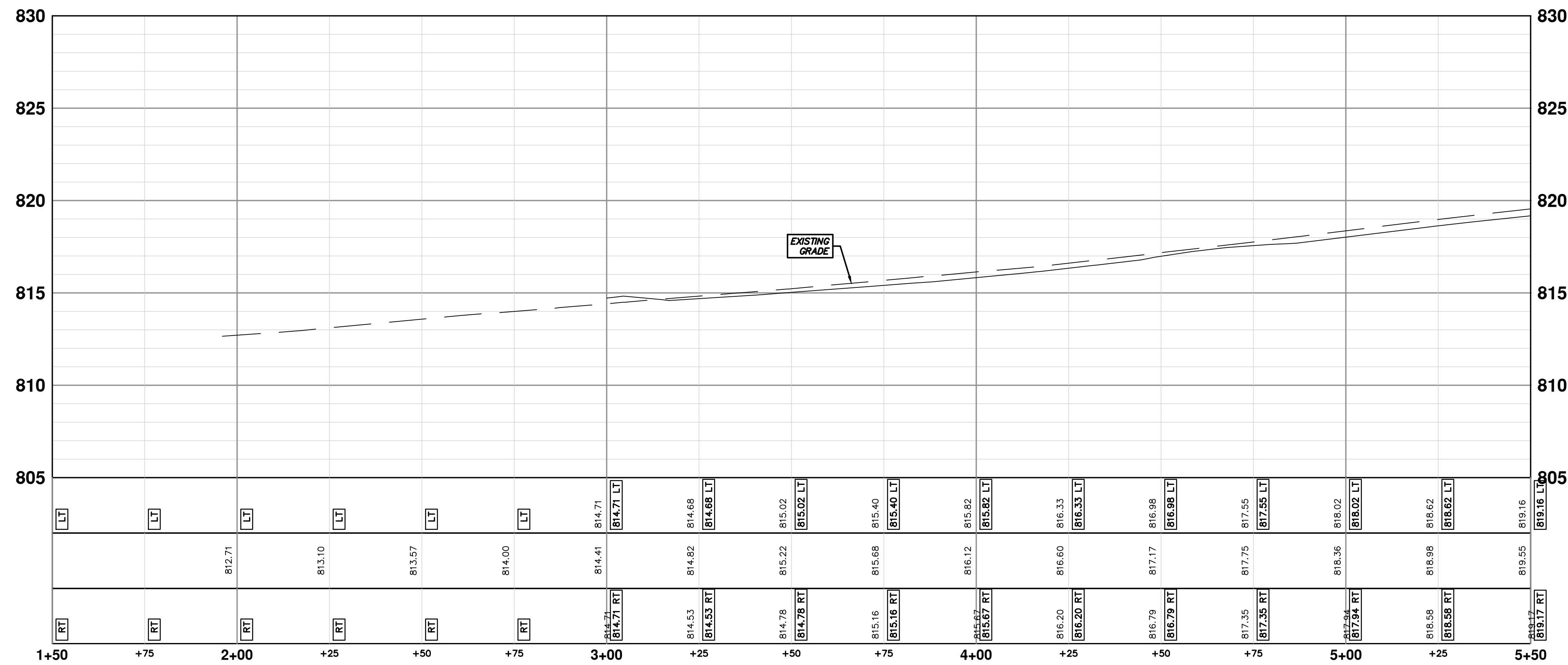
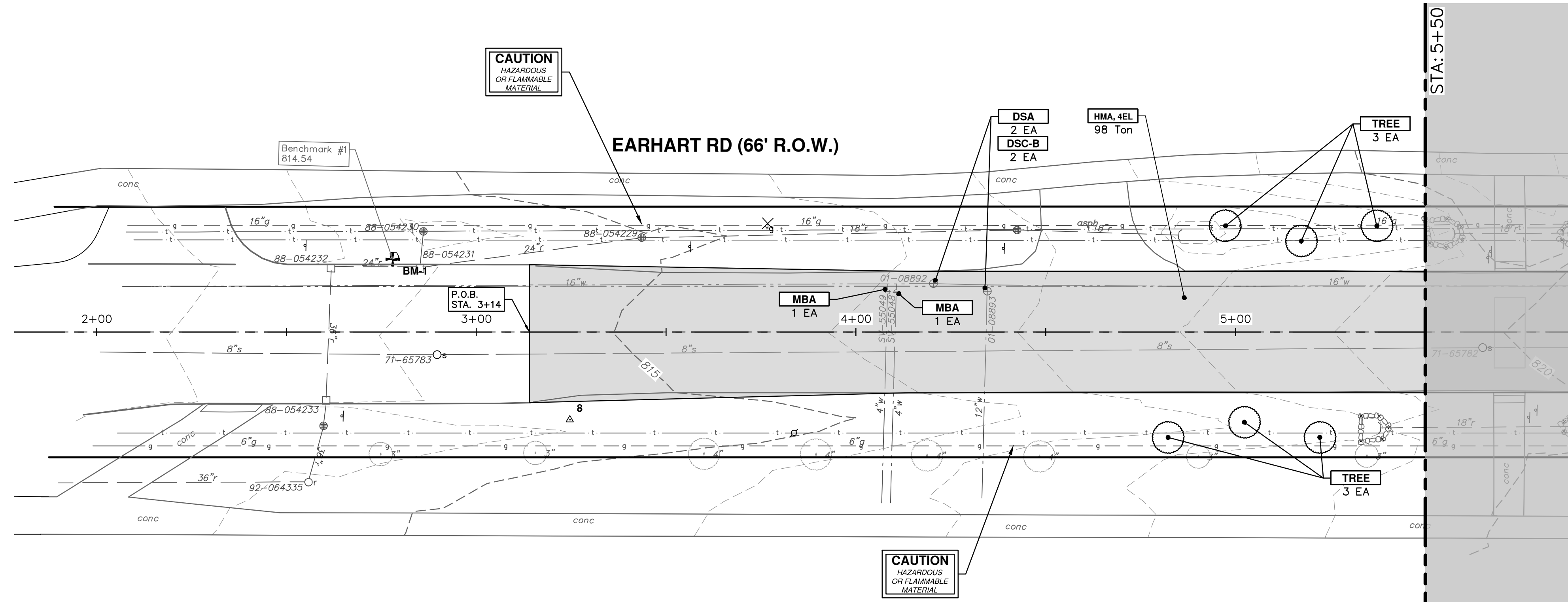


REV.	DESCRIPTION
00	OUT TO BID

DATE	CC/DF	NB	CHECKED
4-27-23			



R:\2021023 Earhart (Geddes to US23)\Plan Production\2021023Prd.dwg Dwg Created: 27-Apr-23 - \_a2 standard bw.stb - Plot Date: 27-Apr-23



KEY	DESCRIPTION
CP	CONC PAVT WITH INTEGRAL CURB, NONREINF, 8 INCH
CG	CURB AND GUTTER, CONC, DET F4, SPECIAL
APP	HMA APPROACH
SR6	CURB RAMP, CONCRETE, 6 INCH, P-NC, ADA, MODIFIED
SC4	SIDEWALK, CONCRETE, 4 INCH, SPECIAL
SC6	SIDEWALK, CONCRETE, 6 INCH, SPECIAL
DWT	DETECTABLE WARNING TILES
HMA	HMA, 4EML / 4EL / 3C
DSC-B	DR STRUCTURE COVER, TYPE B
DSC-K	DR STRUCTURE COVER, TYPE K
DSC-Q	DR STRUCTURE COVER, TYPE Q
ABO	ADJUST BY OTHERS
DSA	DR STRUCTURE COVER, ADJ, CASE 1
MBA	ADJUST MONUMENT BOX OR VALVE BOX
DOM	DRIVEWAY OPENING, CONC, DET M, P-NC
DC-6	PLACE CONC, DRIVE APPROACH, NON-REINFORCED, P-NC, 6 INCH. PLACE AND COMPACT 6 INCH SUBBASE, CIP, OR AGGREGATE BASE, 6 INCH

TREES SHOWN SHALL BE PLANTED AS A MIX OF SPECIES WITH THE MAJORITY BEING LARGE SHADE TREES INTERMIXED WITH MEDIUM AND SMALL TREES.

TOTAL TREES SHOWN = 90  
 LARGE SHADE TREES (L) = 50

LATIN NAME (MATURE SIZE) - COMMON NAME:  
 • Gleditsia triacanthos inermis 'Skyline' (L) - Honeylocust  
 • Gynnocladus dioica (L) - Kentucky coffeetree  
 • Liquidambar styraciflua (M) - Sweetgum  
 • Liriodendron tulipifera (L) - Tuliptree  
 • Nyssa sylvatica (M) - Blackgum  
 • Ostrya virginiana (M) - Ironwood  
 • Quercus rubra (L) - Red oak  
 • Quercus macrocarpa (L) - Bur oak  
 • Amelanchier canadensis, tree form (S) - Serviceberry  
 • Celtis occidentalis (L) - Hackberry  
 • Cercis canadensis (S) - Redbud  
 • Cornus florida (S) - flowering dogwood

**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**

**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**

ROAD PLAN & PROFILE

STA. 3+14 - STA 5+50

SHEET No. 33 OF 74

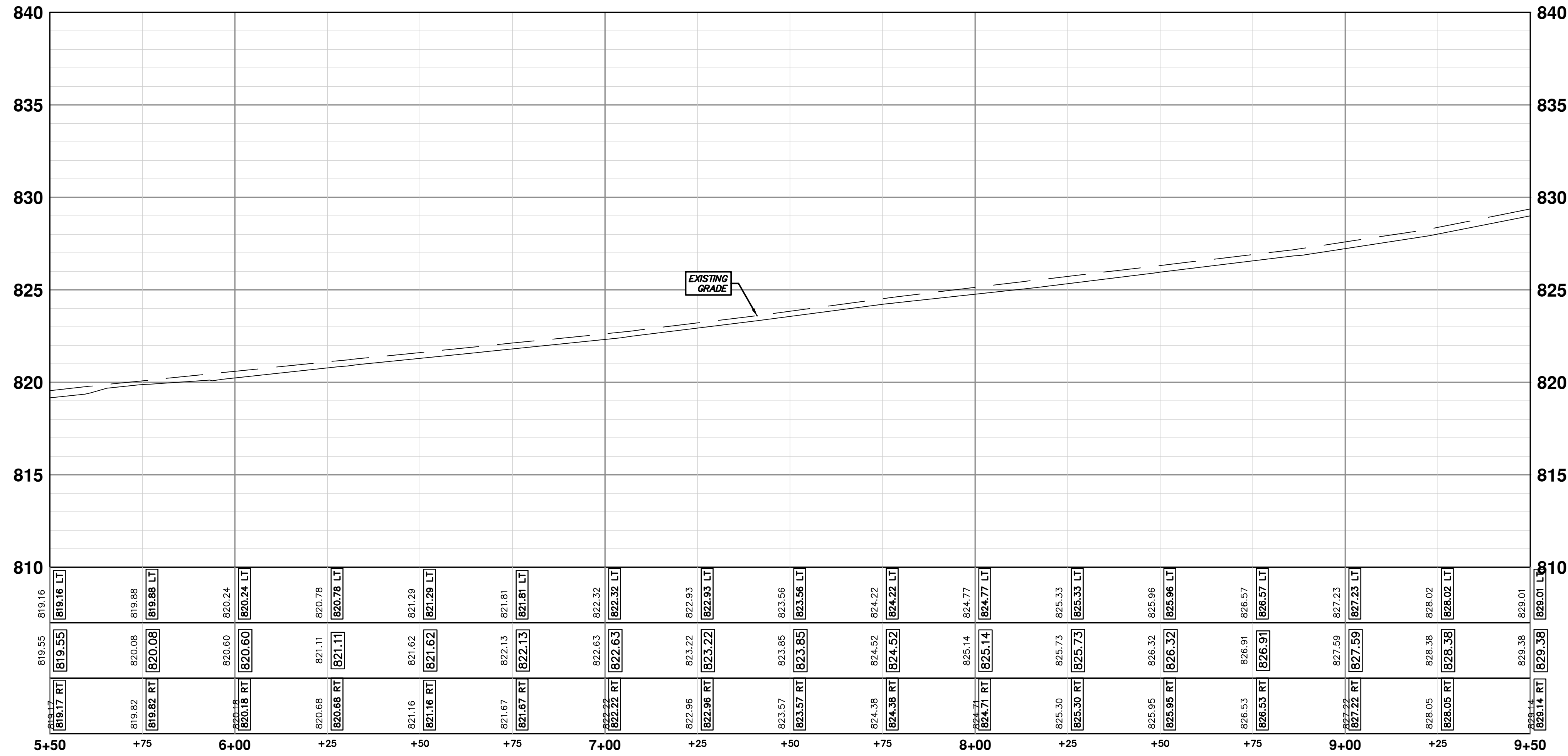
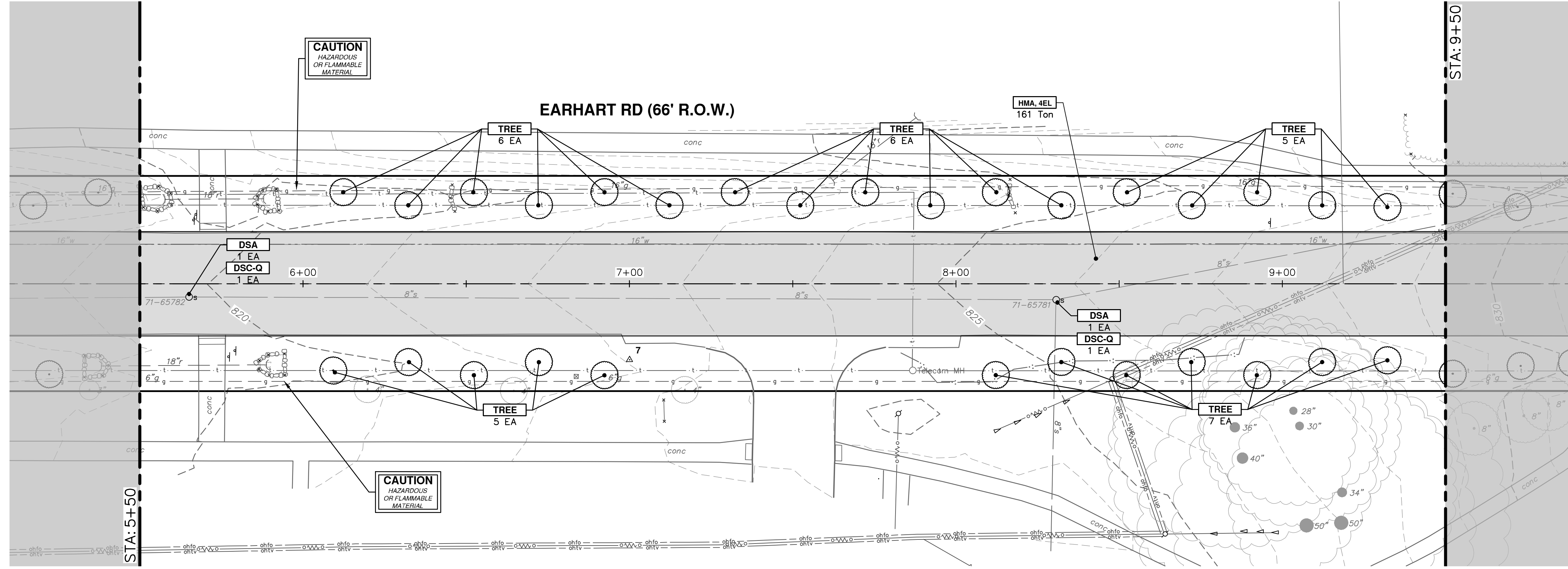
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 ANN ARBOR, MI 48106-6647  
 WWW.A2GOV.ORG

SCALE PLAN: 1" = 40'  
 PROFILE: 1" = 4'

DRAWING No. 2021023-33

NO	DATE	DESCRIPTION	BY	CHECKED
00	4-27-23	OUT TO BID	CC/DF	NB
			DATE	DRAWN
				CHECKED

Know what's below.  
Call Before you dig.



CONSTRUCTION KEY	
KEY	DESCRIPTION
CP	CONC PAVT WITH INTEGRAL CURB, NONREINF, 8 INCH
CG	CURB AND GUTTER, CONC, DET F4, SPECIAL
APP	HMA APPROACH
SR6	CURB RAMP, CONCRETE, 6 INCH, P-NC, ADA, MODIFIED
SC4	SIDEWALK, CONCRETE, 4 INCH, SPECIAL
SC6	SIDEWALK, CONCRETE, 6 INCH, SPECIAL
DWT	DETECTABLE WARNING TILES
HMA	HMA, 4EML / 4EL / 3C
DSC-B	DR STRUCTURE COVER, TYPE B
DSC-K	DR STRUCTURE COVER, TYPE K
DSC-Q	DR STRUCTURE COVER, TYPE Q
ABO	ADJUST BY OTHERS
DSA	DR STRUCTURE COVER, ADJ, CASE 1
MBA	ADJUST MONUMENT BOX OR VALVE BOX
DOM	DRIVEWAY OPENING, CONC, DET M, P-NC
DC-6	PLACE CONC, DRIVE APPROACH, NON-REINFORCED, P-NC, 6 INCH. PLACE AND COMPACT 6 INCH SUBBASE, CIP, OR AGGREGATE BASE, 6 INCH

**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**

**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**

ROAD PLAN & PROFILE

STA. 5+50 - STA. 9+50

**811**  
Know what's below.  
Call Before you dig.

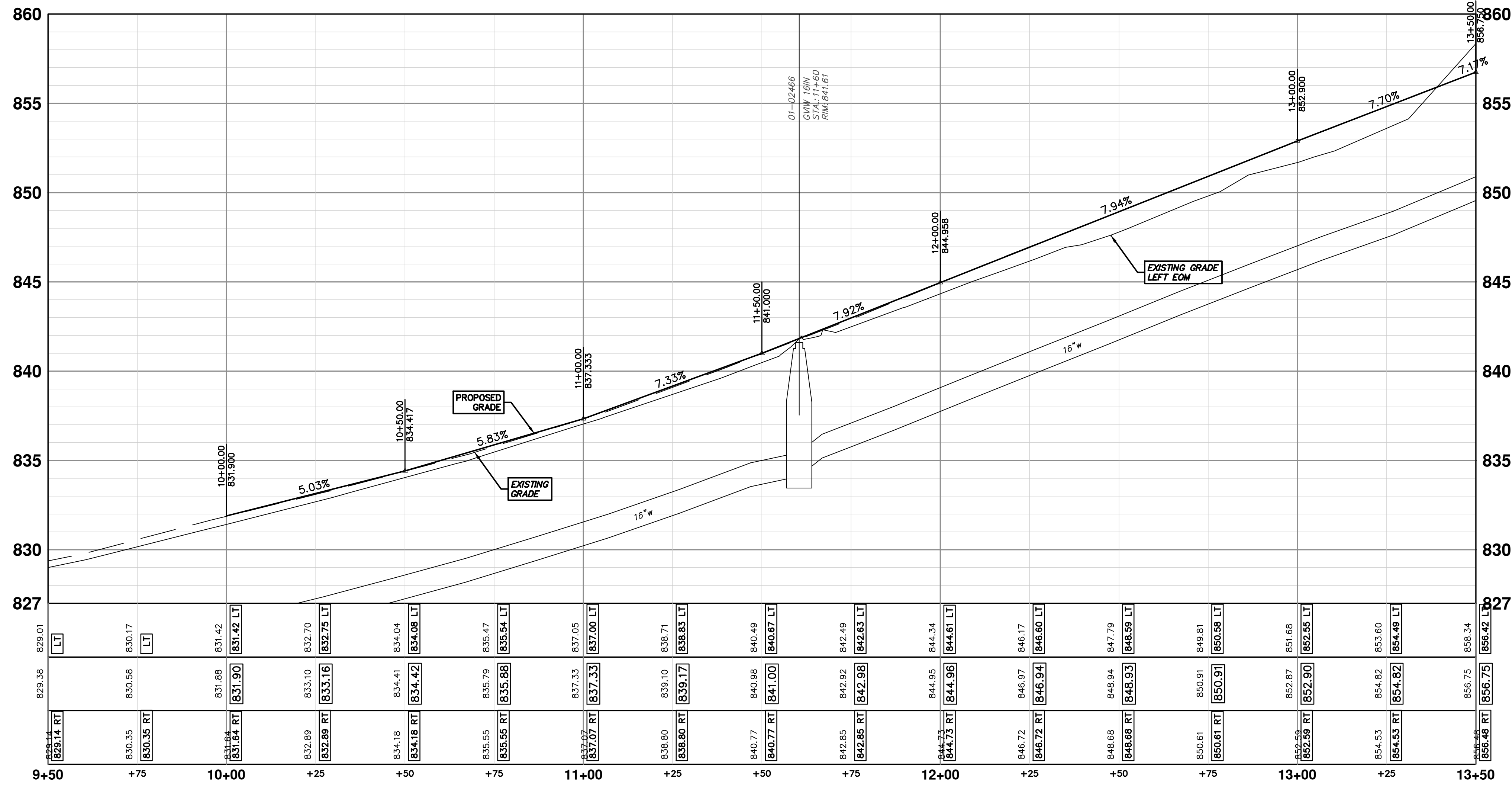
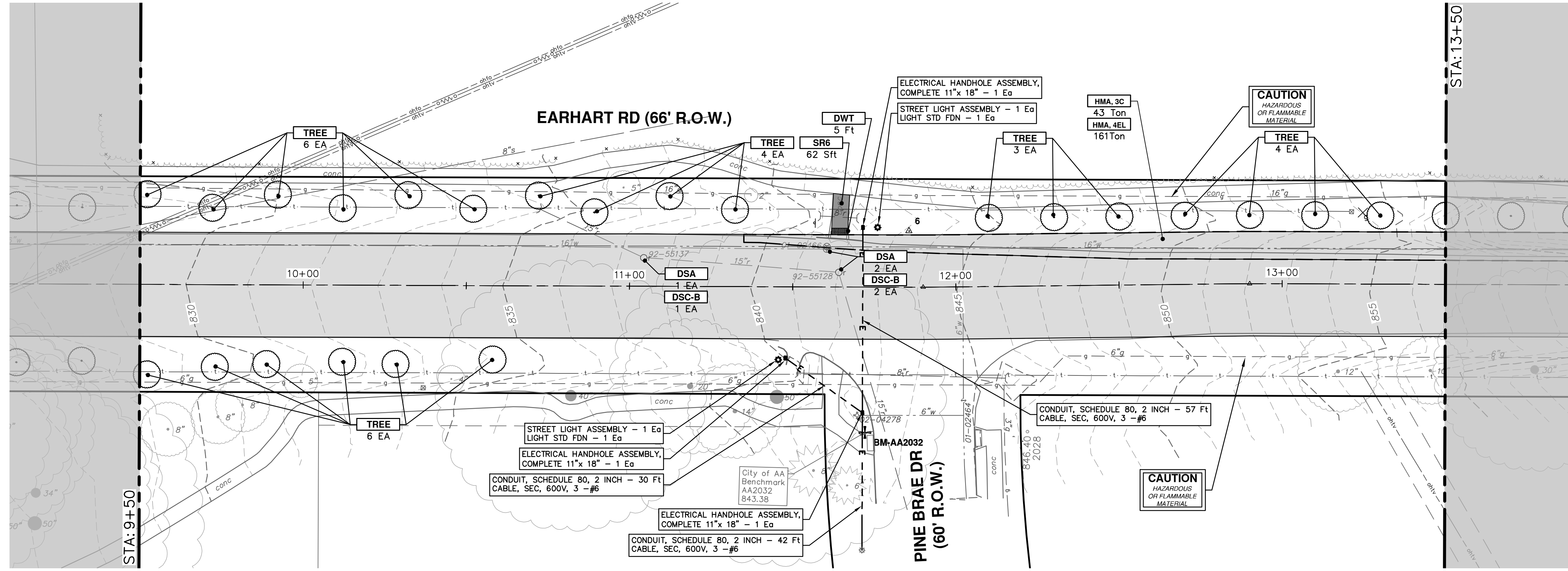
REV.	DESCRIPTION	DATE	DRAWN	CHECKED
00	OUT TO BID	4-27-23	CC/DF	NB

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SCALE PLAN: 1" = 20'  
PROFILE: 1" = 2'

DRAWING NO. **2021023-34**

SHEET NO. **34 OF 74**



CONSTRUCTION KEY	
KEY	DESCRIPTION
CP	CONC PAVT WITH INTEGRAL CURB, NONREINF, 8 INCH
CG	CURB AND GUTTER, CONC, DET F4, SPECIAL
APP	HMA APPROACH
SR6	CURB RAMP, CONCRETE, 6 INCH, P-NC, ADA, MODIFIED
SC4	SIDEWALK, CONCRETE, 4 INCH, SPECIAL
SC6	SIDEWALK, CONCRETE, 6 INCH, SPECIAL
DWT	DETECTABLE WARNING TILES
HMA	HMA, 4EML / 4EL / 3C
DSC-B	DR STRUCTURE COVER, TYPE B
DSC-K	DR STRUCTURE COVER, TYPE K
DSC-Q	DR STRUCTURE COVER, TYPE Q
ABO	ADJUST BY OTHERS
DSA	DR STRUCTURE COVER, ADJ. CASE 1
MBA	ADJUST MONUMENT BOX OR VALVE BOX
DOM	DRIVEWAY OPENING, CONC, DET M, P-NC
DC-6	PLACE CONC, DRIVE APPROACH, NON-REINFORCED, P-NC, 6 INCH. PLACE AND COMPACT 6 INCH SUBBASE, CIP, OR AGGREGATE BASE, 6 INCH

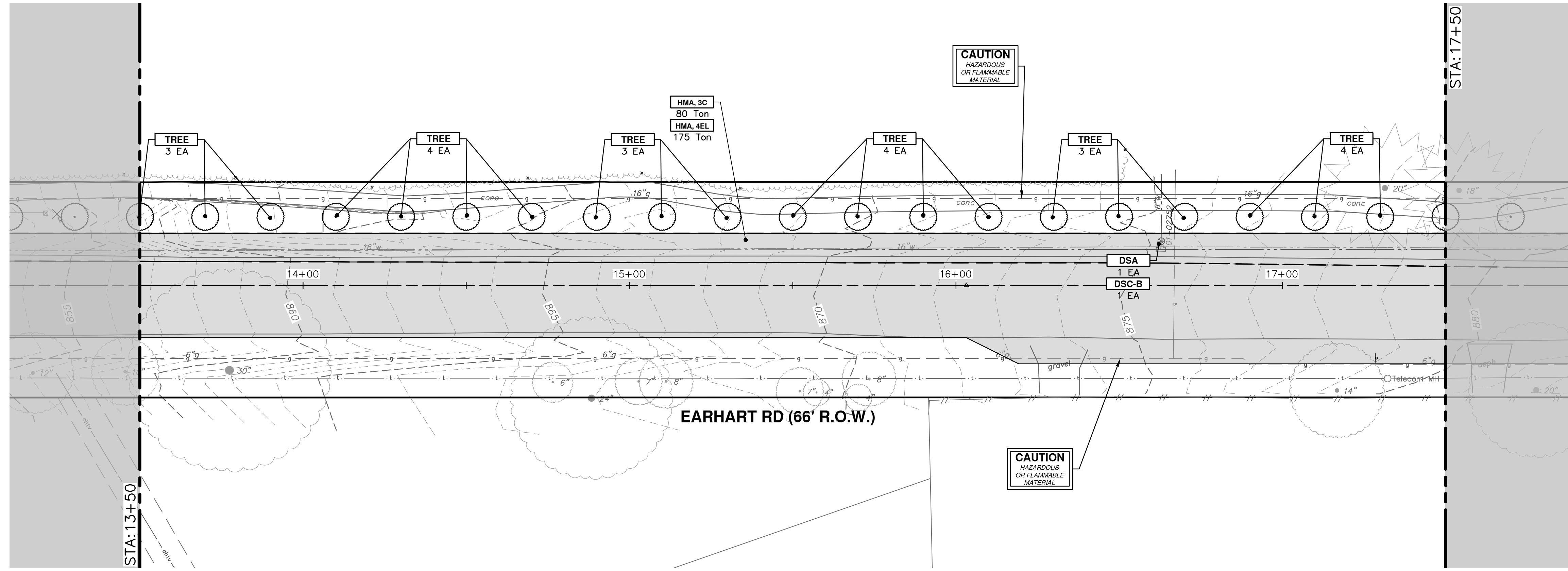


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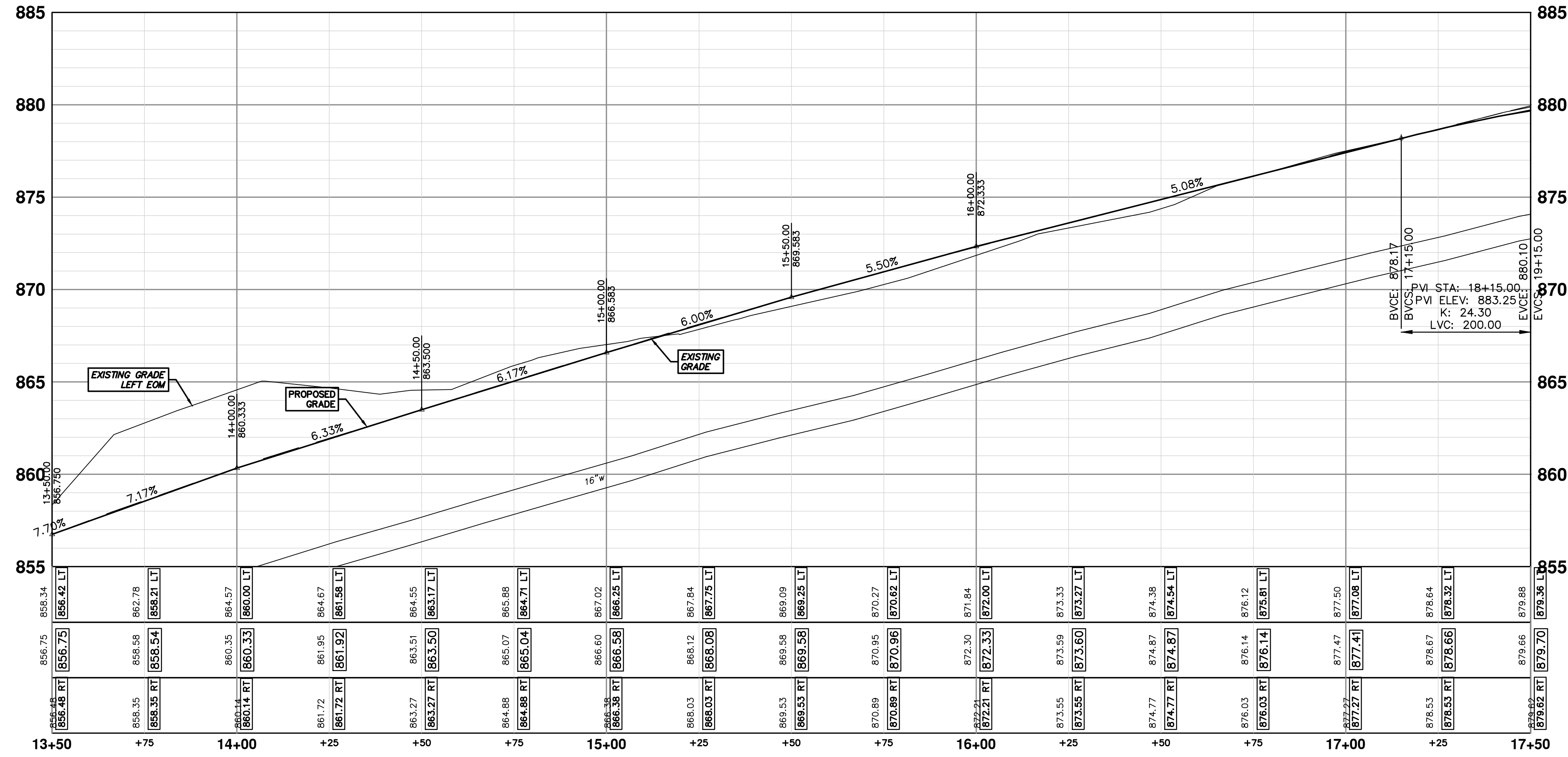


CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING  
EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
ROAD PLAN & PROFILE  
STA. 9+50 - STA. 13+50

SHEET No. 35 OF 74  
SCALE PLAN: 1" = 20'  
PROFILE: 1" = 2'  
DRAWING No. 2021023-35



CONSTRUCTION KEY	
KEY	DESCRIPTION
CP	CONC PAVT WITH INTEGRAL CURB, NONREINF, 8 INCH
CG	CURB AND GUTTER, CONC, DET F4, SPECIAL
APP	HMA APPROACH
SR6	CURB RAMP, CONCRETE, 6 INCH, P-NC, ADA, MODIFIED
SC4	SIDEWALK, CONCRETE, 4 INCH, SPECIAL
SC6	SIDEWALK, CONCRETE, 6 INCH, SPECIAL
DWT	DETECTABLE WARNING TILES
HMA	HMA, 4EML / 4EL / 3C
DSC-B	DR STRUCTURE COVER, TYPE B
DSC-K	DR STRUCTURE COVER, TYPE K
DSC-Q	DR STRUCTURE COVER, TYPE Q
ABO	ADJUST BY OTHERS
DSA	DR STRUCTURE COVER, ADJ. CASE 1
MBA	ADJUST MONUMENT BOX OR VALVE BOX
DOM	DRIVEWAY OPENING, CONC, DET M, P-NC
DC-6	PLACE CONC, DRIVE APPROACH, NON-REINFORCED, P-NC, 6 INCH. PLACE AND COMPACT 6 INCH SUBBASE, CIP, OR AGGREGATE BASE, 6 INCH

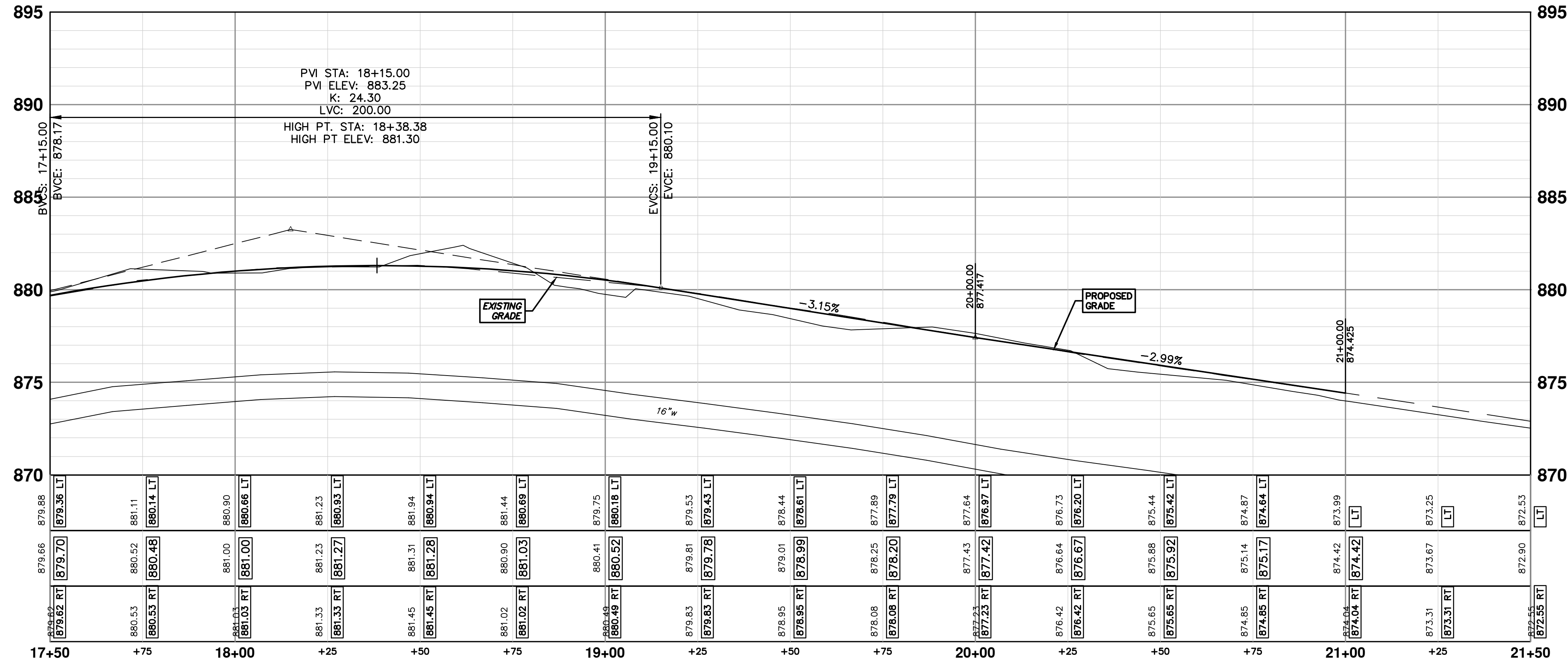
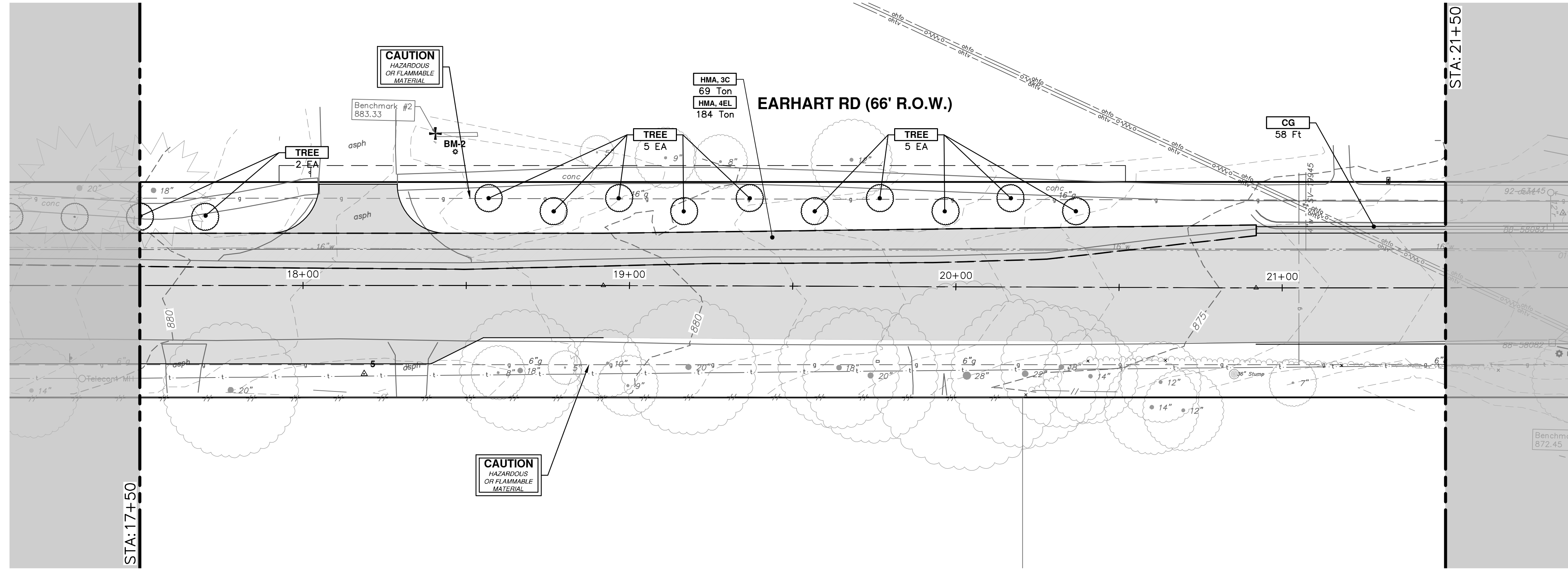


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CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING  
EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
ROAD PLAN & PROFILE  
STA. 13+50 - STA. 17+50

SCALE PLAN: 1" = 20'  
PROFILE: 1" = 2'  
DRAWING No. 2021023-36



CONSTRUCTION KEY	
KEY	DESCRIPTION
CP	CONC PAVT WITH INTEGRAL CURB, NONREINF, 8 INCH
CG	CURB AND GUTTER, CONC, DET F4, SPECIAL
APP	HMA APPROACH
SR6	CURB RAMP, CONCRETE, 6 INCH, P-NC, ADA, MODIFIED
SC4	SIDEWALK, CONCRETE, 4 INCH, SPECIAL
SC6	SIDEWALK, CONCRETE, 6 INCH, SPECIAL
DWT	DETECTABLE WARNING TILES
HMA	HMA, 4EML / 4EL / 3C
DSC-B	DR STRUCTURE COVER, TYPE B
DSC-K	DR STRUCTURE COVER, TYPE K
DSC-Q	DR STRUCTURE COVER, TYPE Q
ABO	ADJUST BY OTHERS
DSA	DR STRUCTURE COVER, ADJ. CASE 1
MBA	ADJUST MONUMENT BOX OR VALVE BOX
DOM	DRIVEWAY OPENING, CONC, DET M, P-NC
DC-6	PLACE CONC, DRIVE APPROACH, NON-REINFORCED, P-NC, 6 INCH. PLACE AND COMPACT 6 INCH SUBBASE, CIP, OR AGGREGATE BASE, 6 INCH



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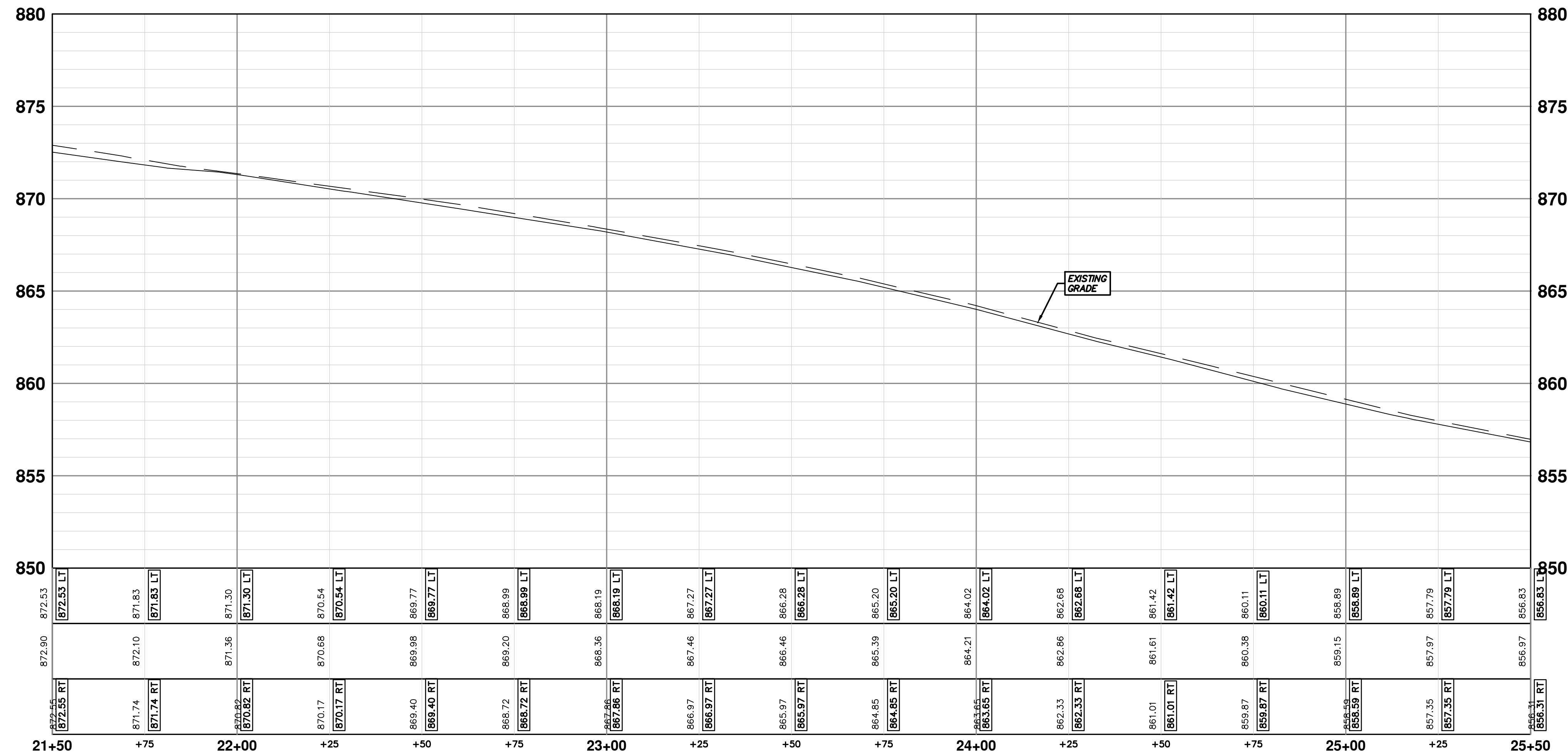
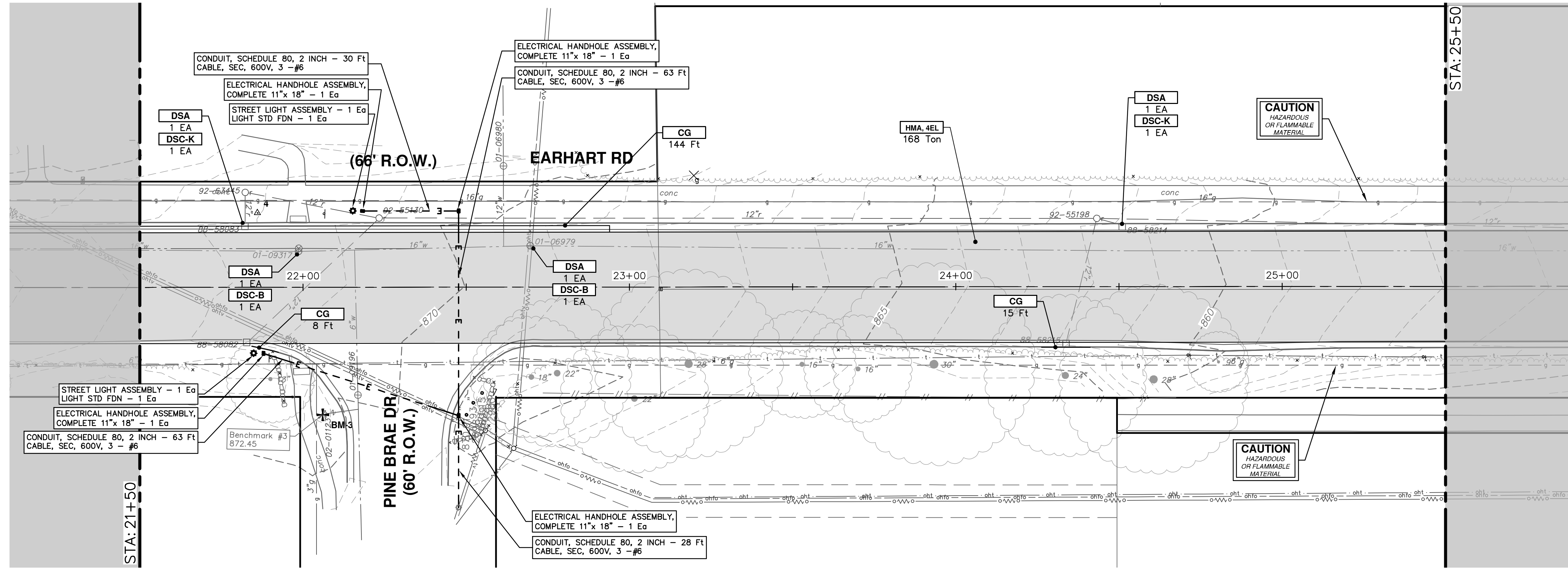


CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING  
EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
ROAD PLAN & PROFILE  
STA. 17+50 - STA. 21+50

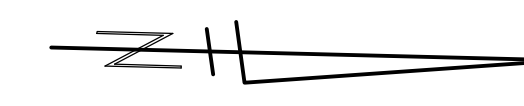
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PROFILE: 1" = 2'  
DRAWING No. 2021023-37

REV.	DESCRIPTION	DATE	DRAWN	CHECKED
00	OUT TO BID	4-27-23		

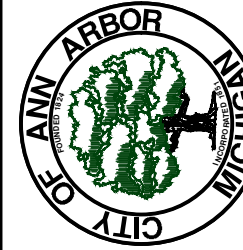
R:\2021023 Earhart (Geddes to US23)\Plan Production\2021023Prd.dwg Dwg Created: 27-Apr-23 - \_a2 standard bw.stb - Plot Date: 27-Apr-23



CONSTRUCTION KEY	
KEY	DESCRIPTION
CP	CONC PAVT WITH INTEGRAL CURB, NONREINF, 8 INCH
CG	CURB AND GUTTER, CONC, DET F4, SPECIAL
APP	HMA APPROACH
SR6	CURB RAMP, CONCRETE, 6 INCH, P-NC, ADA, MODIFIED
SC4	SIDEWALK, CONCRETE, 4 INCH, SPECIAL
SC6	SIDEWALK, CONCRETE, 6 INCH, SPECIAL
DWT	DETECTABLE WARNING TILES
HMA	HMA, 4EML / 4EL / 3C
DSC-B	DR STRUCTURE COVER, TYPE B
DSC-K	DR STRUCTURE COVER, TYPE K
DSC-Q	DR STRUCTURE COVER, TYPE Q
ABO	ADJUST BY OTHERS
DSA	DR STRUCTURE COVER, ADJ, CASE 1
MBA	ADJUST MONUMENT BOX OR VALVE BOX
DOM	DRIVEWAY OPENING, CONC, DET M, P-NC
DC-6	PLACE CONC, DRIVE APPROACH, NON-REINFORCED, P-NC, 6 INCH. PLACE AND COMPACT 6 INCH SUBBASE, CIP, OR AGGREGATE BASE, 6 INCH



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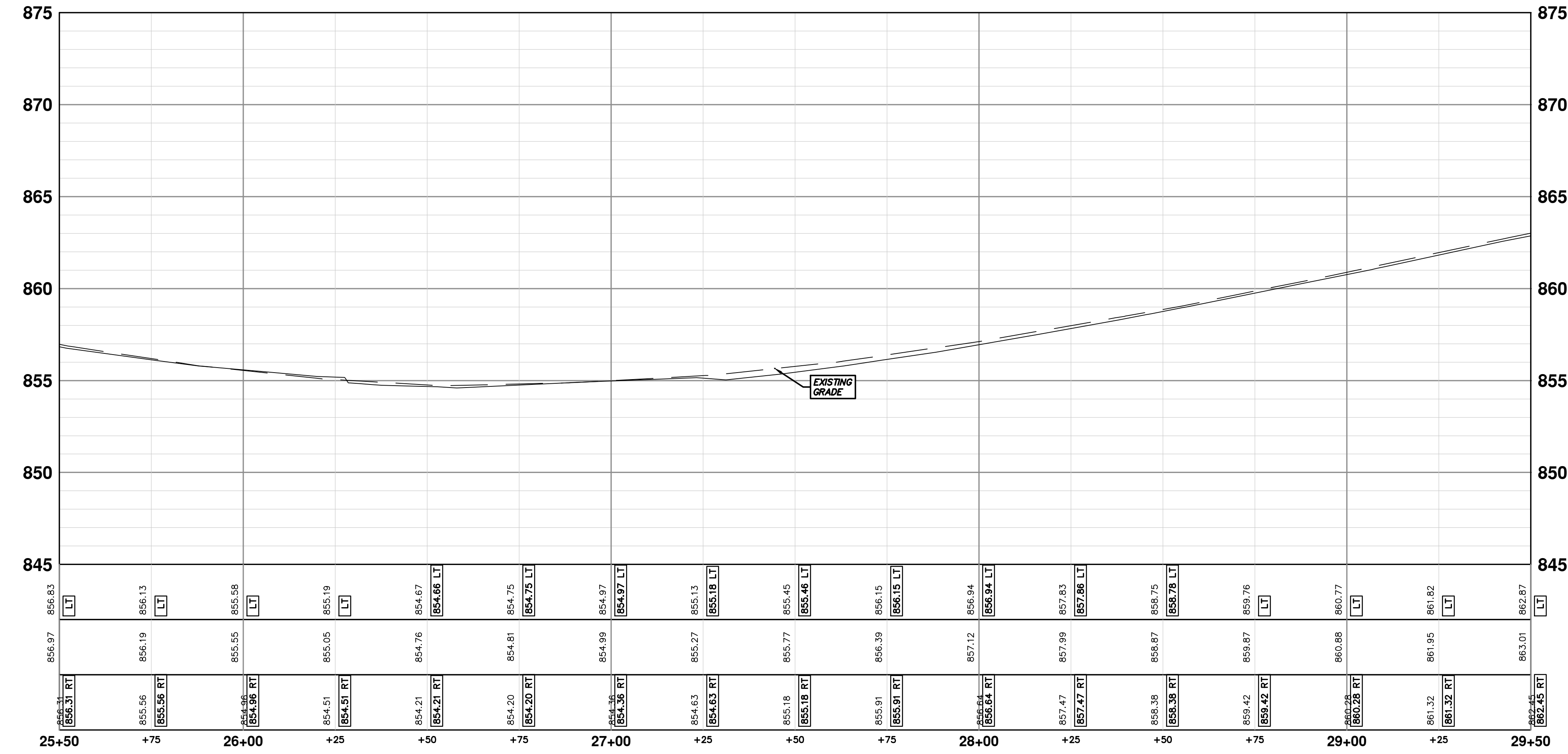
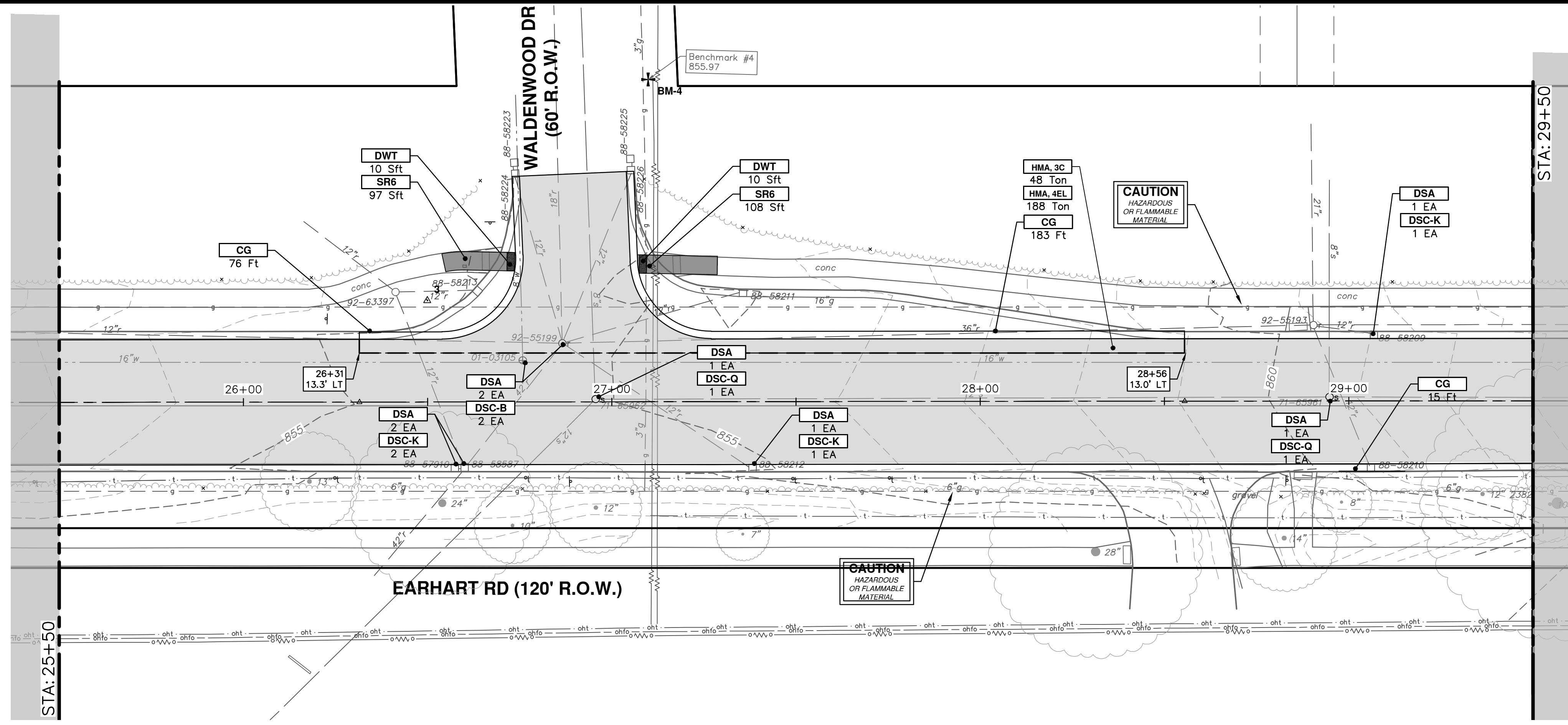


CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING  
EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
ROAD PLAN & PROFILE  
STA. 21+50 - STA. 25+50

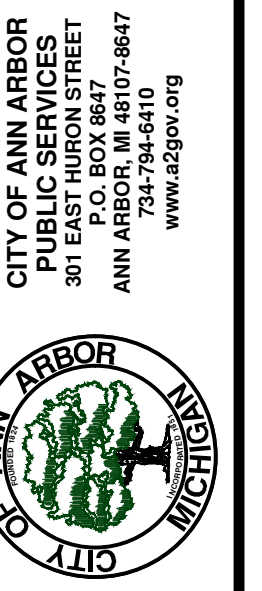
SHEET No. 38 OF 74  
SCALE PLAN: 1" = 20'  
PROFILE: 1" = 2'  
DRAWING No. 2021023-38

NO	REV.	DESCRIPTION	DATE	DRAWN	CHECKED
00		OUT TO BID	4-27-23		

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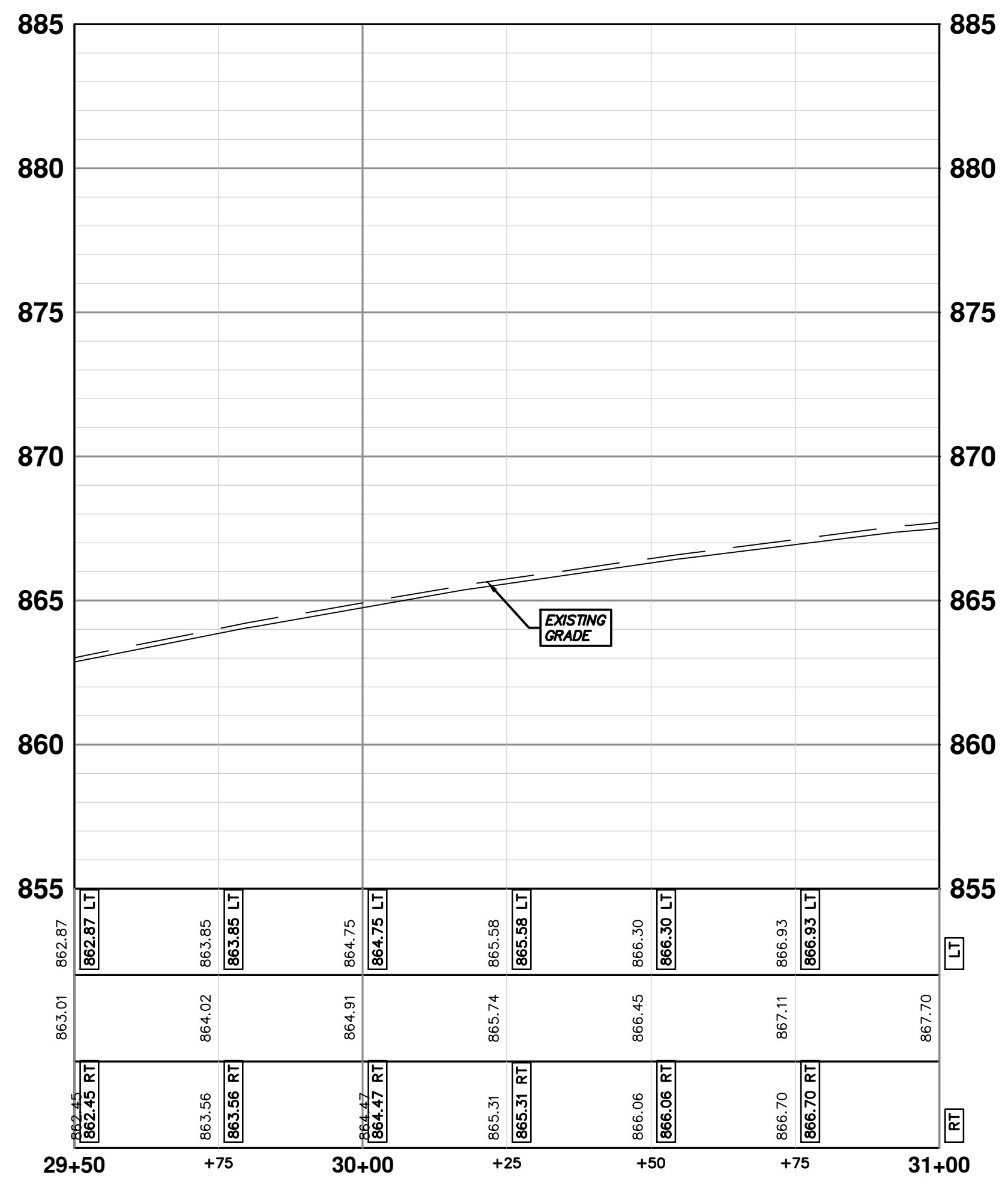
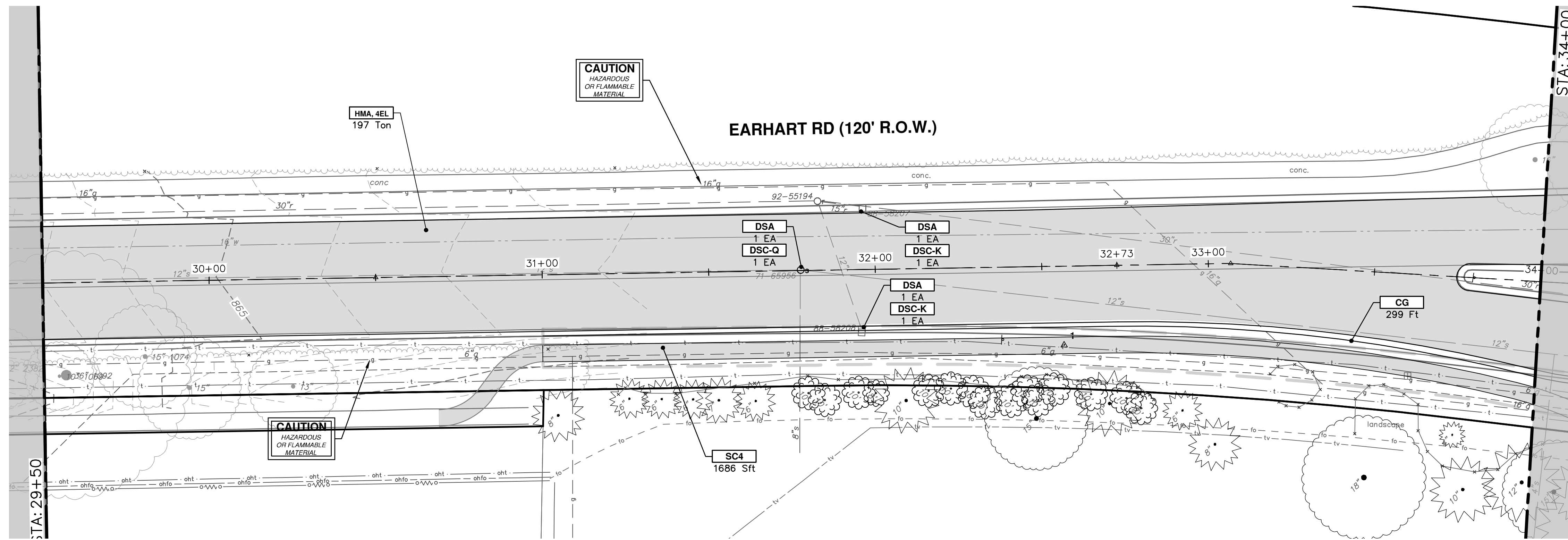


CONSTRUCTION KEY	
KEY	DESCRIPTION
CP	CONC PAVT WITH INTEGRAL CURB, NONREINF, 8 INCH
CG	CURB AND GUTTER, CONC, DET F4, SPECIAL
APP	HMA APPROACH
SR6	CURB RAMP, CONCRETE, 6 INCH, P-NC, ADA, MODIFIED
SC4	SIDEWALK, CONCRETE, 4 INCH, SPECIAL
SC6	SIDEWALK, CONCRETE, 6 INCH, SPECIAL
DWT	DETECTABLE WARNING TILES
HMA	HMA, 4EML / 4EL / 3C
DSC-B	DR STRUCTURE COVER, TYPE B
DSC-K	DR STRUCTURE COVER, TYPE K
DSC-Q	DR STRUCTURE COVER, TYPE Q
ABO	ADJUST BY OTHERS
DSA	DR STRUCTURE COVER, ADJ. CASE 1
MBA	ADJUST MONUMENT BOX OR VALVE BOX
DOM	DRIVEWAY OPENING, CONC, DET M, P-NC
DC-6	PLACE CONC, DRIVE APPROACH, NON-REINFORCED, P-NC, 6 INCH. PLACE AND COMPACT 6 INCH SUBBASE, CIP, OR AGGREGATE BASE, 6 INCH



CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING  
 ROAD PLAN & PROFILE  
 STA. 25+50 - STA. 29+50  
 SHEET No. 39 OF 74  
 SCALE PLAN: 1" = 20' PROFILE: 1" = 2'  
 DRAWING No. 2021023-39

REV.	DESCRIPTION	DATE	DRAWN	CHECKED
00	OUT TO BID	4-27-23		



CONSTRUCTION KEY	
KEY	DESCRIPTION
CP	CONC PAVT WITH INTEGRAL CURB, NONREINF, 8 INCH
CG	CURB AND GUTTER, CONC, DET F4, SPECIAL
APP	HMA APPROACH
SR6	CURB RAMP, CONCRETE, 6 INCH, P-NC, ADA, MODIFIED
SC4	SIDEWALK, CONCRETE, 4 INCH, SPECIAL
SC6	SIDEWALK, CONCRETE, 6 INCH, SPECIAL
DWT	DETECTABLE WARNING TILES
HMA	HMA, 4EML / 4EL / 3C
DSC-B	DR STRUCTURE COVER, TYPE B
DSC-K	DR STRUCTURE COVER, TYPE K
DSC-Q	DR STRUCTURE COVER, TYPE Q
ABO	ADJUST BY OTHERS
DSA	DR STRUCTURE COVER, ADJ, CASE 1
MBA	ADJUST MONUMENT BOX OR VALVE BOX
DOM	DRIVEWAY OPENING, CONC, DET M, P-NC
DC-6	PLACE CONC, DRIVE APPROACH, NON-REINFORCED, P-NC, 6 INCH. PLACE AND COMPACT 6 INCH SUBBASE, CIP, OR AGGREGATE BASE, 6 INCH



**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**

**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**

ROAD PLAN & PROFILE

STA. 29+50 - STA. 34+00

SHEET No. **40 OF 74**

SCALE PLAN: 1" = 20'  
PROFILE: 1" = 2'

DRAWING No. **2021023-40**

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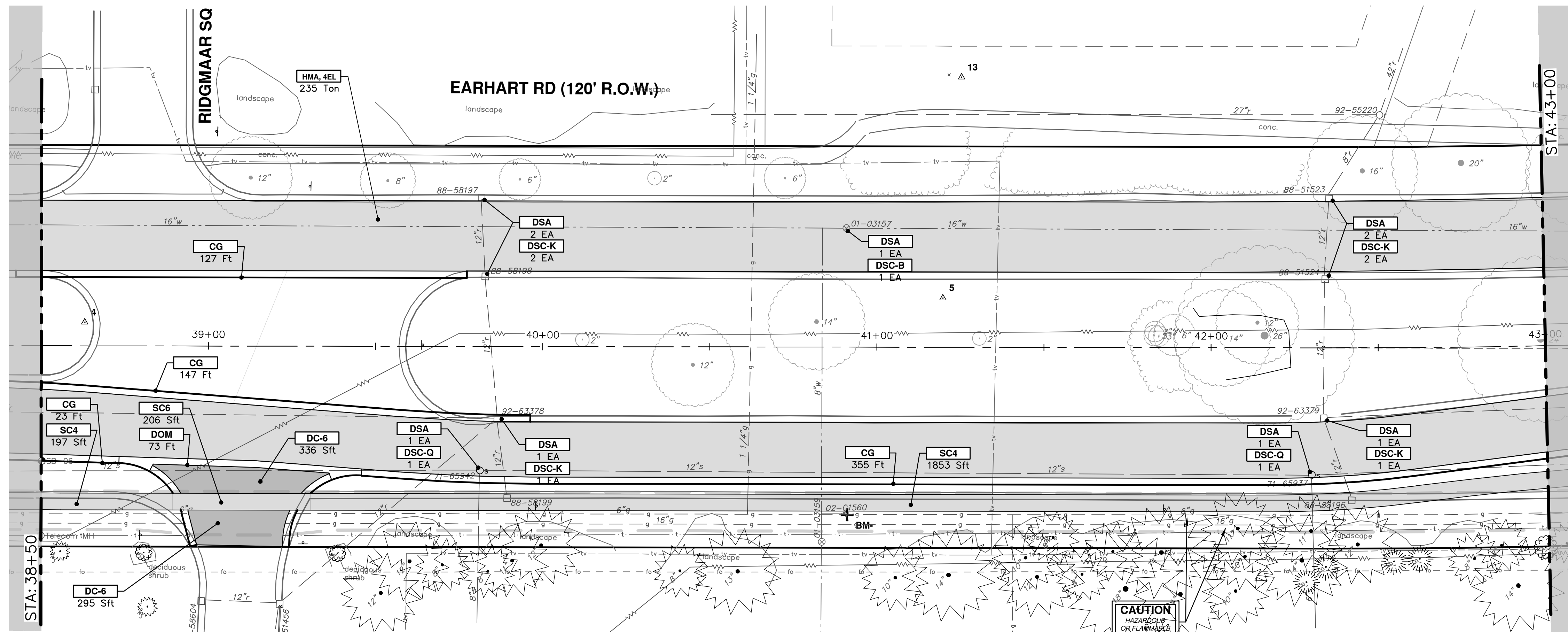
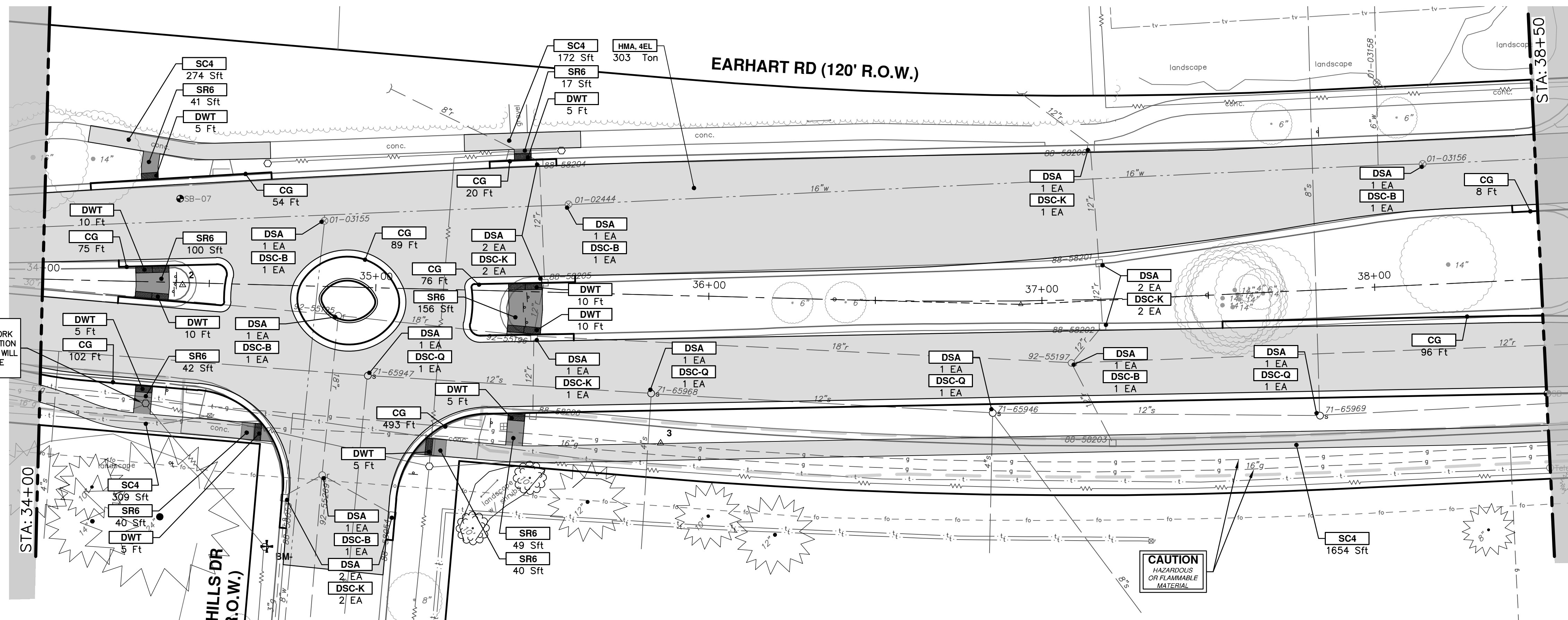
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ANN ARBOR MICHIGAN

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LIGHT POLE TO BE RELOCATED BY OTHERS. THE CONTRACTOR SHALL COORDINATE ALL WORK WITH DTE ENERGY SO THAT THEIR CONSTRUCTION OPERATIONS ARE NOT IMPACTED. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE RELATED ITEMS OF WORK.



**CONSTRUCTION KEY**

KEY	DESCRIPTION
CP	CONC PAVT WITH INTEGRAL CURB, NONREIN, 8 INCH
CG	CURB AND GUTTER, CONC, DET F4, SPECIAL
APP	HMA APPROACH
SR6	CURB RAMP, CONCRETE, 6 INCH, P-NC, ADA, MODIFIED
SC4	SIDEWALK, CONCRETE, 4 INCH, SPECIAL
SC6	SIDEWALK, CONCRETE, 6 INCH, SPECIAL
DWT	DETECTABLE WARNING TILES
HMA	HMA, 4EML / 4EL / 3C
DSC-B	DR STRUCTURE COVER, TYPE B
DSC-K	DR STRUCTURE COVER, TYPE K
DSC-Q	DR STRUCTURE COVER, TYPE Q
ABO	ADJUST BY OTHERS
DSA	DR STRUCTURE COVER, ADJ, CASE 1
MBA	ADJUST MONUMENT BOX OR VALVE BOX
DOM	DRIVEWAY OPENING, CONC, DET M, P-NC
DC-6	PLACE CONC, DRIVE APPROACH, NON-REINFORCED, P-NC, 6 INCH. PLACE AND COMPACT 6 INCH SUBBASE, CP, OR AGGREGATE BASE, 6 INCH



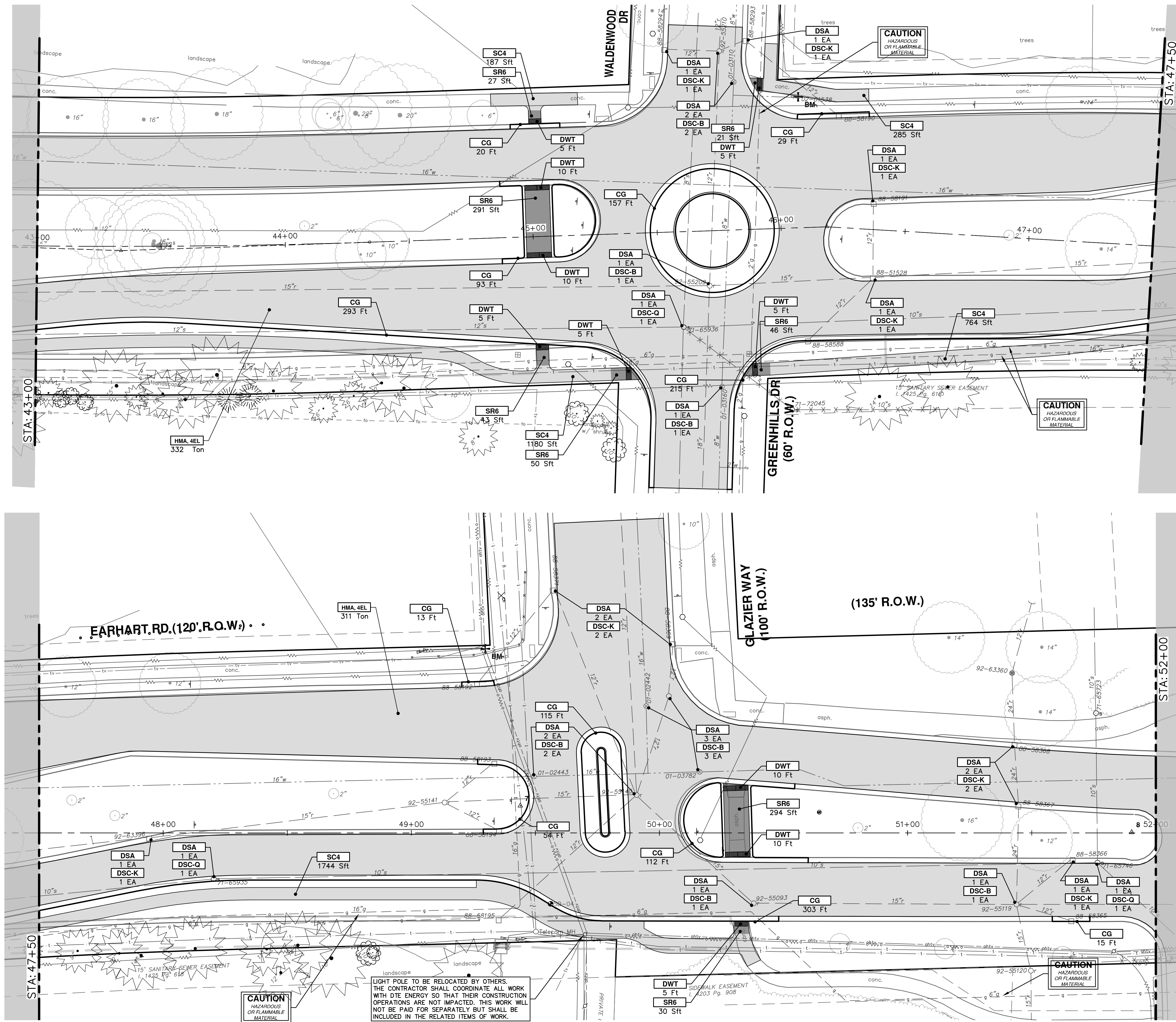
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EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
ROAD PLAN & PROFILE  
STA. 34+00 - STA. 43+00

SHEET No. 41 OF 74  
DRAWING No. 2021023-41

R:\2021023 Earhart (Geddes to US23)\Plan Production\2021023Prd1.dwg Dwg Created: 27-Apr-23 - Plot Date: 27-Apr-23



CONSTRUCTION KEY	
KEY	DESCRIPTION
CP	CONC PAVT WITH INTEGRAL CURB, NONREINF, 8 INCH
CG	CURB AND GUTTER, CONC, DET F4, SPECIAL
APP	HMA APPROACH
SR6	CURB RAMP, CONCRETE, 6 INCH, P-NC, ADA, MODIFIED
SC4	SIDEWALK, CONCRETE, 4 INCH, SPECIAL
SC6	SIDEWALK, CONCRETE, 6 INCH, SPECIAL
DWT	DETECTABLE WARNING TILES
HMA	HMA, 4EML / 4EL / 3C
DSC-B	DR STRUCTURE COVER, TYPE B
DSC-K	DR STRUCTURE COVER, TYPE K
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DC-6	PLACE CONC, DRIVE APPROACH, NON-REINFORCED, P-NC, 6 INCH. PLACE AND COMPACT 6 INCH SUBBASE, CIP, OR AGGREGATE BASE, 6 INCH

**CAUTION**  
HAZARDOUS  
OR FLAMMABLE  
MATERIAL

LIGHT POLE TO BE RELOCATED BY OTHERS. THE CONTRACTOR SHALL COORDINATE ALL WORK WITH DTE ENERGY SO THAT THEIR CONSTRUCTION OPERATIONS ARE NOT IMPACTED. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE RELATED ITEMS OF WORK.



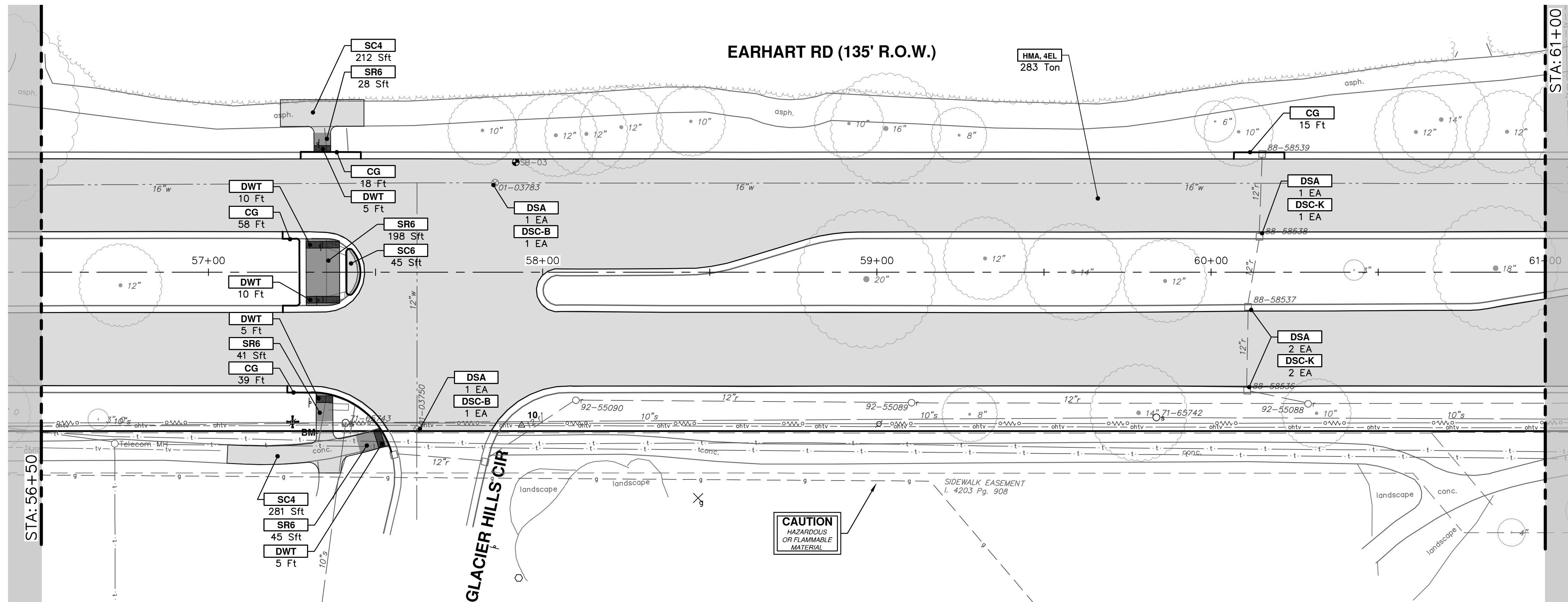
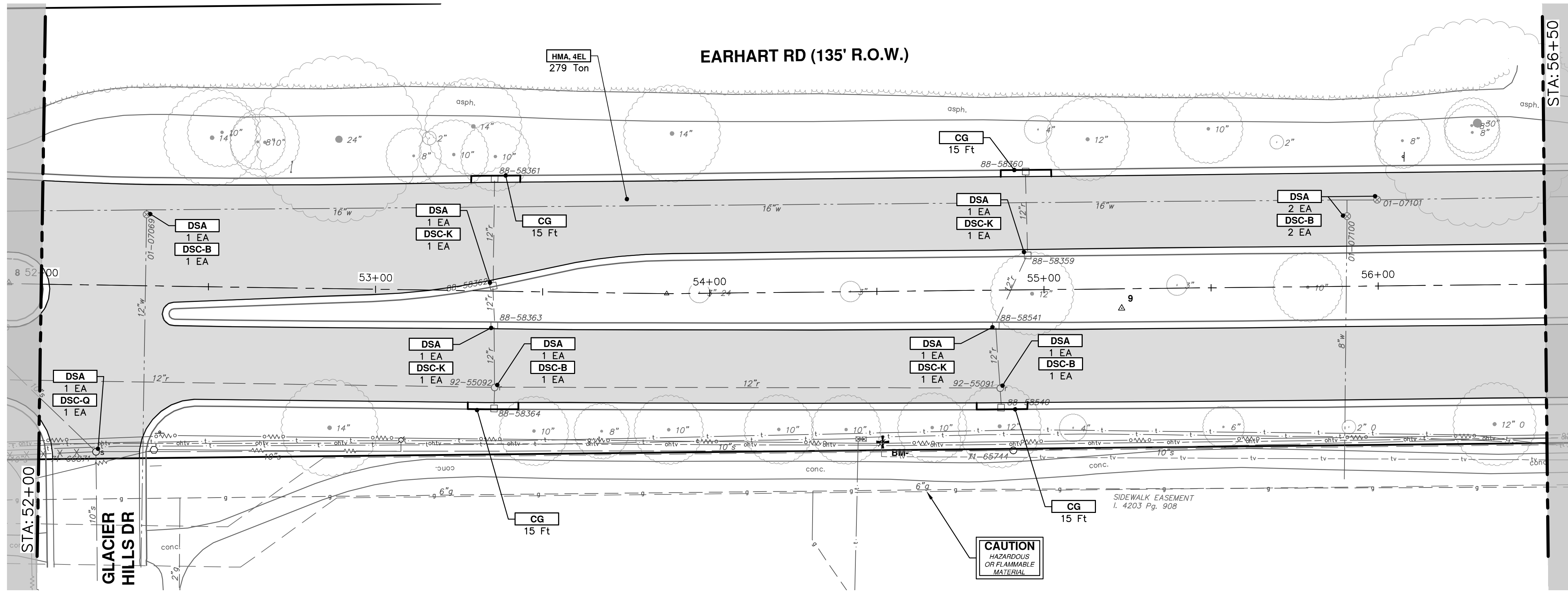
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1	4-27-23	DRAWN	CC/DF
2		CHECKED	NB

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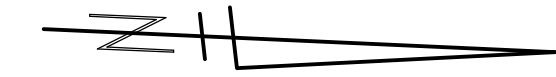
**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - U523)**  
ROAD PLAN & PROFILE  
STA. 43+00 - STA. 52+00

SCALE: 1" = 20'  
DRAWING No. 2021023-42  
SHEET No. 42 OF 74

R:\2021023 Earhart (Geddes to US23)\Plan Production\2021023Prd1.dwg Dwg Created: 27-Apr-23 -- \_a2 standard bw.stb - Plot Date: 27-Apr-23



CONSTRUCTION KEY	
KEY	DESCRIPTION
CP	CONC PAVT WITH INTEGRAL CURB, NONREINF, 8 INCH
CG	CURB AND GUTTER, CONC, DET F4, SPECIAL
APP	HMA APPROACH
SR6	CURB RAMP, CONCRETE, 6 INCH, P-NC, ADA, MODIFIED
SC4	SIDEWALK, CONCRETE, 4 INCH, SPECIAL
SC6	SIDEWALK, CONCRETE, 6 INCH, SPECIAL
DWT	DETECTABLE WARNING TILES
HMA	HMA, 4EML / 4EL / 3C
DSC-B	DR STRUCTURE COVER, TYPE B
DSC-K	DR STRUCTURE COVER, TYPE K
DSC-Q	DR STRUCTURE COVER, TYPE Q
ABO	ADJUST BY OTHERS
DSA	DR STRUCTURE COVER, ADJ, CASE 1
MBA	ADJUST MONUMENT BOX OR VALVE BOX
DOM	DRIVEWAY OPENING, CONC, DET M, P-NC
DC-6	PLACE CONC, DRIVE APPROACH, NON-REINFORCED, P-NC, 6 INCH. PLACE AND COMPACT 6 INCH SUBBASE, CIP, OR AGGREGATE BASE, 6 INCH



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DRAWN: CC/DF  
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REV. DESCRIPTION

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DESCRIPTION

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EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
ROAD PLAN & PROFILE

STA. 52+00 - STA. 61+00

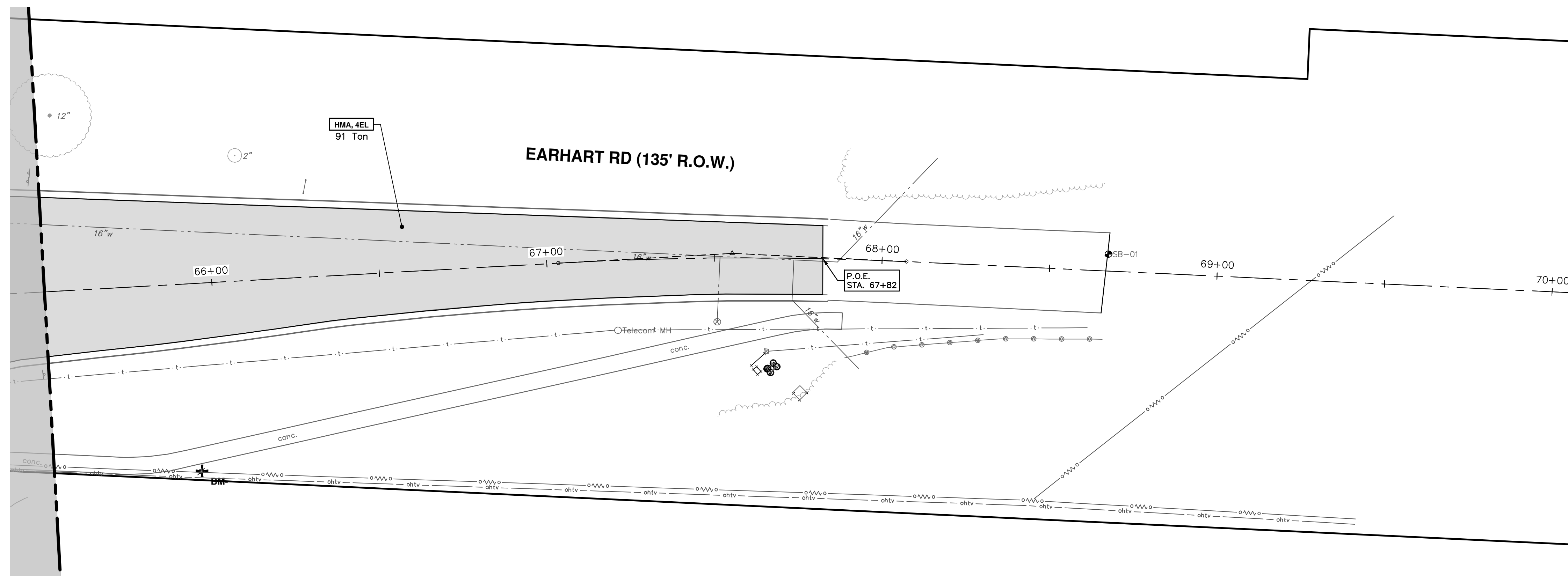
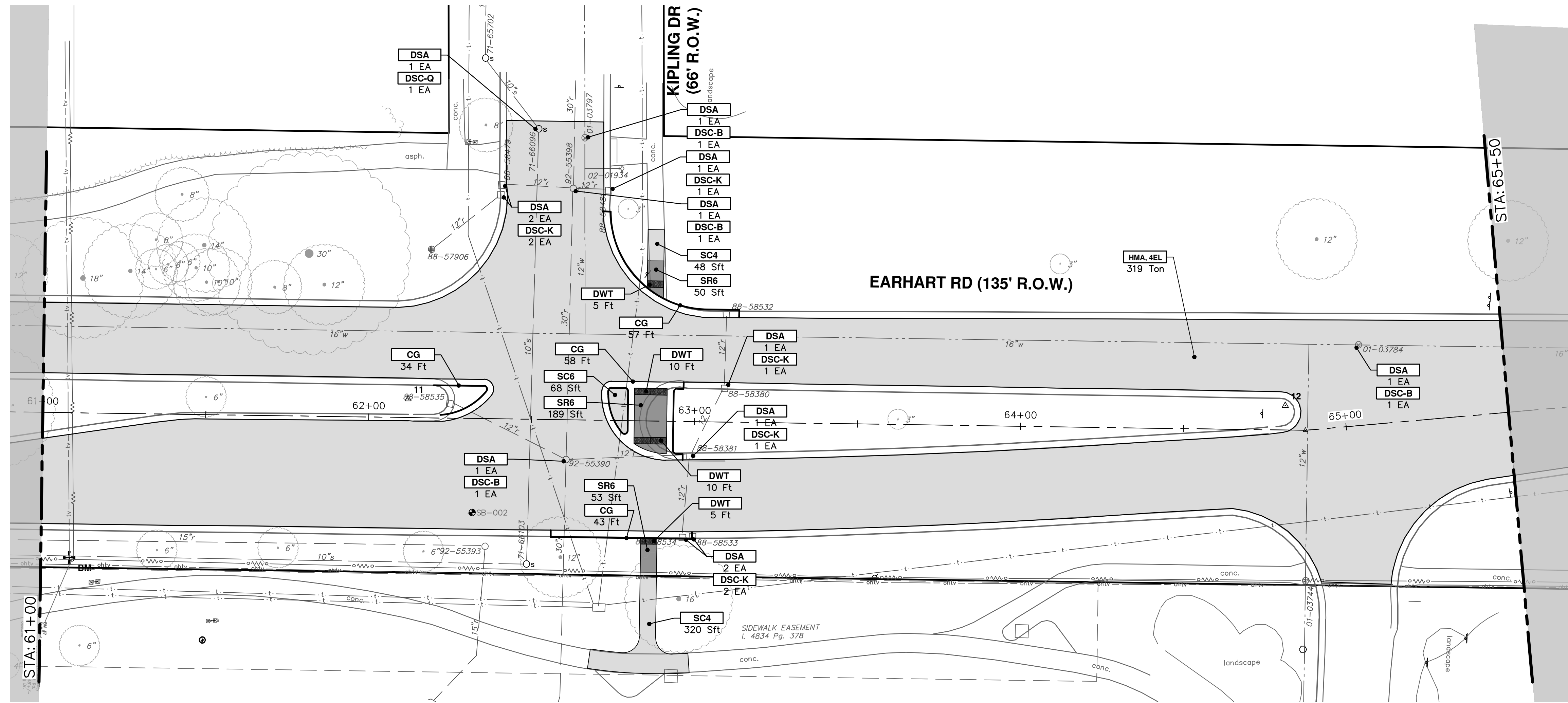
SCALE: 1" = 20'

DRAWING No. 2021023-43

SHEET No.

43 OF 74

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CONSTRUCTION KEY	
KEY	DESCRIPTION
CP	CONC PAVT WITH INTEGRAL CURB, NONREINF, 8 INCH
CG	CURB AND GUTTER, CONC, DET F4, SPECIAL
APP	HMA APPROACH
SR6	CURB RAMP, CONCRETE, 6 INCH, P-NC, ADA, MODIFIED
SC4	SIDEWALK, CONCRETE, 4 INCH, SPECIAL
SC6	SIDEWALK, CONCRETE, 6 INCH, SPECIAL
DWT	DETECTABLE WARNING TILES
HMA	HMA, 4EML / 4EL / 3C
DSC-B	DR STRUCTURE COVER, TYPE B
DSC-K	DR STRUCTURE COVER, TYPE K
DSC-Q	DR STRUCTURE COVER, TYPE Q
ABO	ADJUST BY OTHERS
DSA	DR STRUCTURE COVER, ADJ, CASE 1
MBA	ADJUST MONUMENT BOX OR VALVE BOX
DOM	DRIVEWAY OPENING, CONC, DET M, P-NC
DC-6	PLACE CONC, DRIVE APPROACH, NON-REINFORCED, P-NC, 6 INCH, PLACE AND COMPACT 6 INCH SUBBASE, CIP, OR AGGREGATE BASE, 6 INCH

**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**

**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**

ROAD PLAN & PROFILE

STA. 61+00 - STA. 67+82

SCALE: 1" = 20'

DRAWING No. 2021023-44

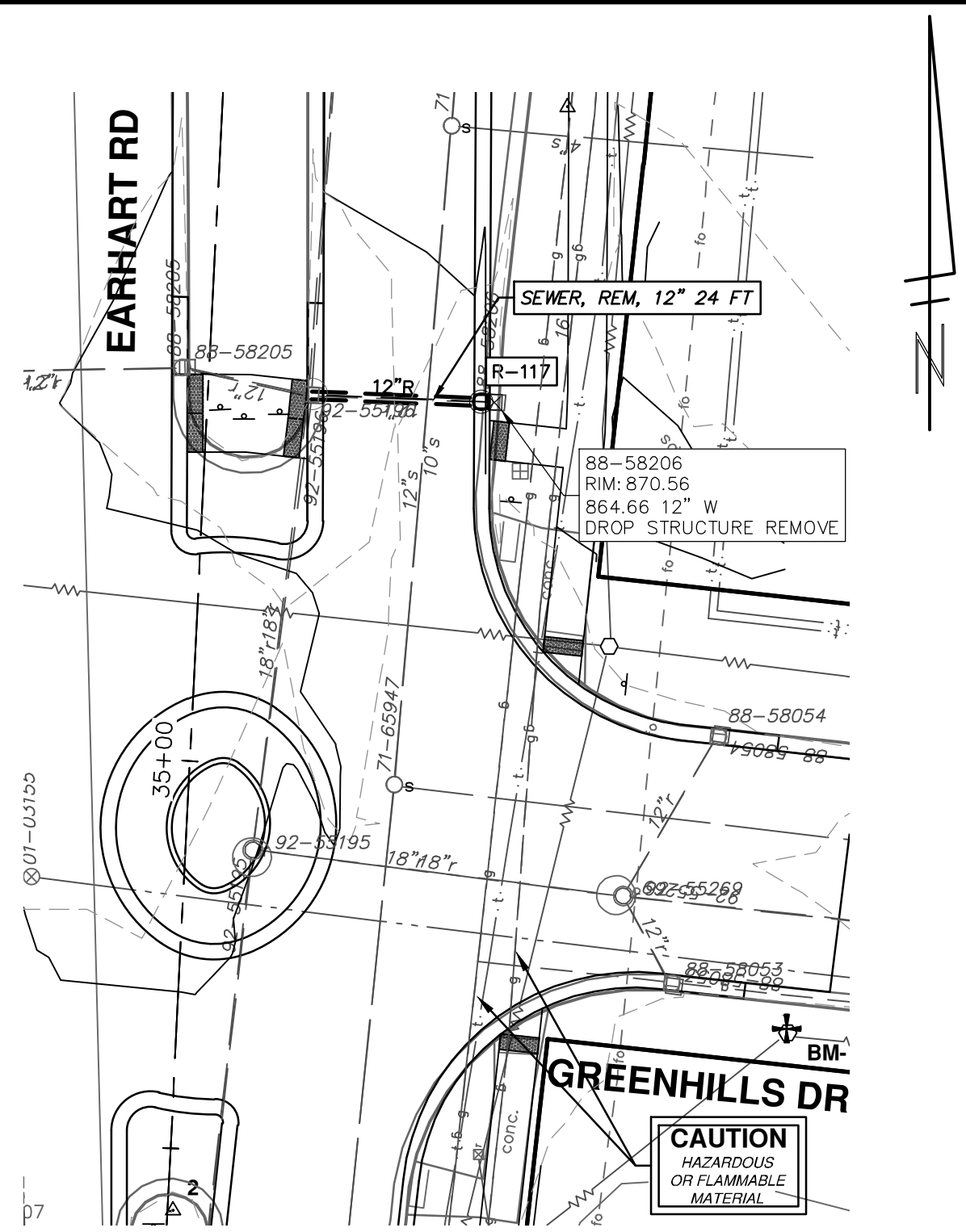
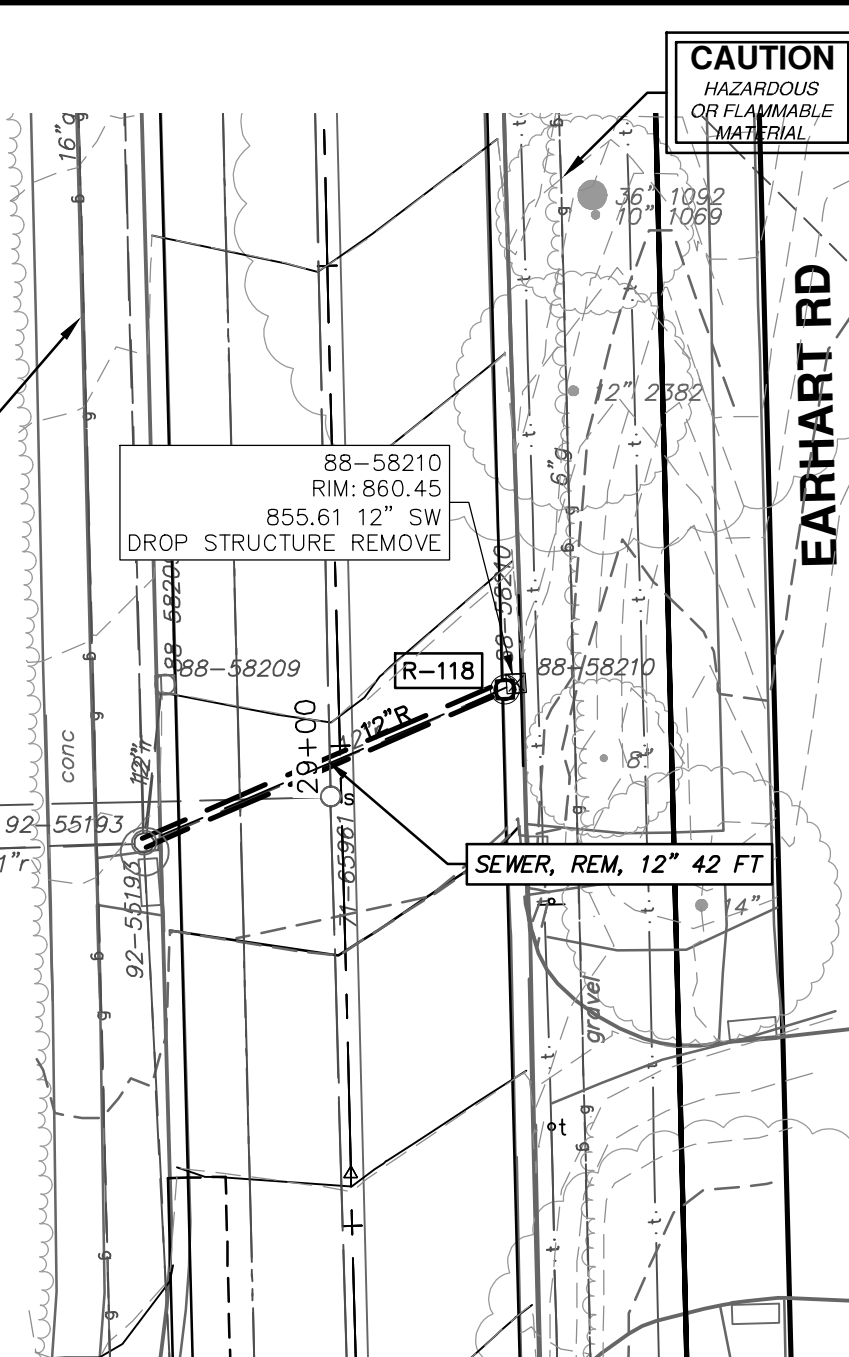
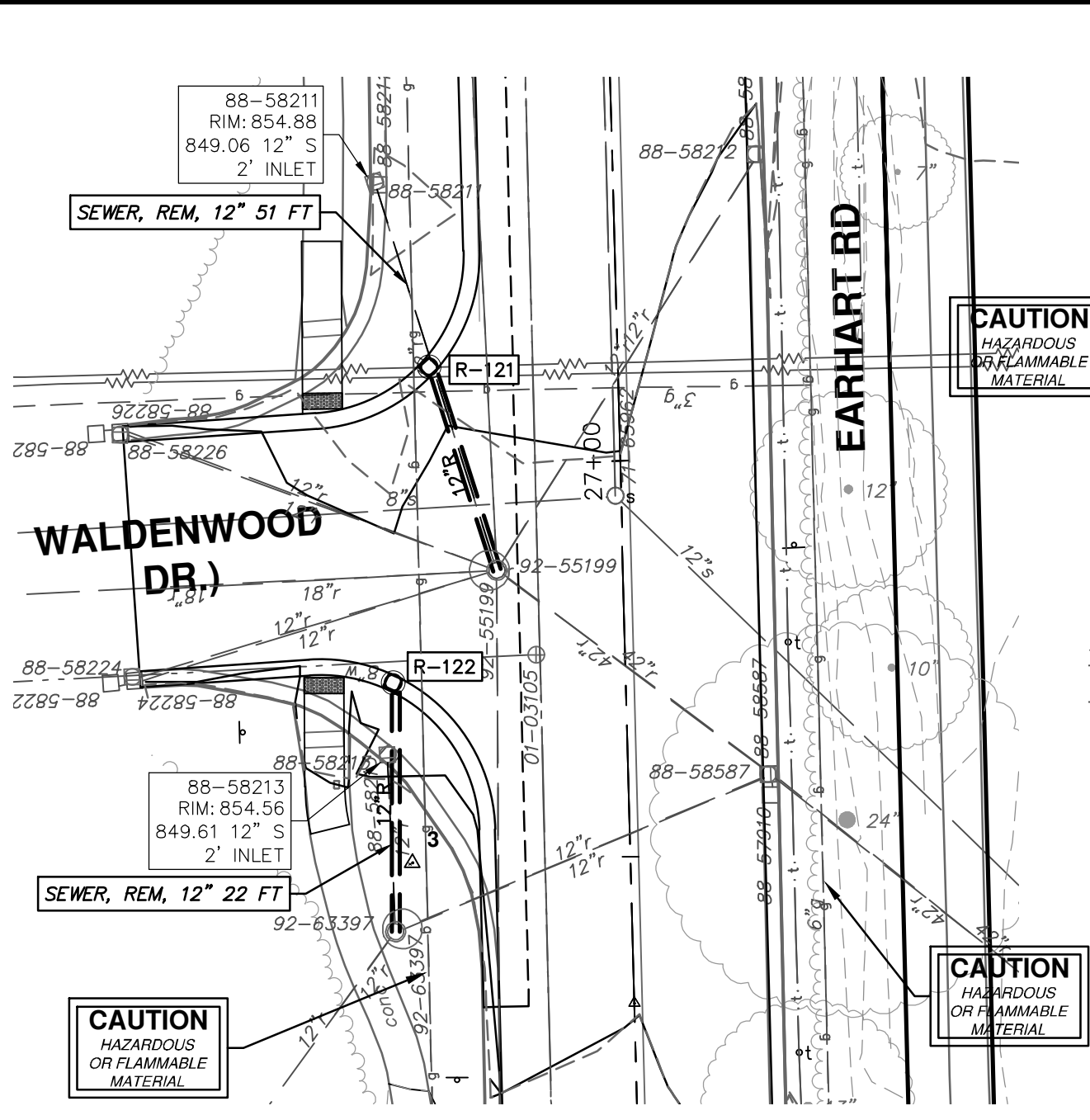
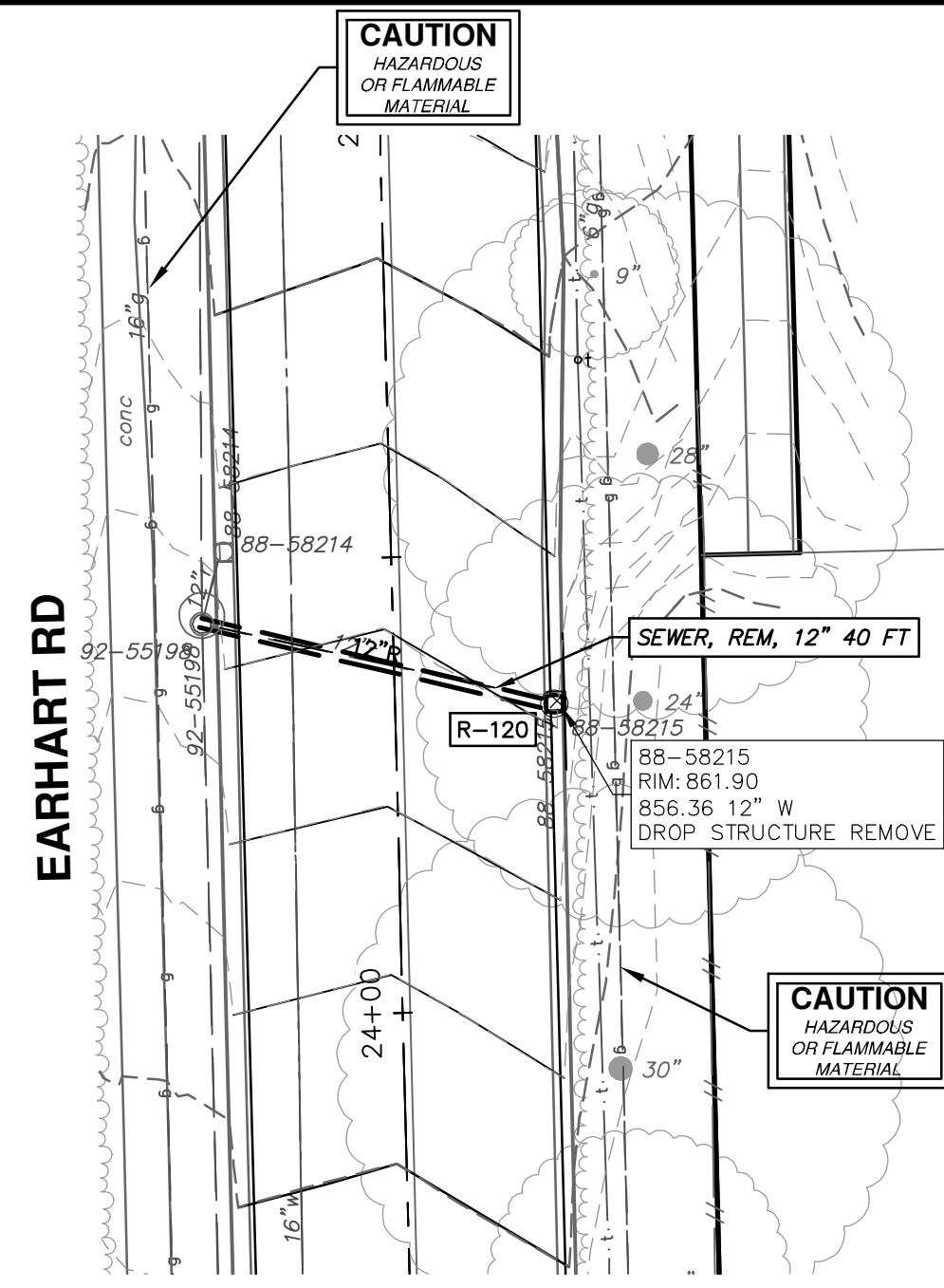
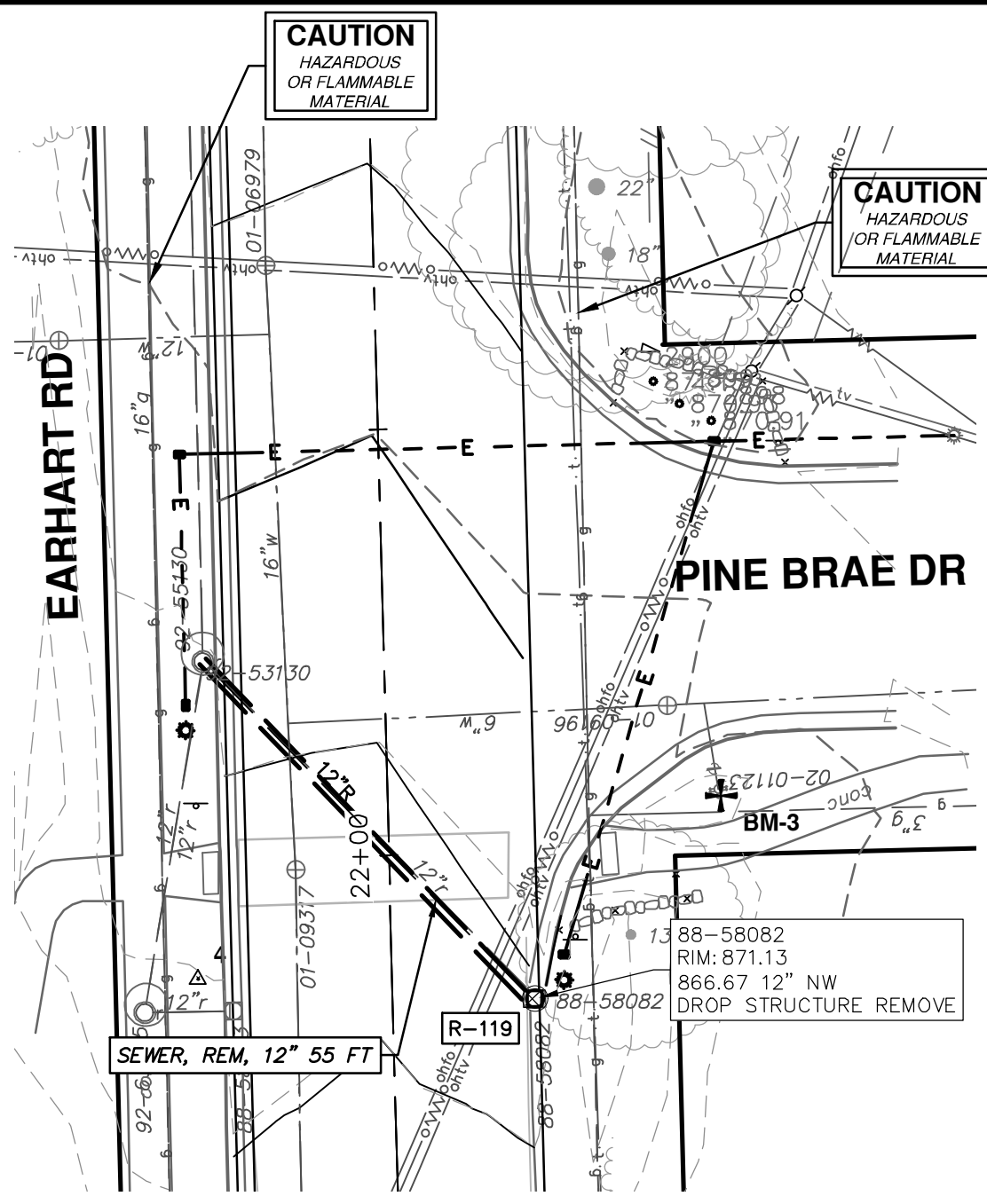
SHEET No. 44 OF 74

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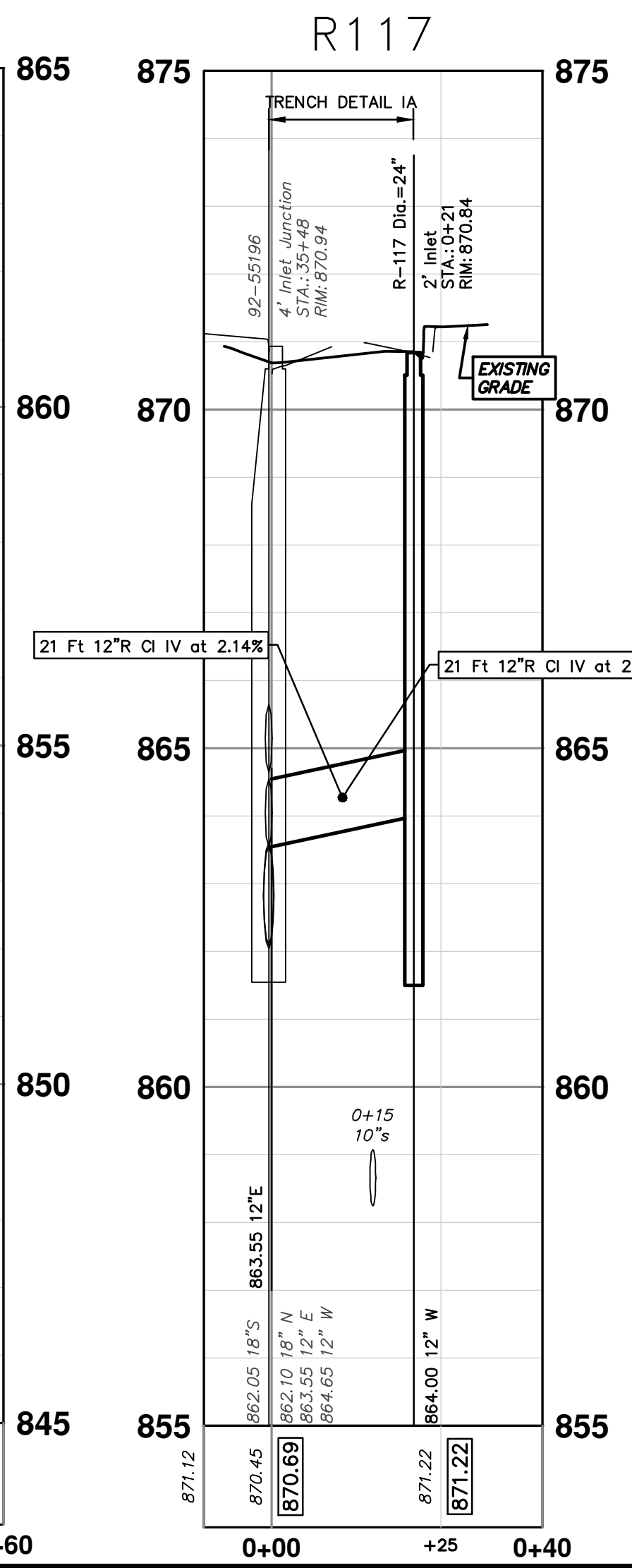
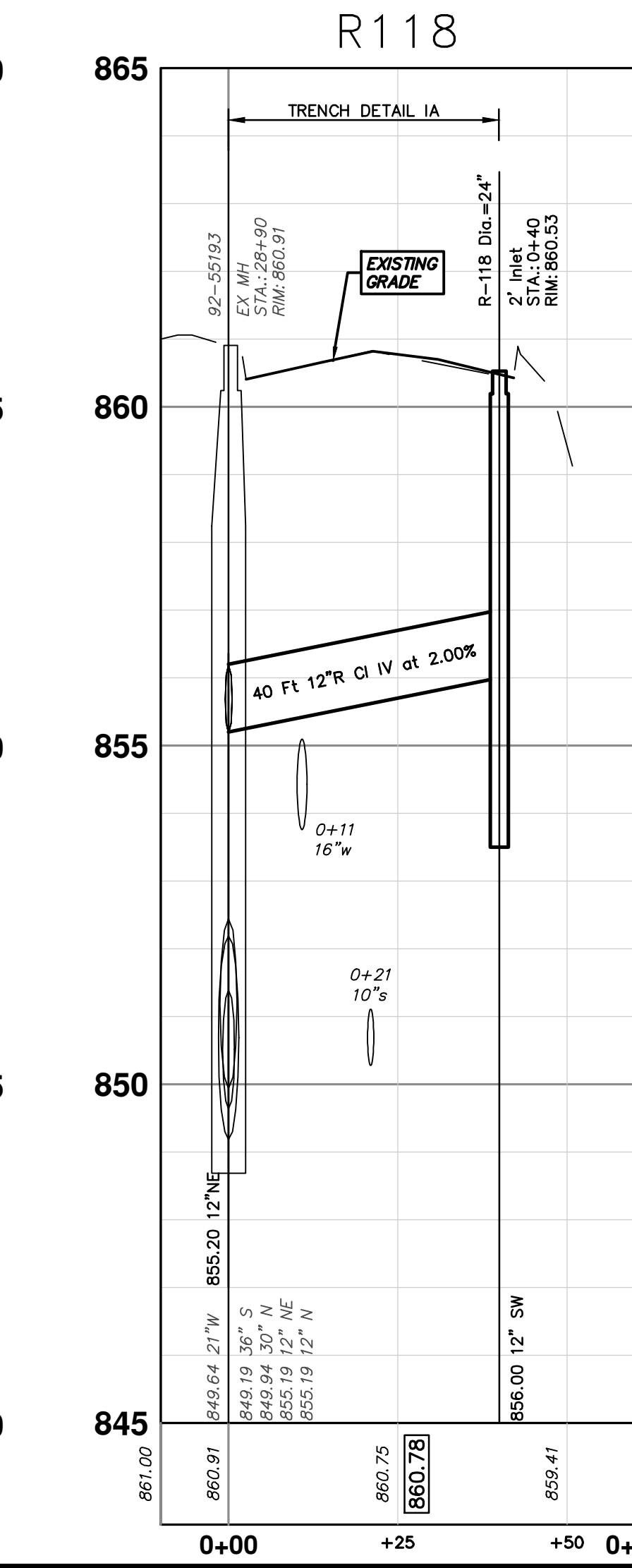
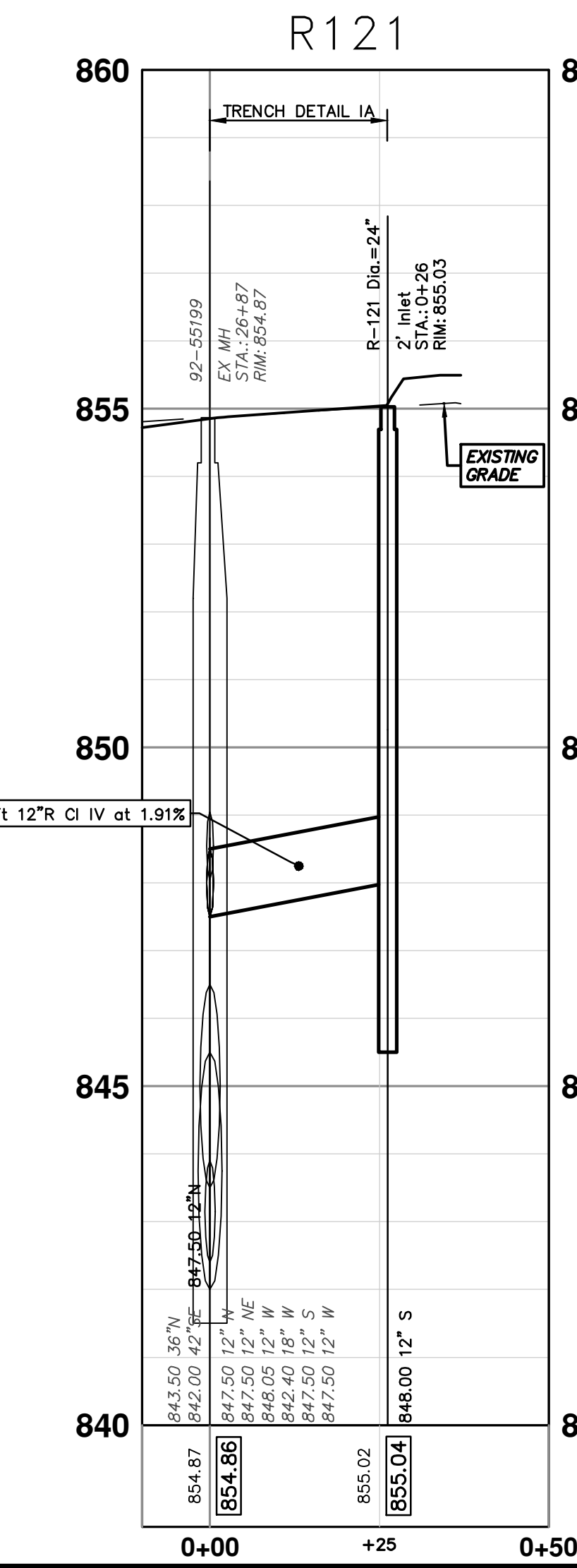
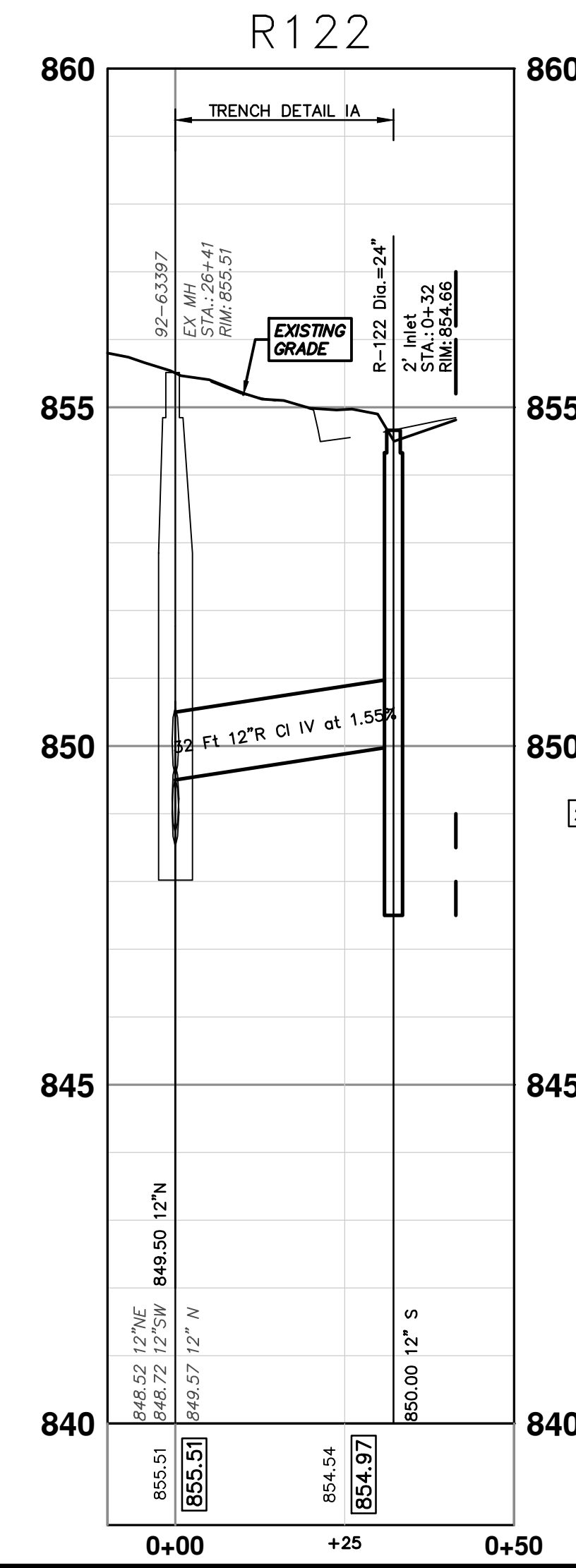
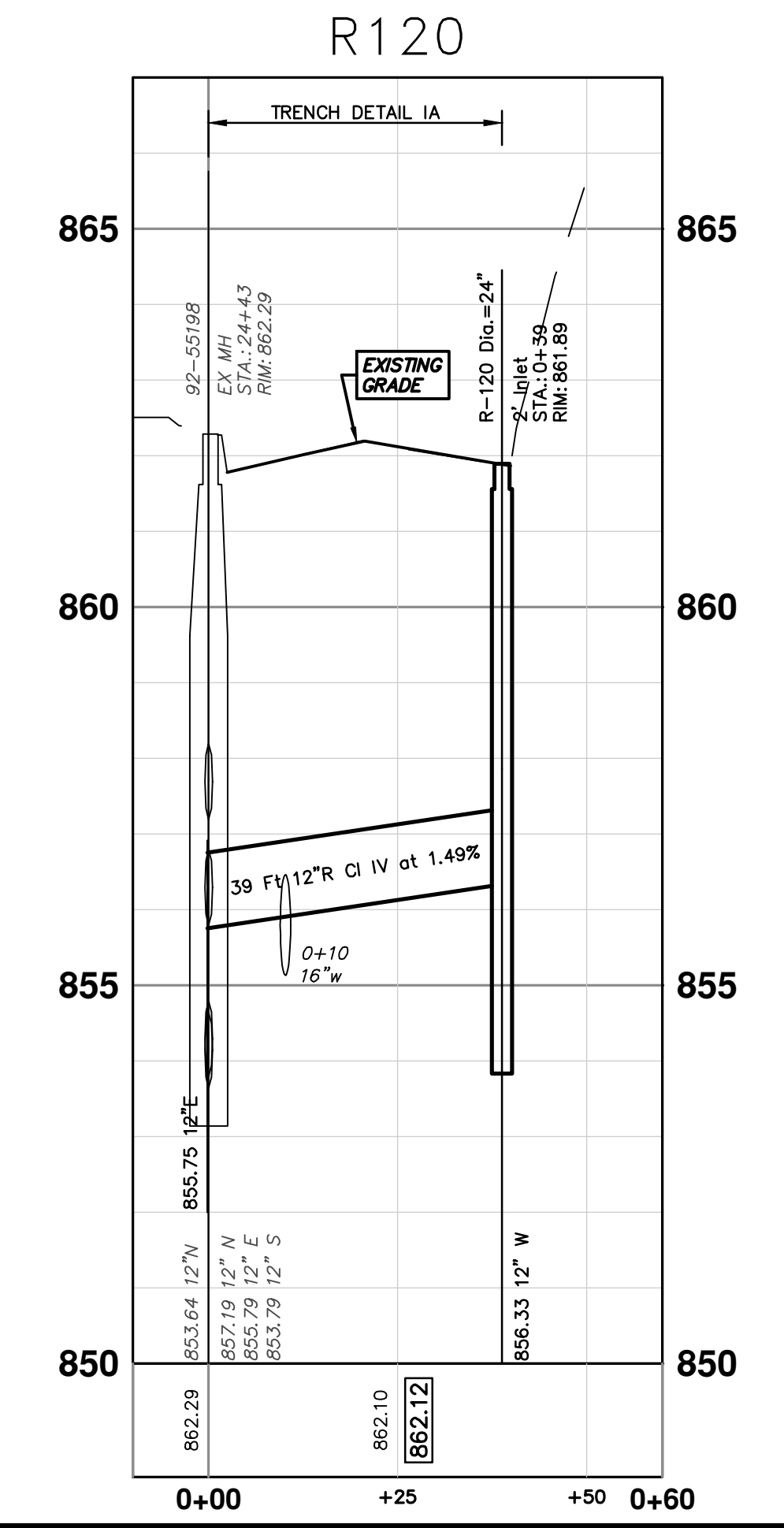
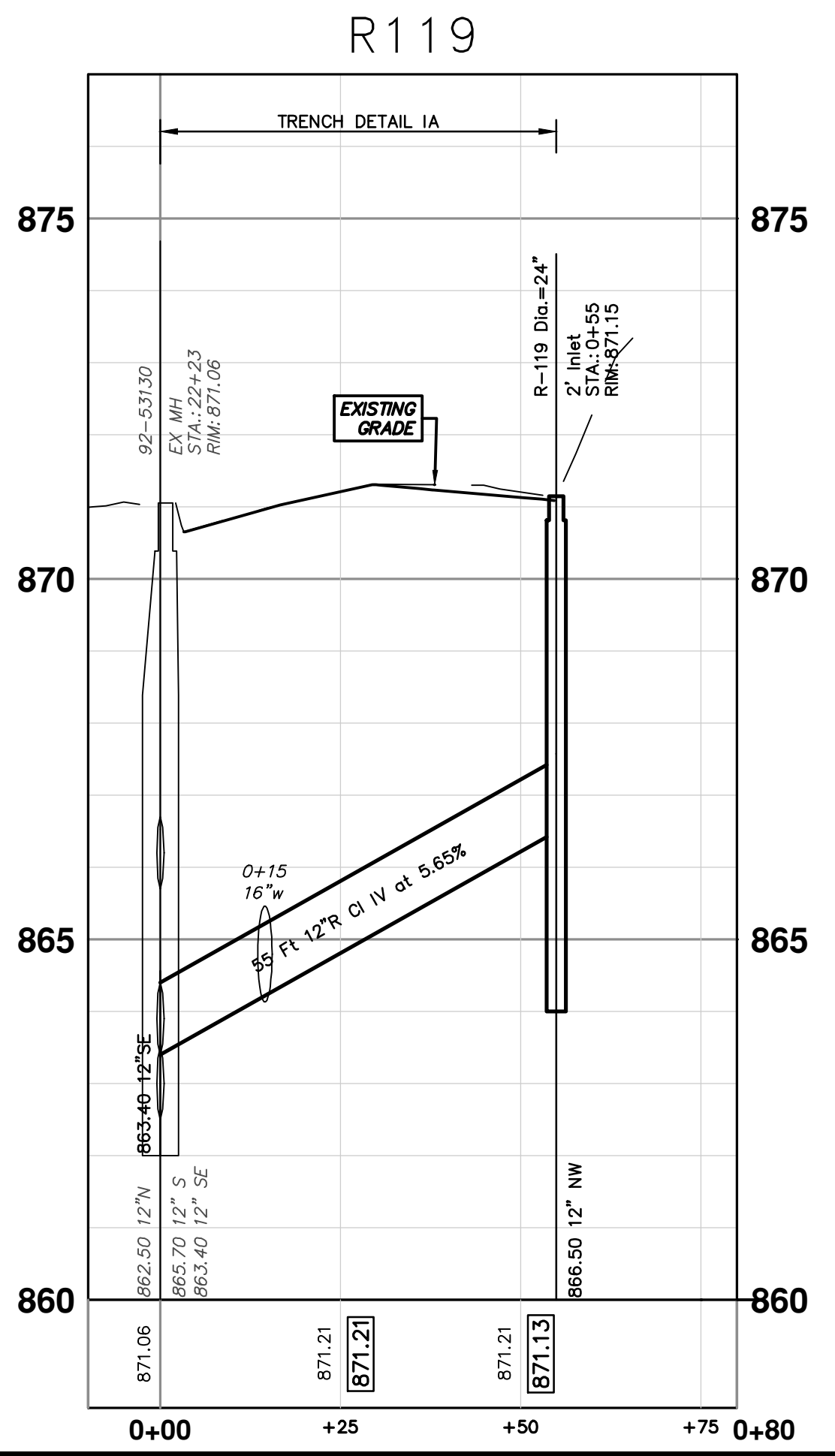
REV.	DESCRIPTION	DATE	CC/DF	NB	CHECKED
00	OUT TO BID	4-27-23			

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**CITY OF ANN ARBOR MICHIGAN**



STORM SEWER STRUCTURE TABLE						
STRUCTURE	UTILITY STATION	TYPE	RIM	INVERTS	DEPTH (Feet)	SUMP
R-117	0+21	2' Inlet	870.84	12" W 864.00	8.84	2'
R-118	0+40	2' Inlet	860.53	12" SW 856.00	6.53	2'
R-119	0+55	2' Inlet	871.15	12" NW 866.50	6.65	2'
R-120	0+39	2' Inlet	861.89	12" W 856.33	7.56	2'
R-121	0+26	2' Inlet	855.03	12" S 848.00	9.03	2'
R-122	0+32	2' Inlet	854.66	12" S 850.00	6.66	2'



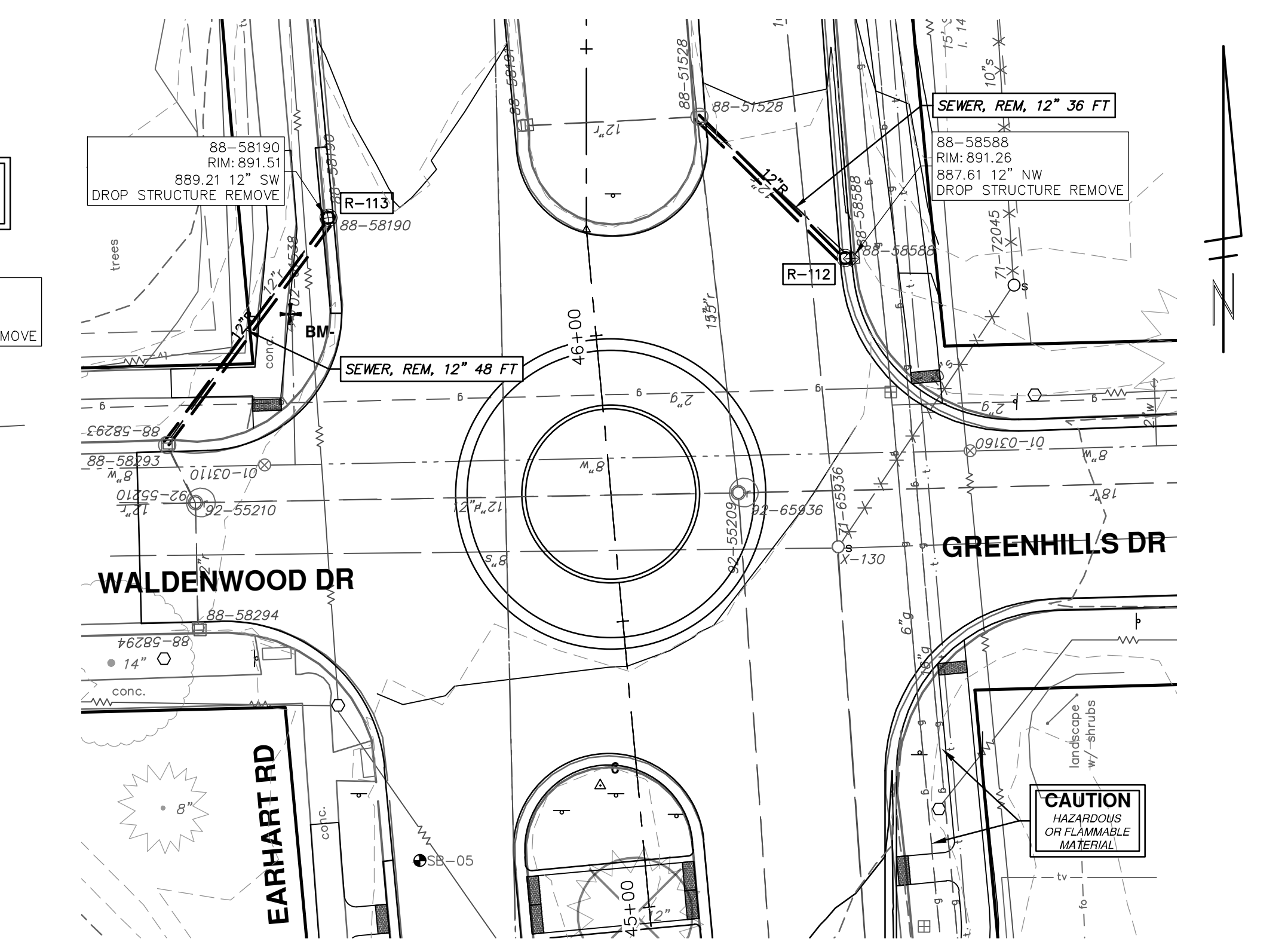
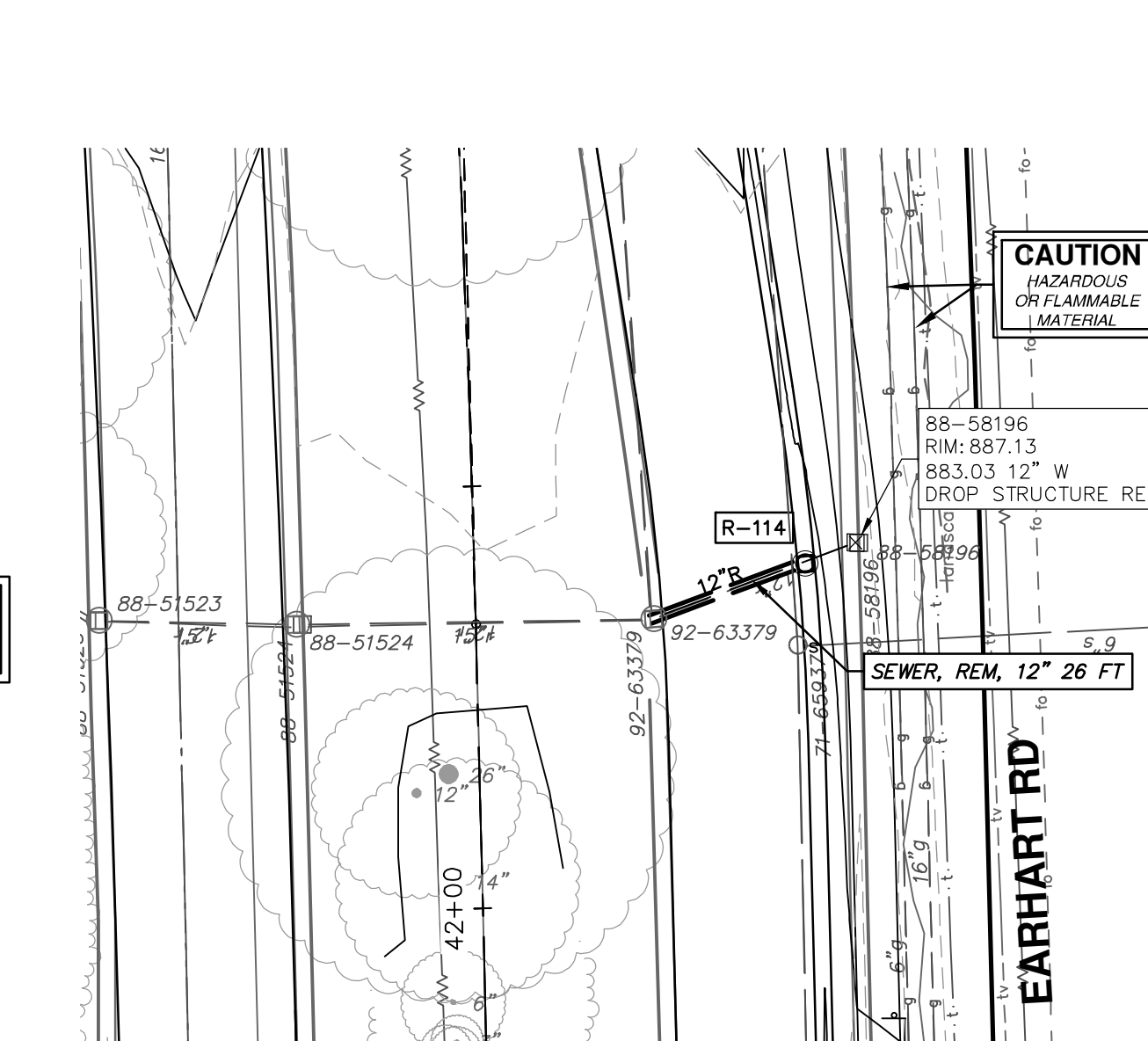
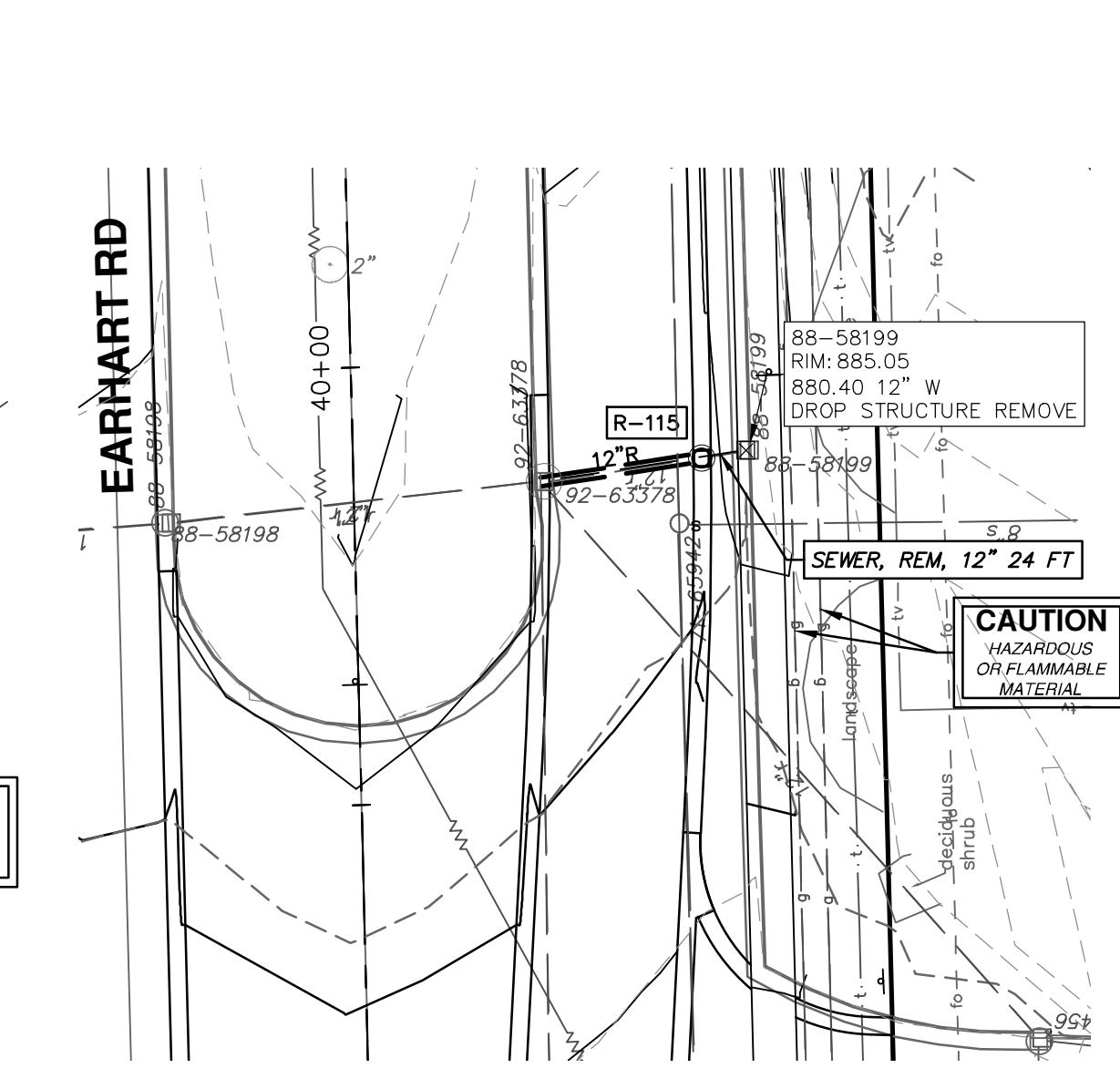
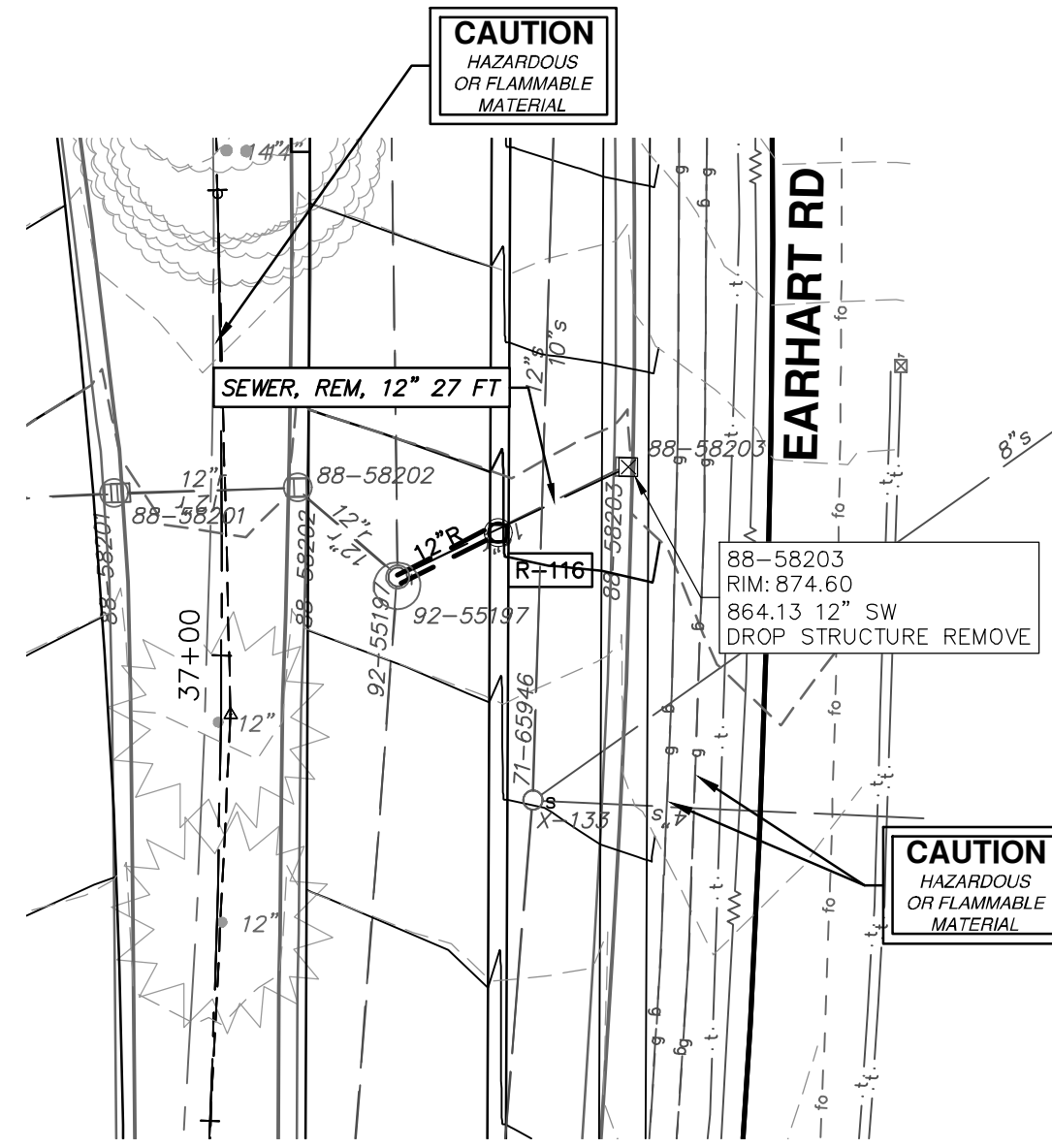
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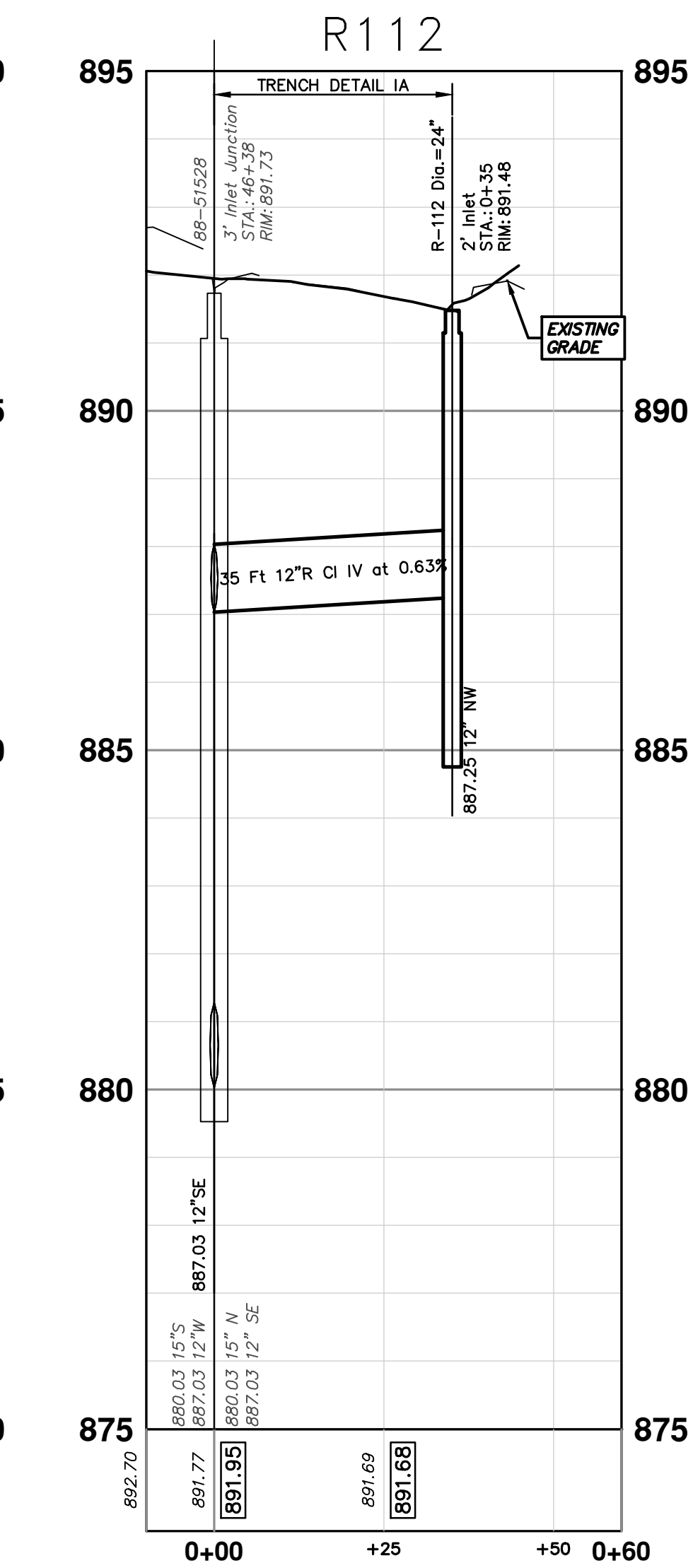
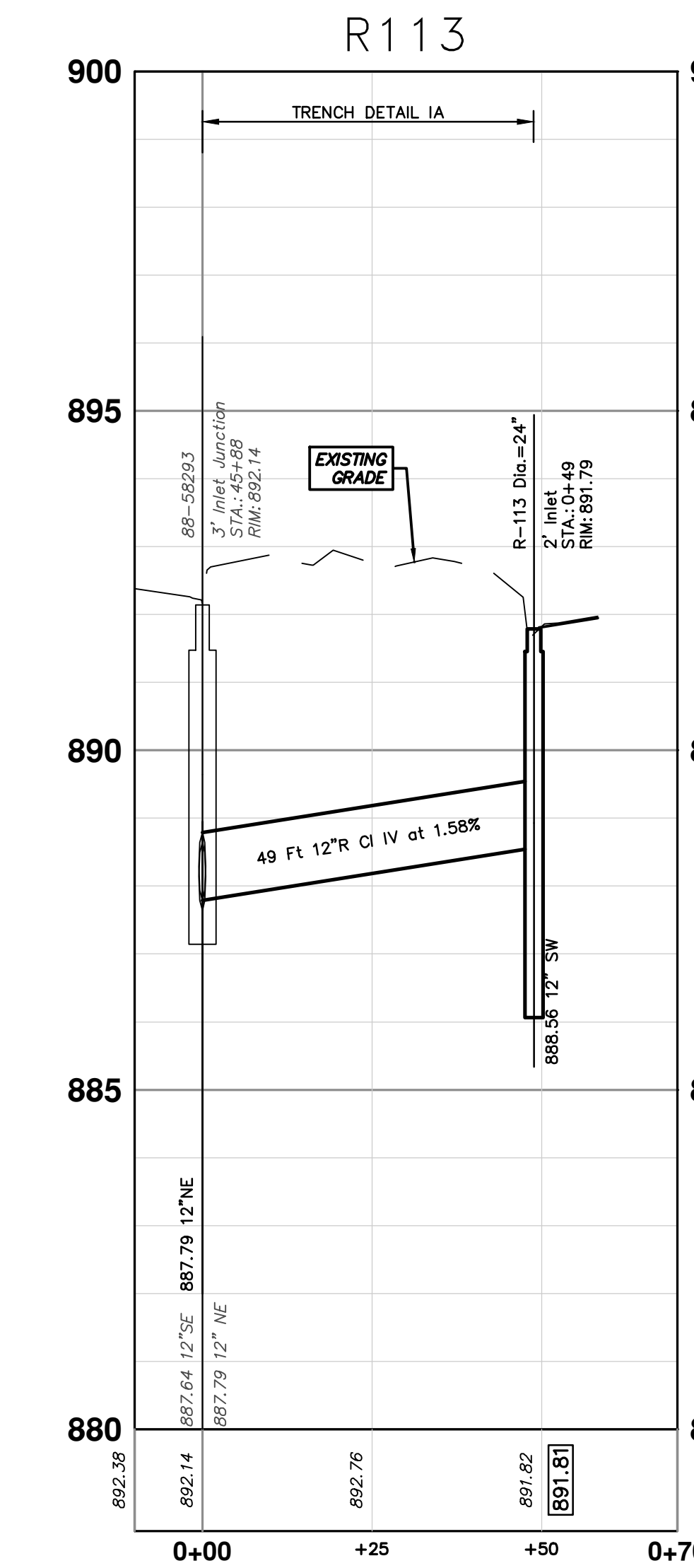
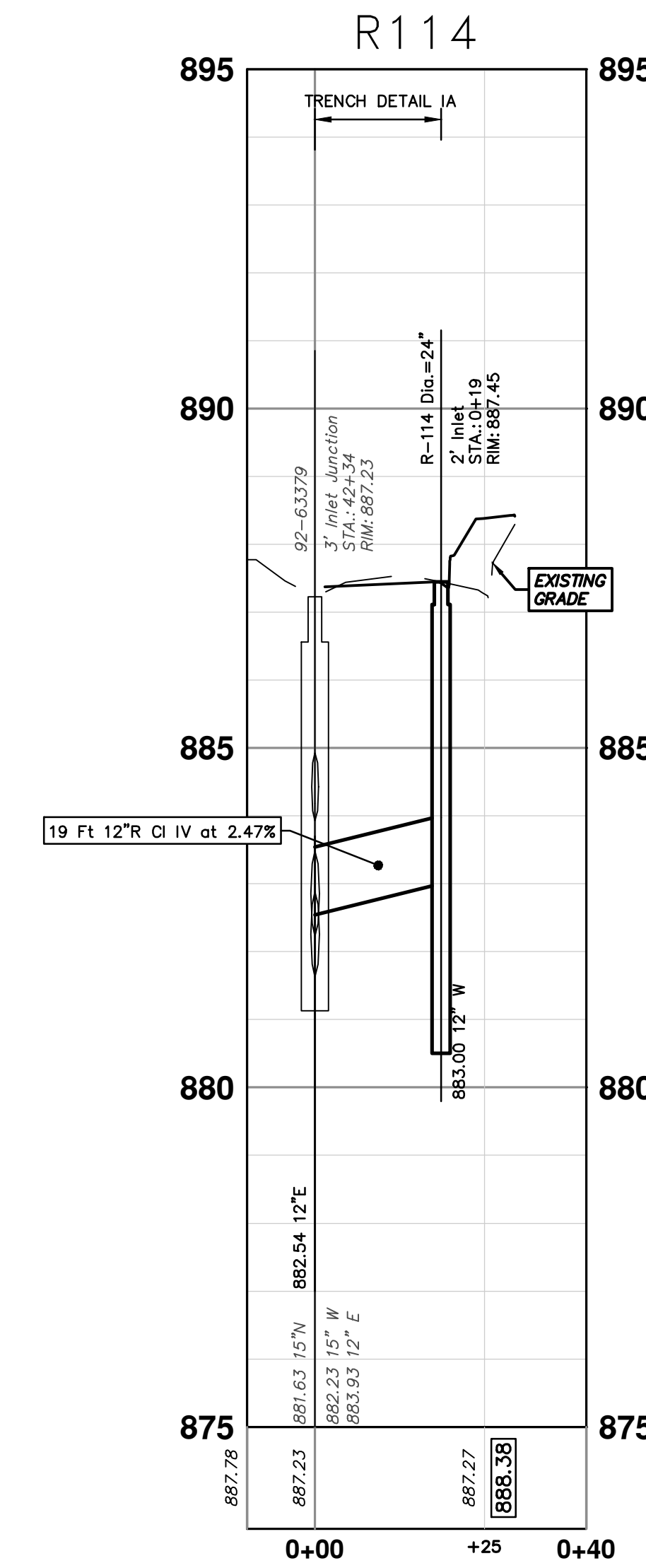
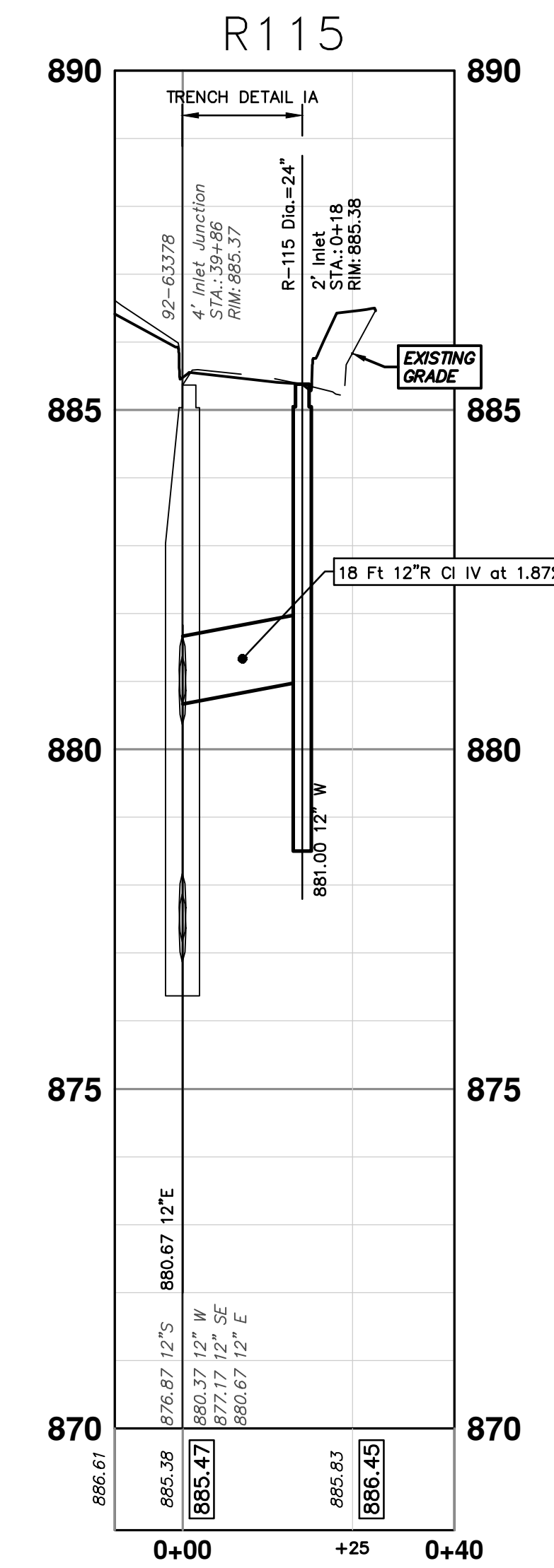
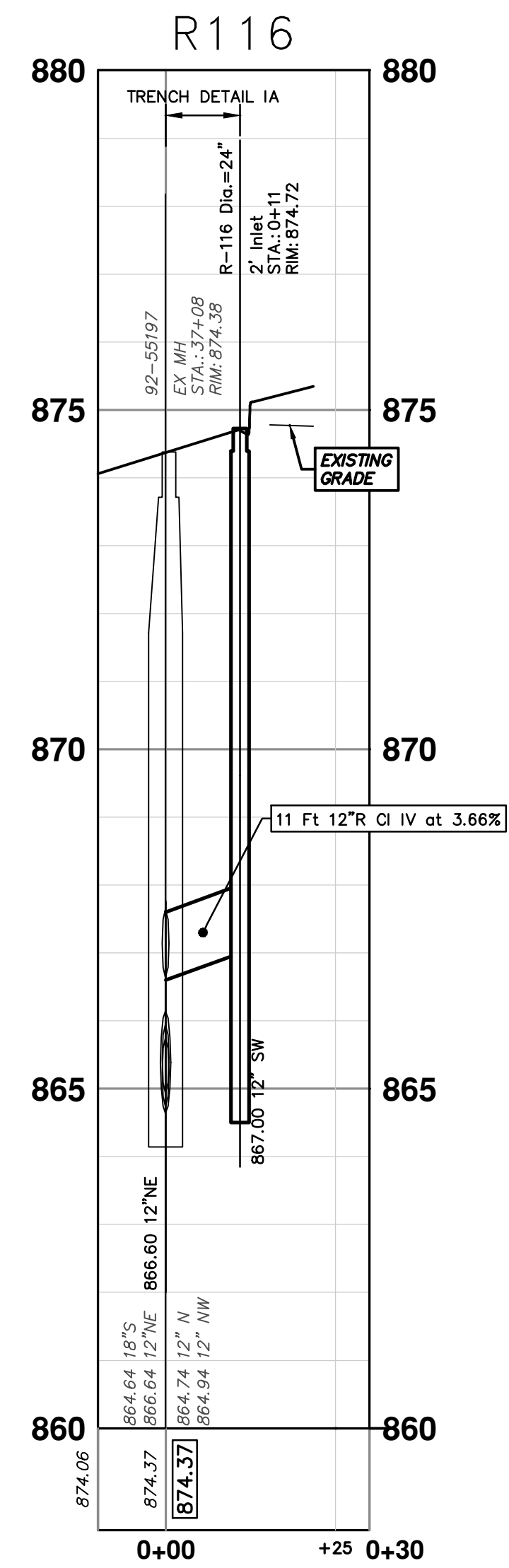
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01		DRAWN
02		CHECKED
03		DATE
04		CC/DF
05		NO

CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING  
EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
STORM SEWER PLAN & PROFILE  
R117, R118, R119, R120, R121, R122

SHEET No. 45 OF 74  
SCALE PLAN: 1" = 20'  
PROFILE: 1" = 2'  
DRAWING No. 2021023-45



STORM SEWER STRUCTURE TABLE						
STRUCTURE	UTILITY STATION	TYPE	RIM	INVERTS	DEPTH (Feet)	SUMP
R-112	0+35	2' Inlet	891.48	12" NW 887.25	6.23	2'
R-113	0+49	2' Inlet	891.79	12" SW 888.56	5.23	2'
R-114	0+19	2' Inlet	887.45	12" W 883.00	6.45	2'
R-115	0+18	2' Inlet	885.38	12" W 881.00	6.38	2'
R-116	0+11	2' Inlet	874.72	12" SW 867.00	9.72	2'



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DESCRIPTION

REV.

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EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
STORM SEWER PLAN & PROFILE  
R112, R113, R114, R115, R116

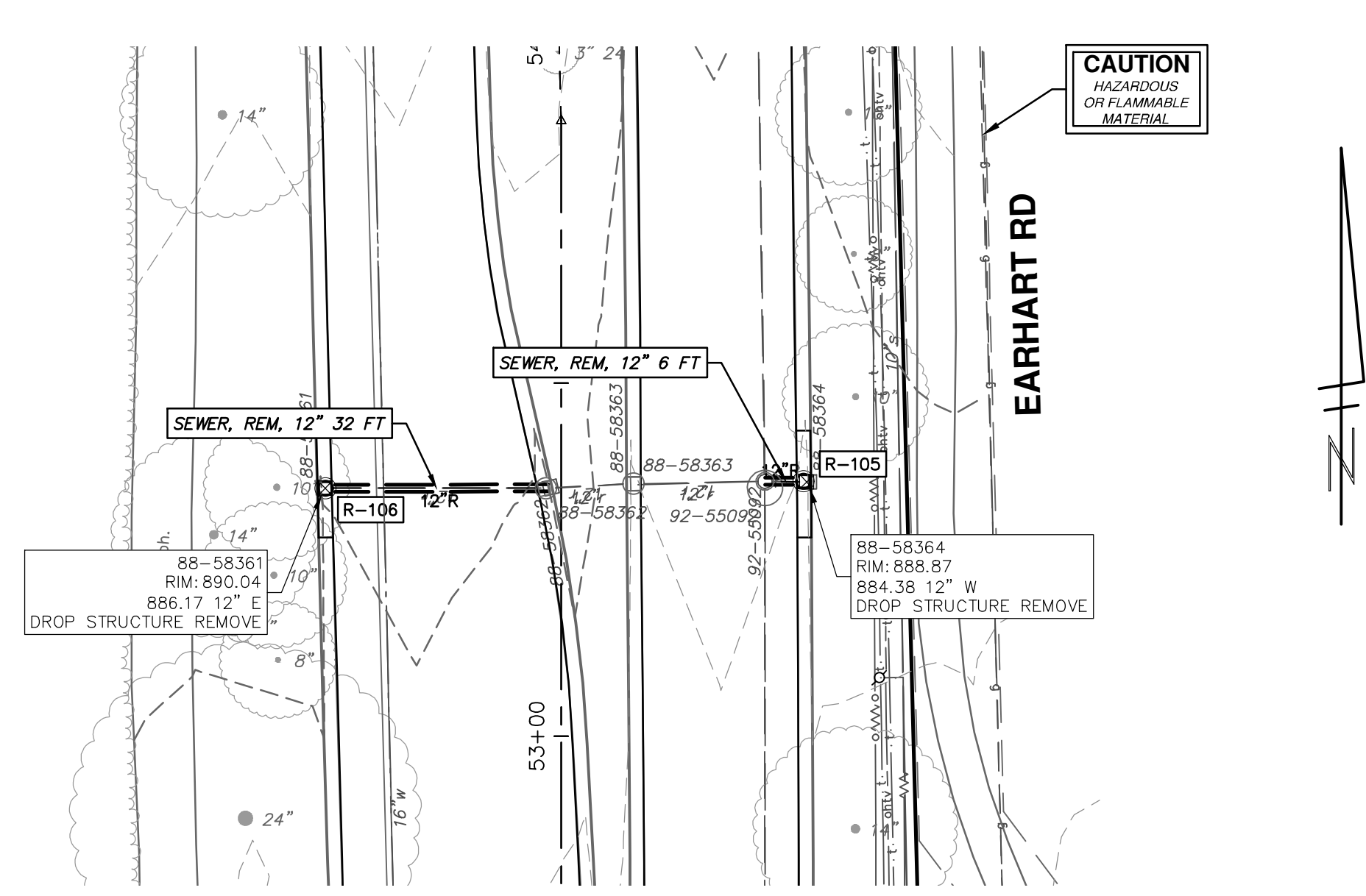
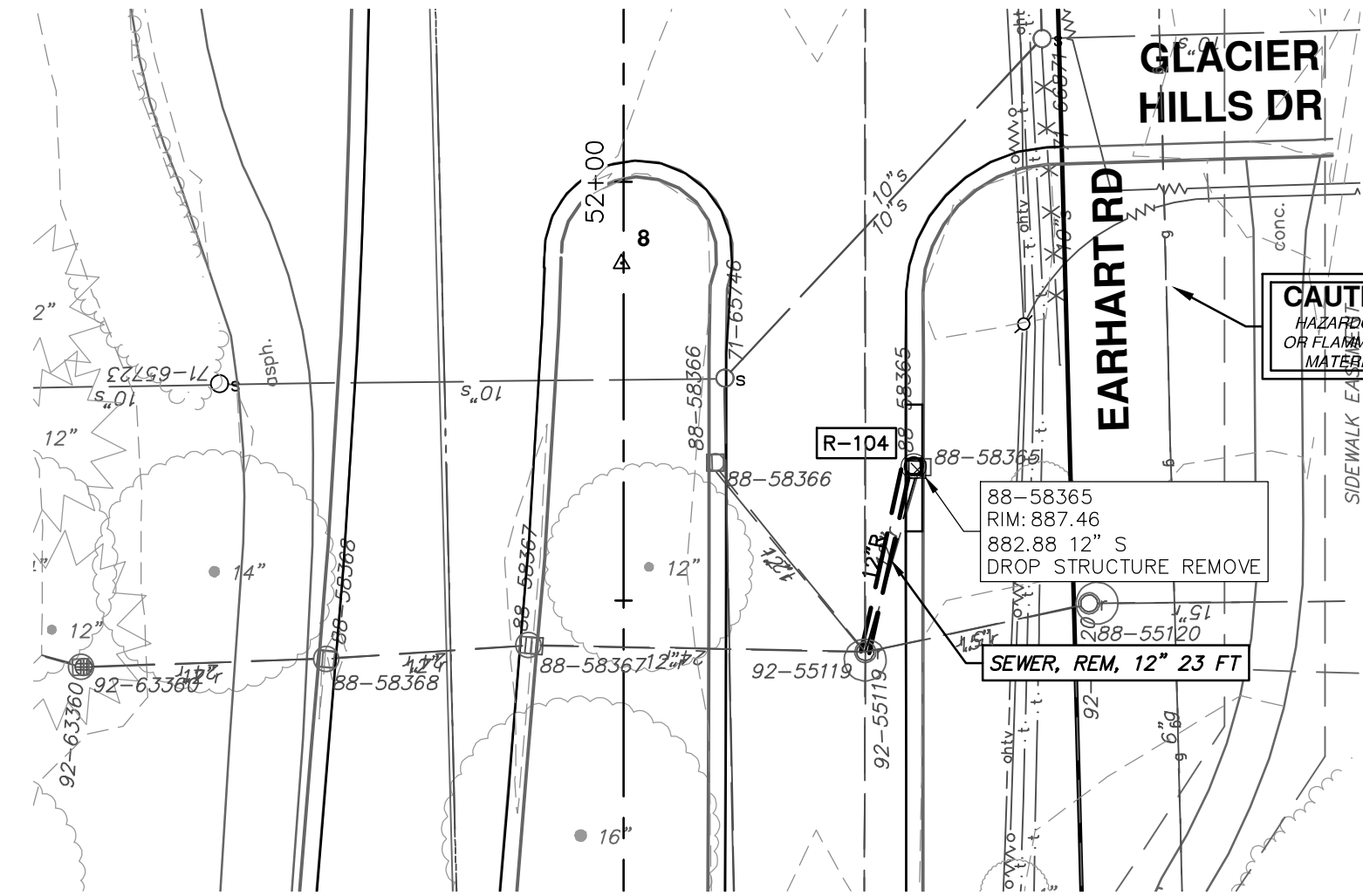
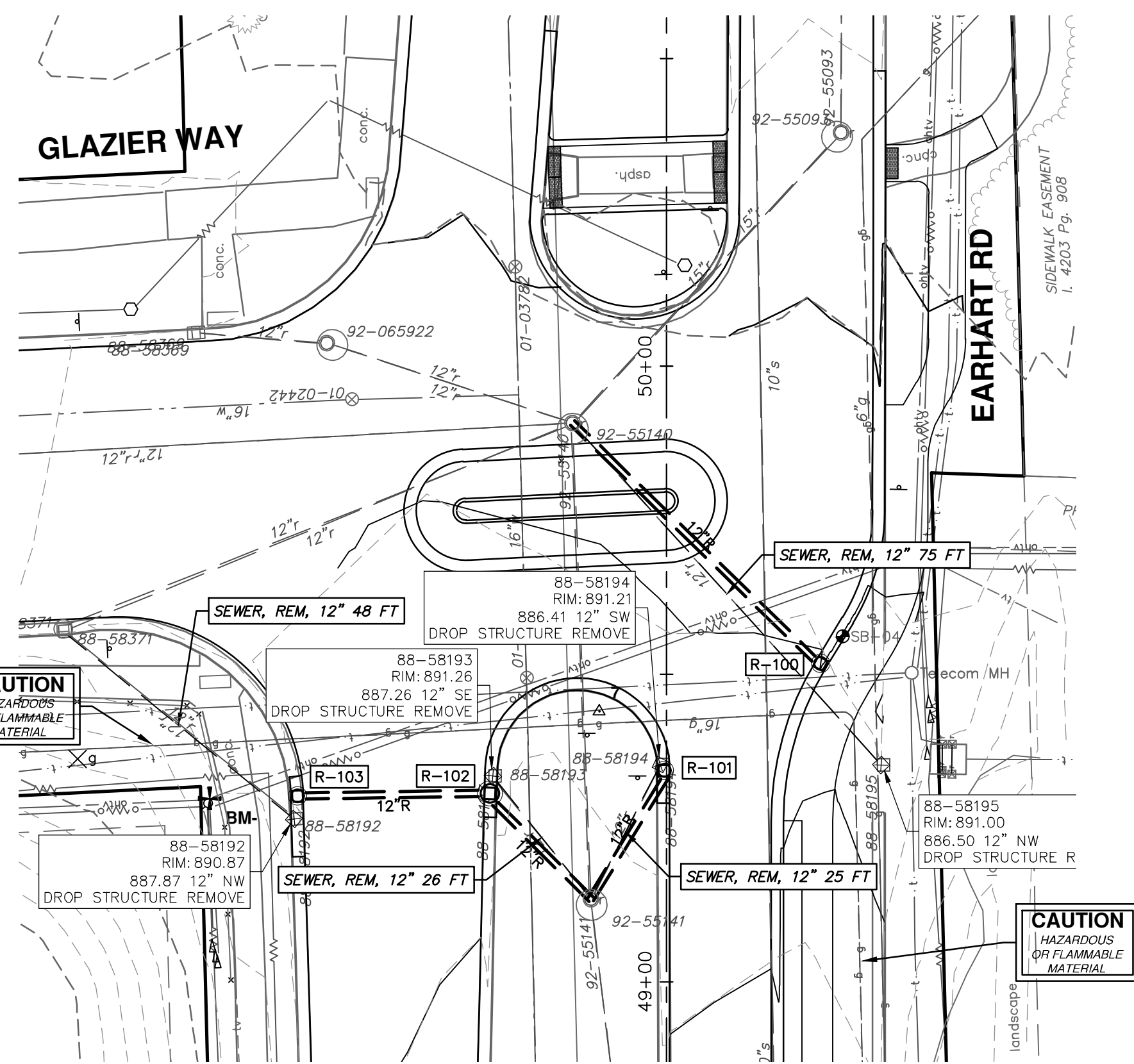
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SHEET No. 46 OF 74

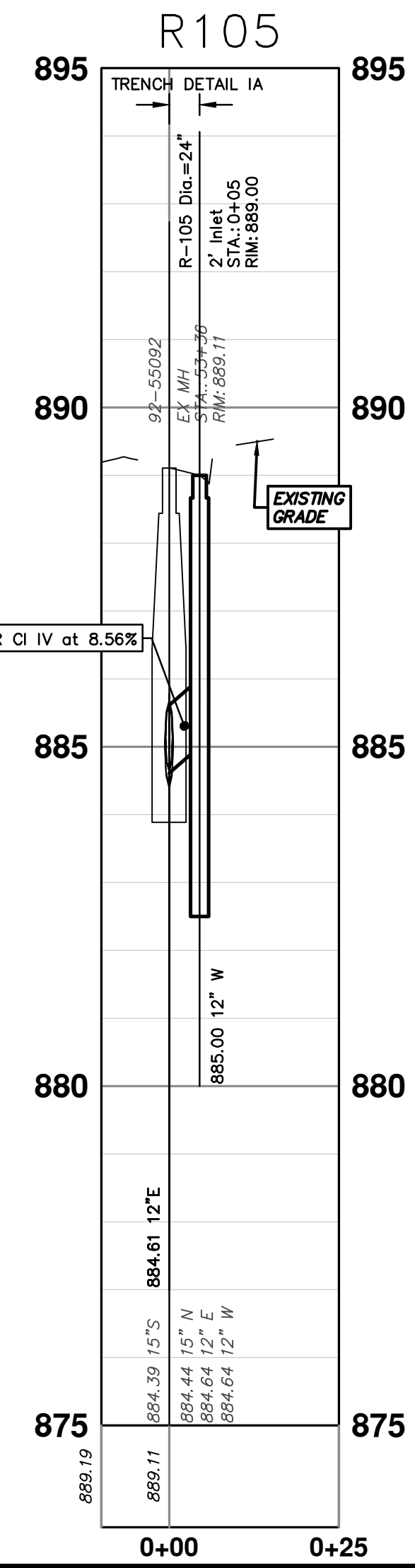
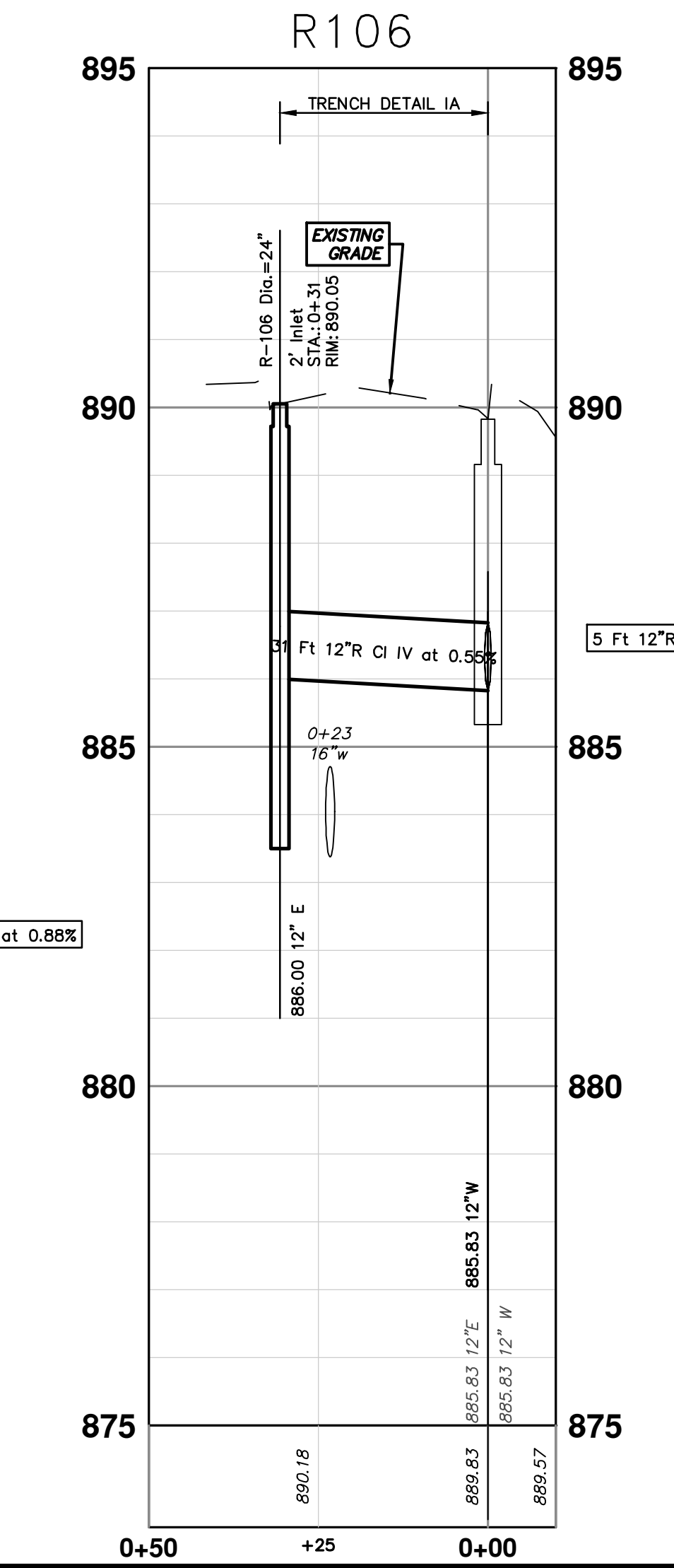
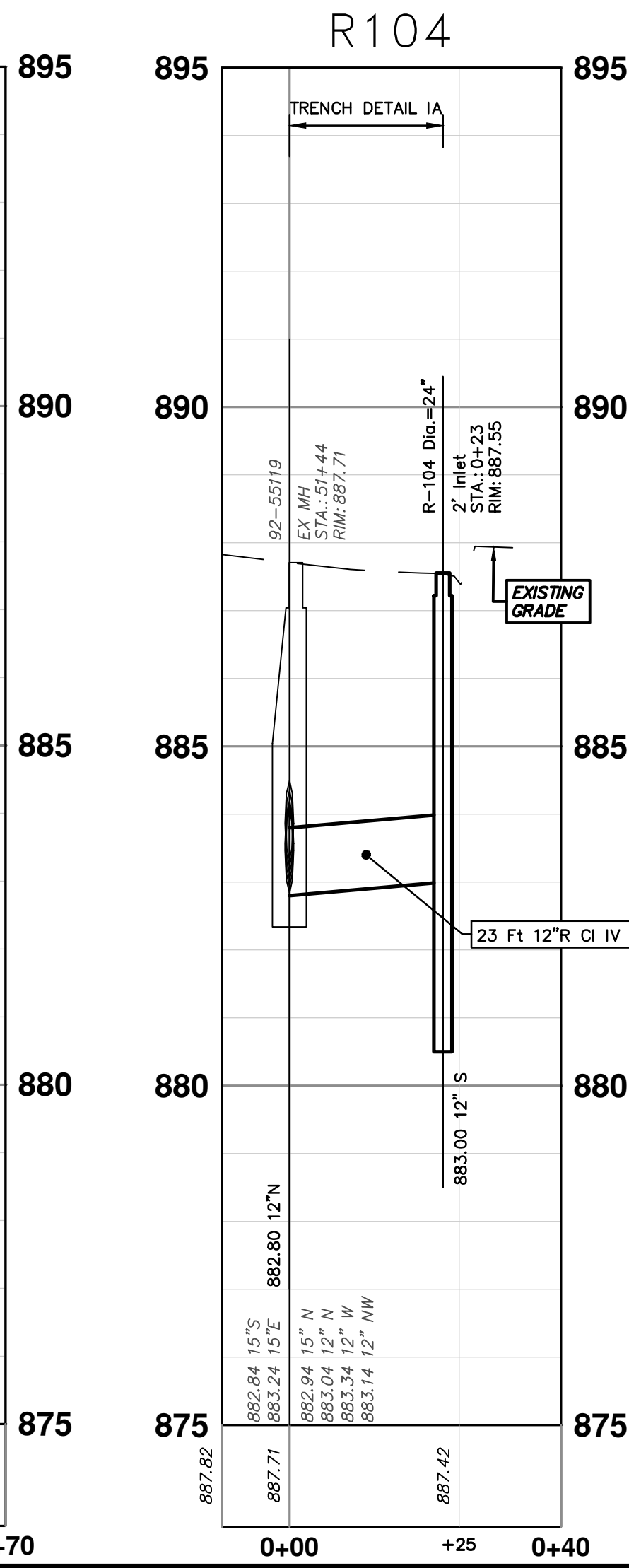
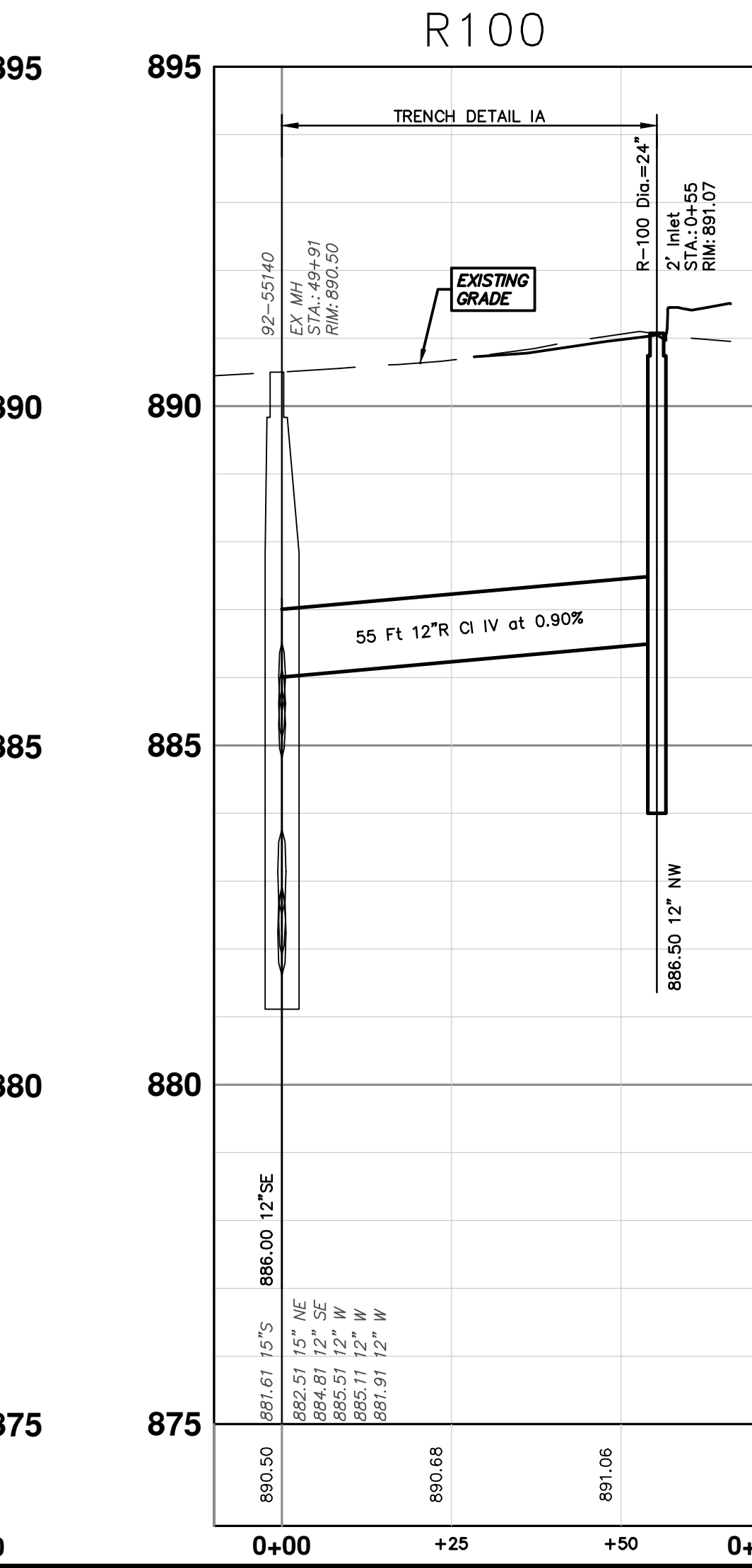
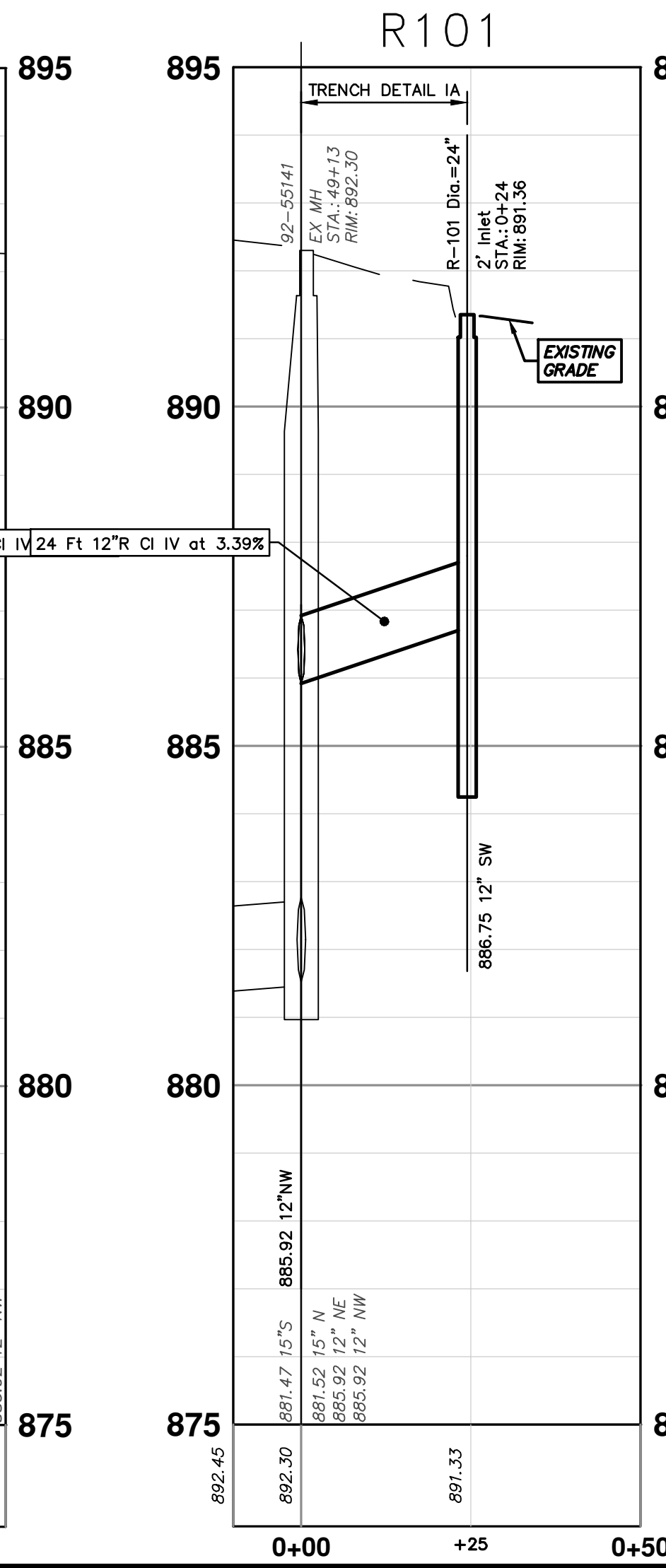
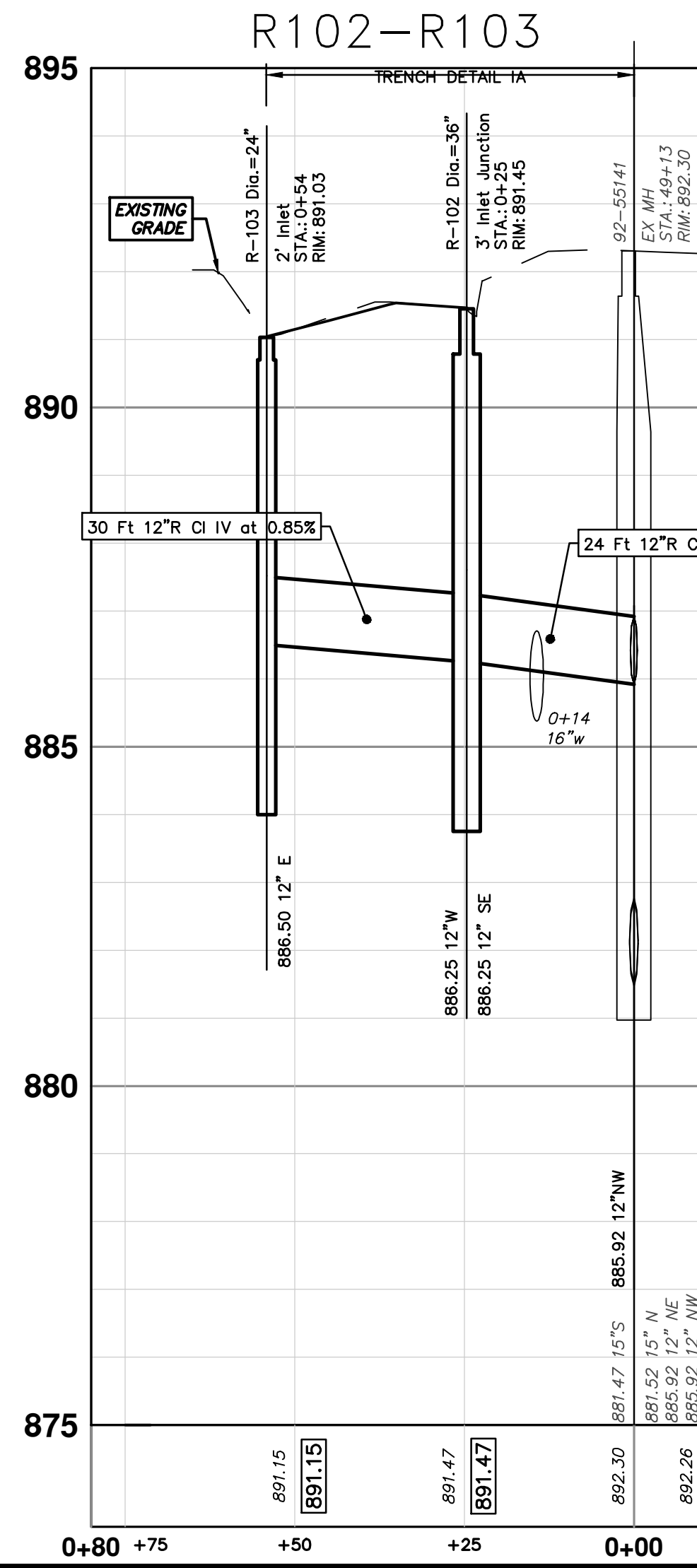
SCALE PLAN: 1" = 20'  
PROFILE: 1" = 2'

DRAWING No. 2021023-46

R:\2012023 Earhart (Geddes to US23)\Plan Production\2012023Stm.dwg Dwg Created: 26-Apr-23 - \_a2 standard bw.stb - Plot Date: 27-Apr-23



STRUCTURE	UTILITY STATION	TYPE	RIM	INVERTS	DEPTH (Feet)	SUMP
R-100	0+55	2' Inlet	891.07	12" NW 886.50	6.57	2'
R-101	0+24	2' Inlet	891.36	12" SW 886.75	6.61	2'
R-102	0+25	3' Inlet Junction	891.45	12" SE 886.25 12" W 886.25	7.20	2'
R-103	0+54	2' Inlet	891.03	12" E 886.50	6.53	2'
R-104	0+23	2' Inlet	887.55	12" S 883.00	6.55	2'
R-105	0+05	2' Inlet	889.00	12" W 885.00	6.00	2'
R-106	0+31	2' Inlet	890.05	12" E 886.00	6.05	2'



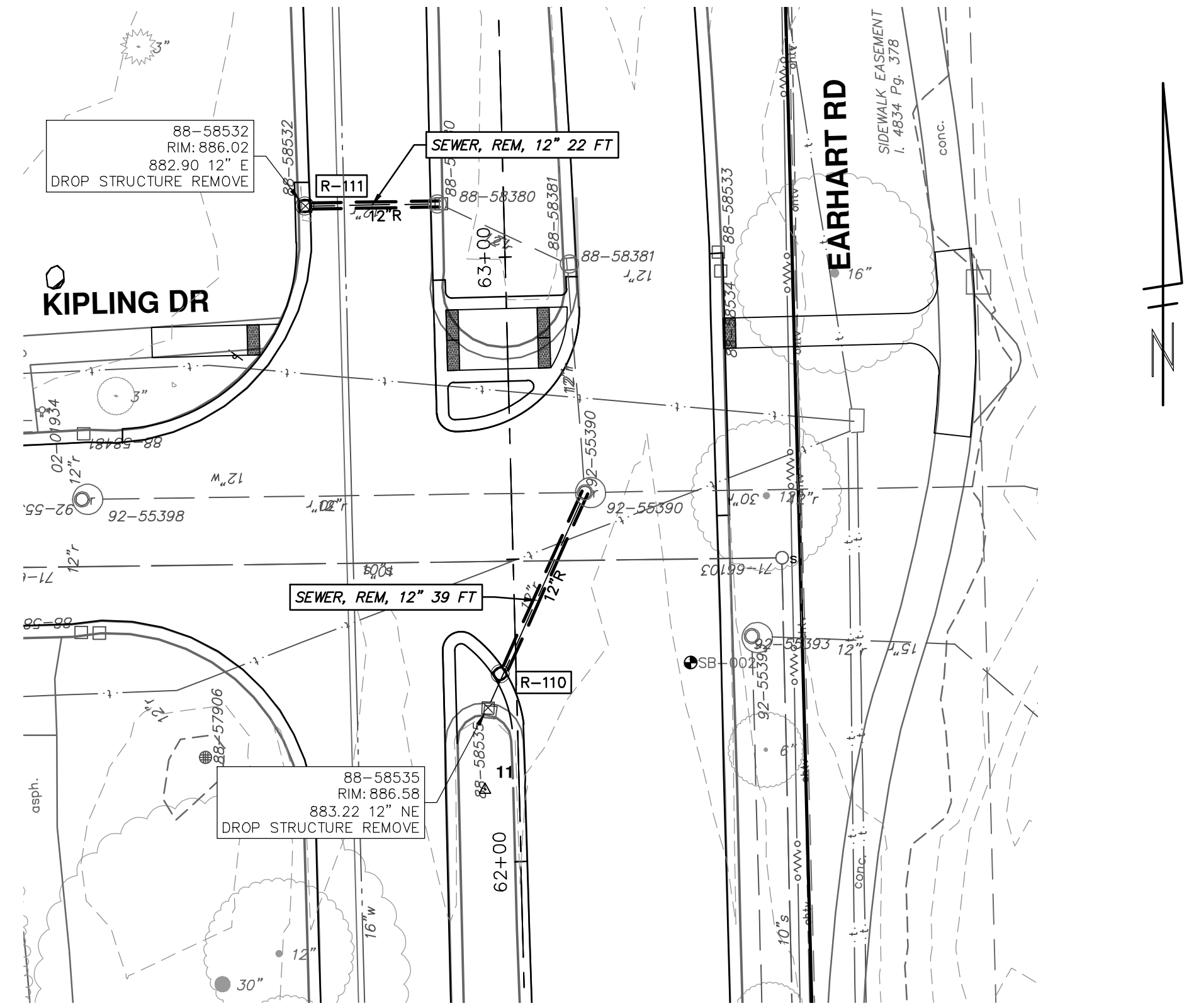
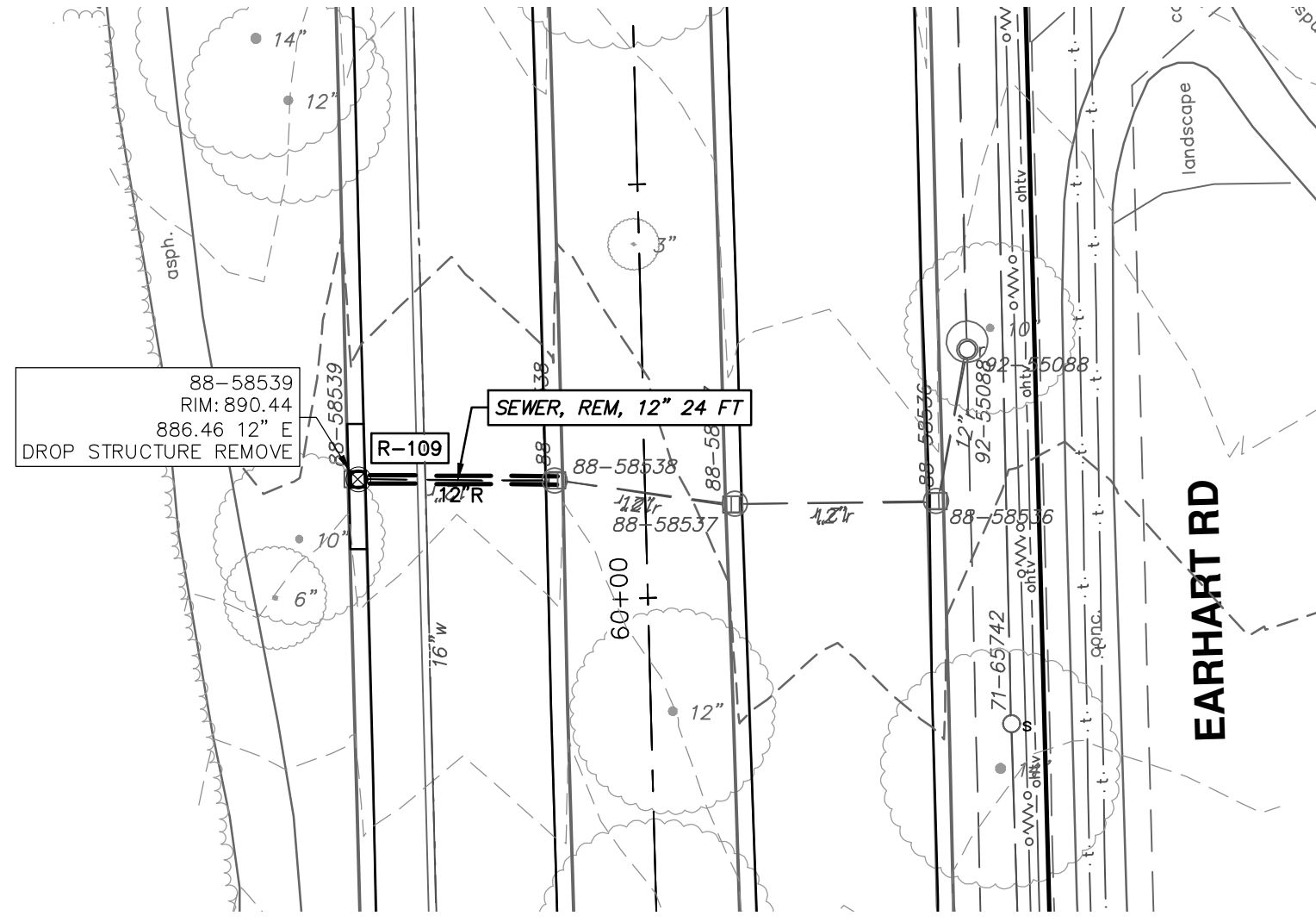
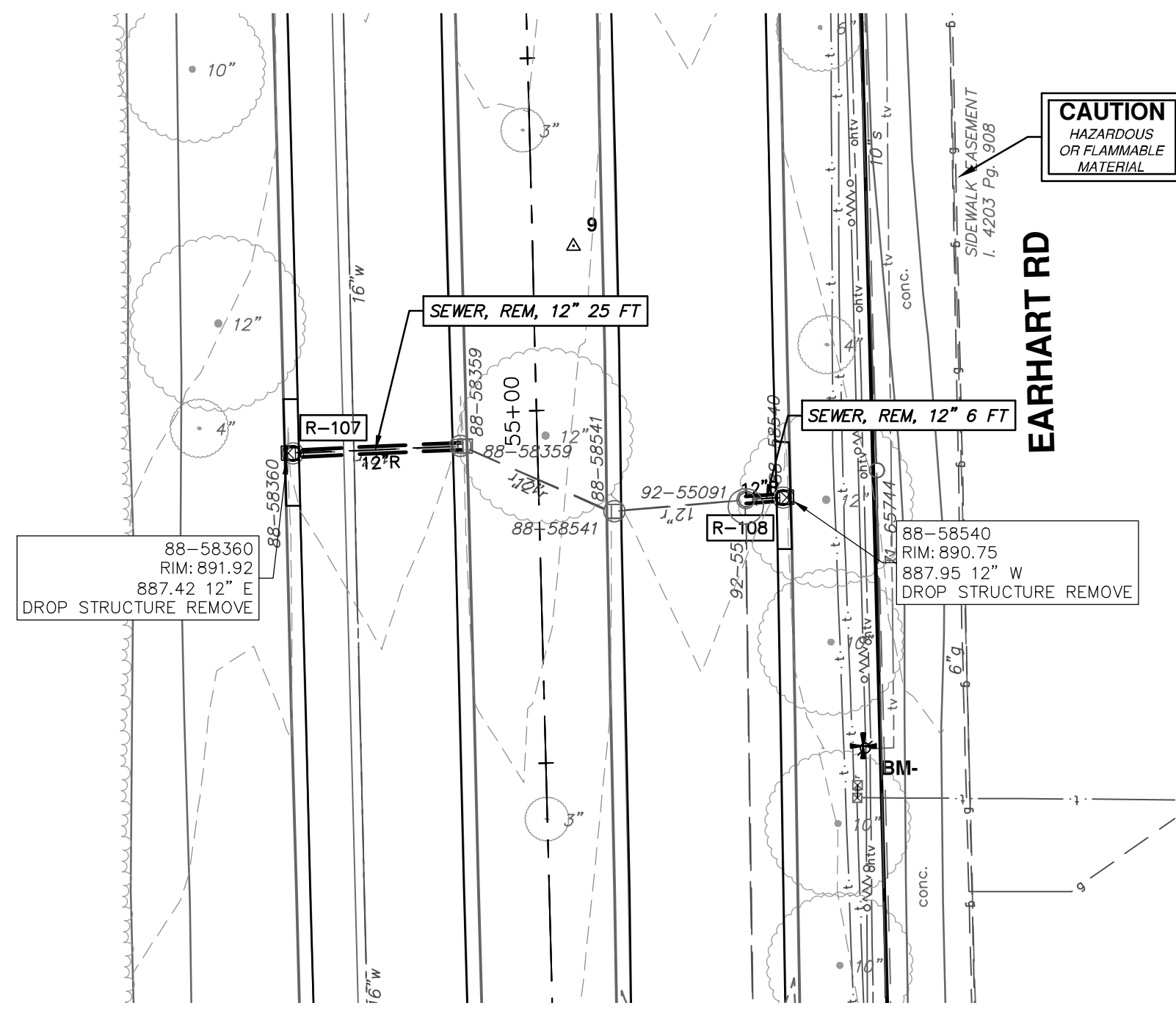
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		DRAWN		
		CC/DF		
		DATE		
		NO		

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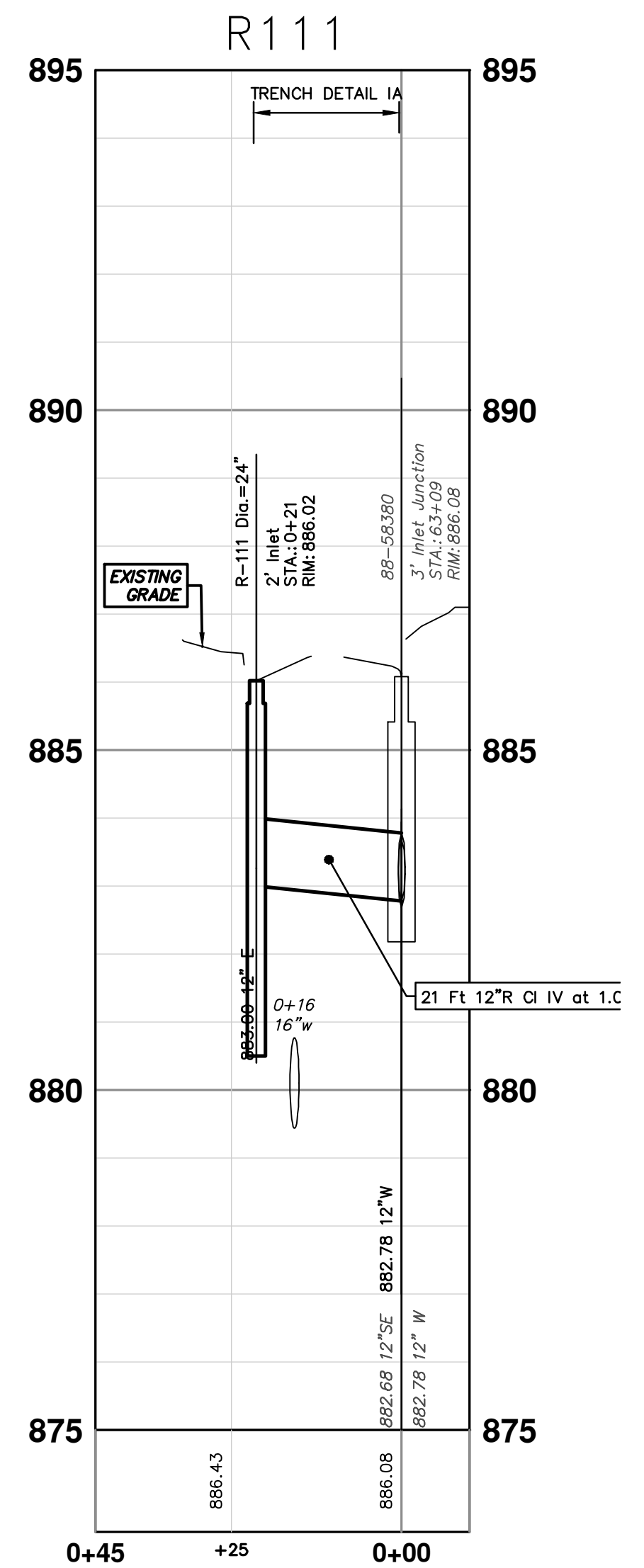
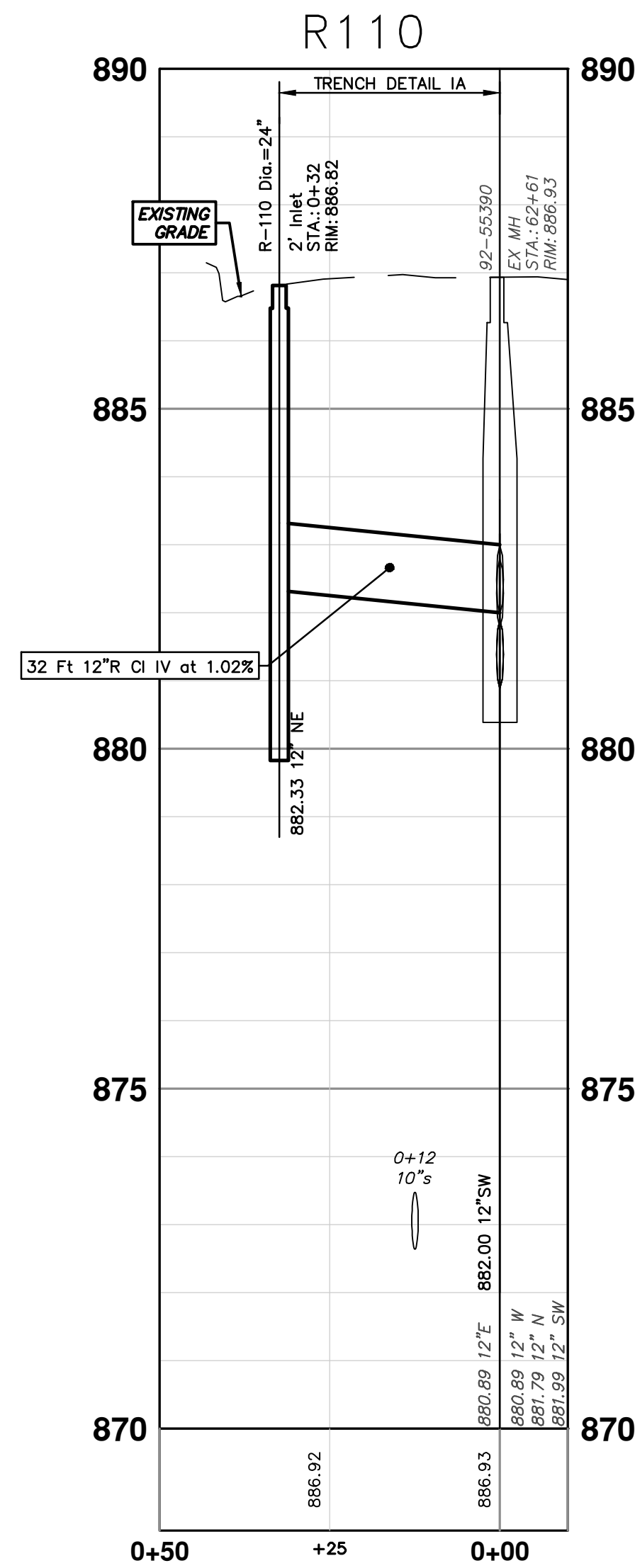
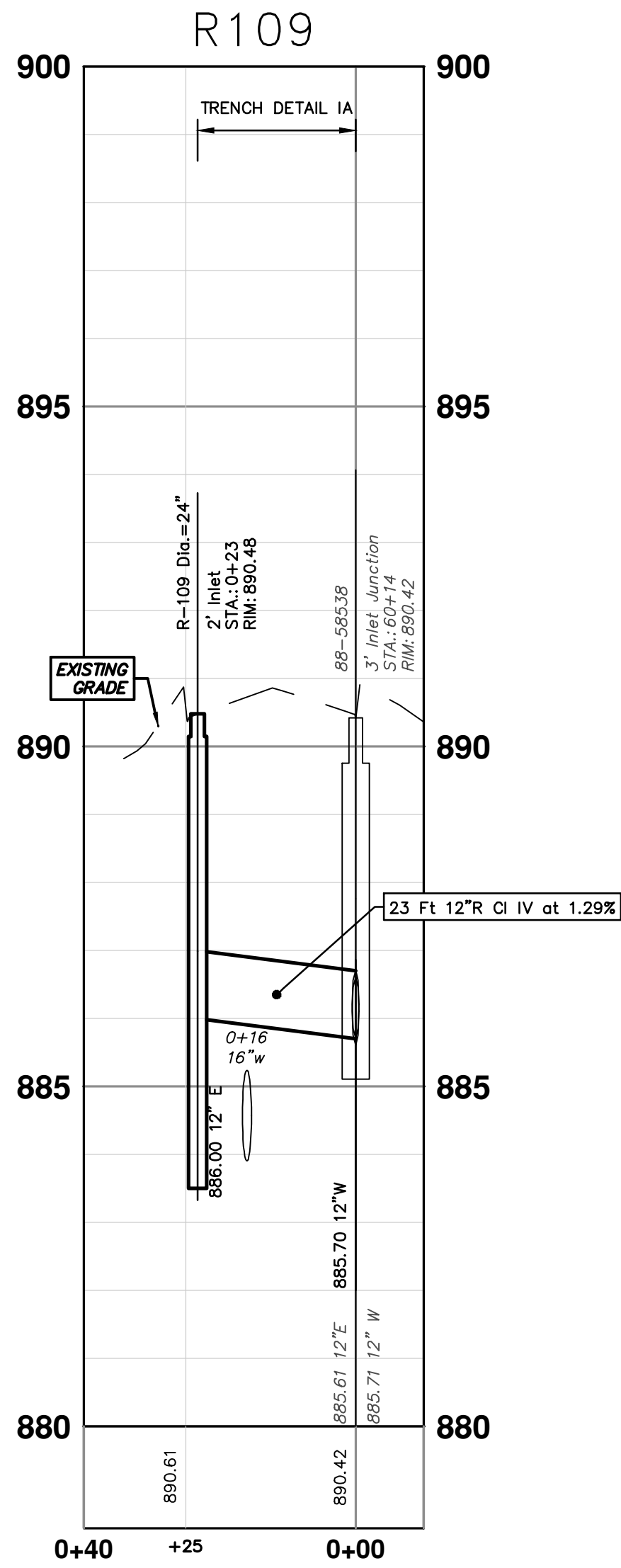
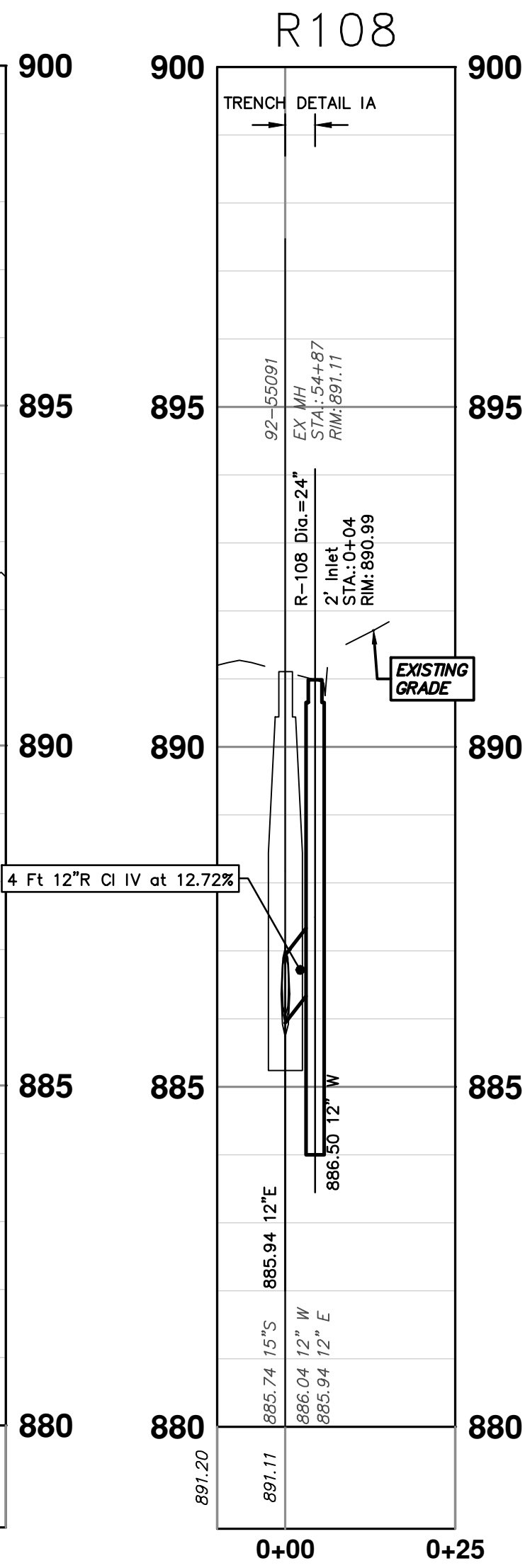
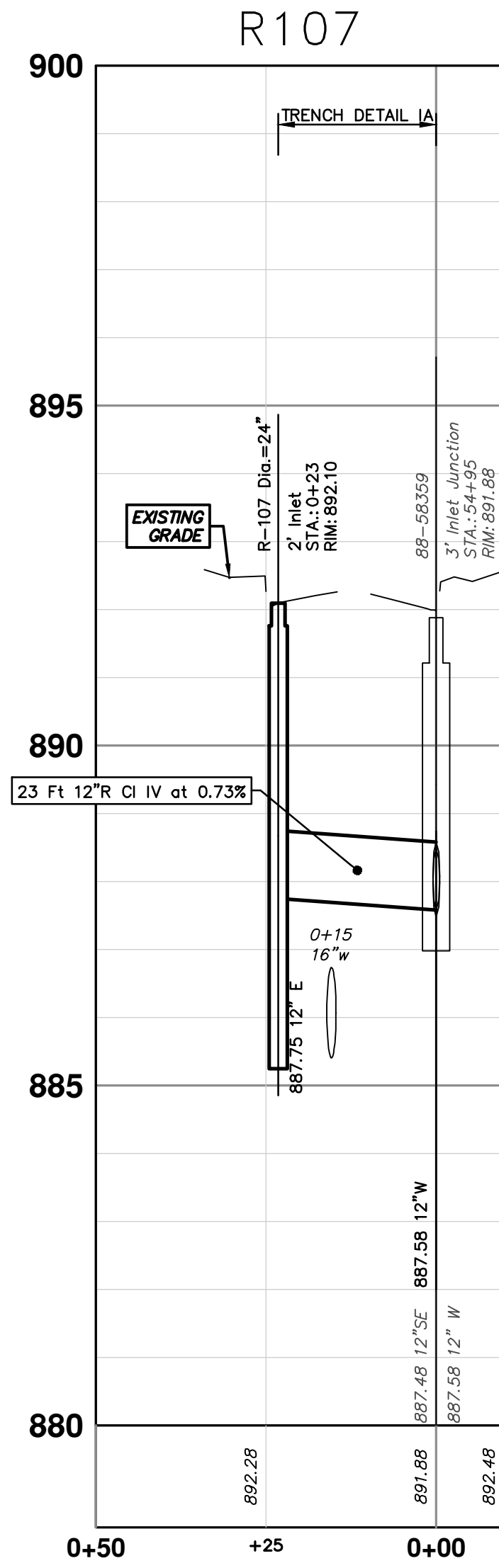


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EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
STORM SEWER PLAN & PROFILE  
R100, R101, R102, R103, R104, R105, R106

SHEET No. 47 OF 74  
SCALE PLAN: 1" = 20'  
PROFILE: 1" = 2'  
DRAWING No. 2021023-47



STORM SEWER STRUCTURE TABLE						
STRUCTURE	UTILITY STATION	TYPE	RIM	INVERTS	DEPTH (Feet)	SUMP
R-107	0+23	2' Inlet	892.10	12" E 887.75	6.35	2'
R-108	0+04	2' Inlet	890.99	12" W 886.50	6.49	2'
R-109	0+23	2' Inlet	890.48	12" E 886.00	6.48	2'
R-110	0+32	2' Inlet	886.82	12" NE 882.33	6.49	2'
R-111	0+21	2' Inlet	886.02	12" E 883.00	5.02	2'



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00	OUT TO BID	4-27-23			

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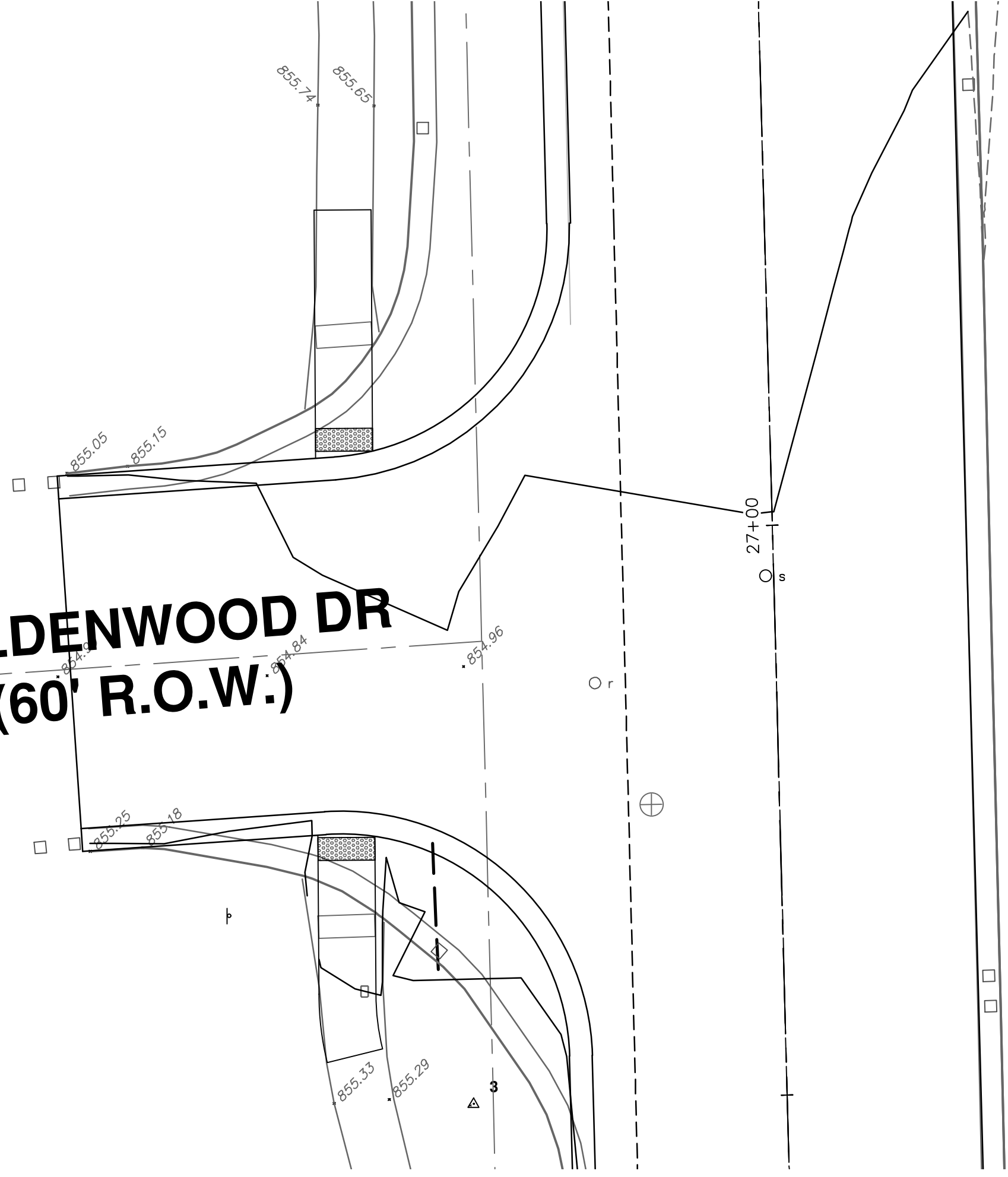


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EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
STORM SEWER PLAN & PROFILE  
R107, R108, R109, R110, R111

SHEET No. 48 OF 74  
SCALE PLAN: 1" = 20'  
PROFILE: 1" = 2'  
DRAWING No. 2021023-48

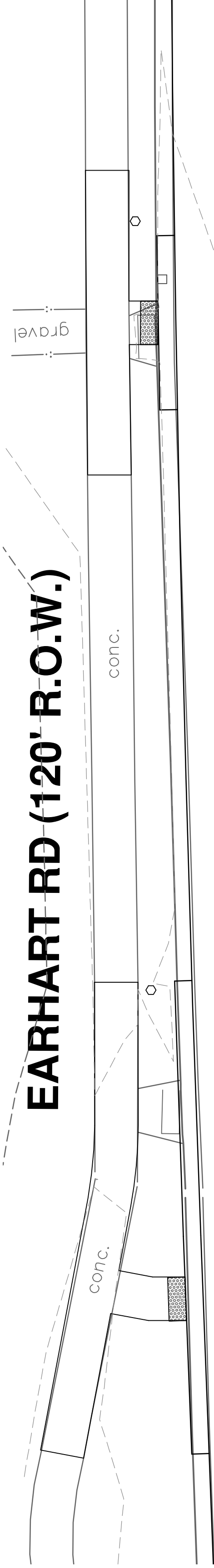


**WALDENWOOD DR  
(60' R.O.W.)**

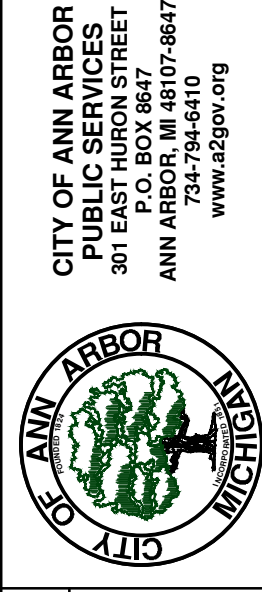
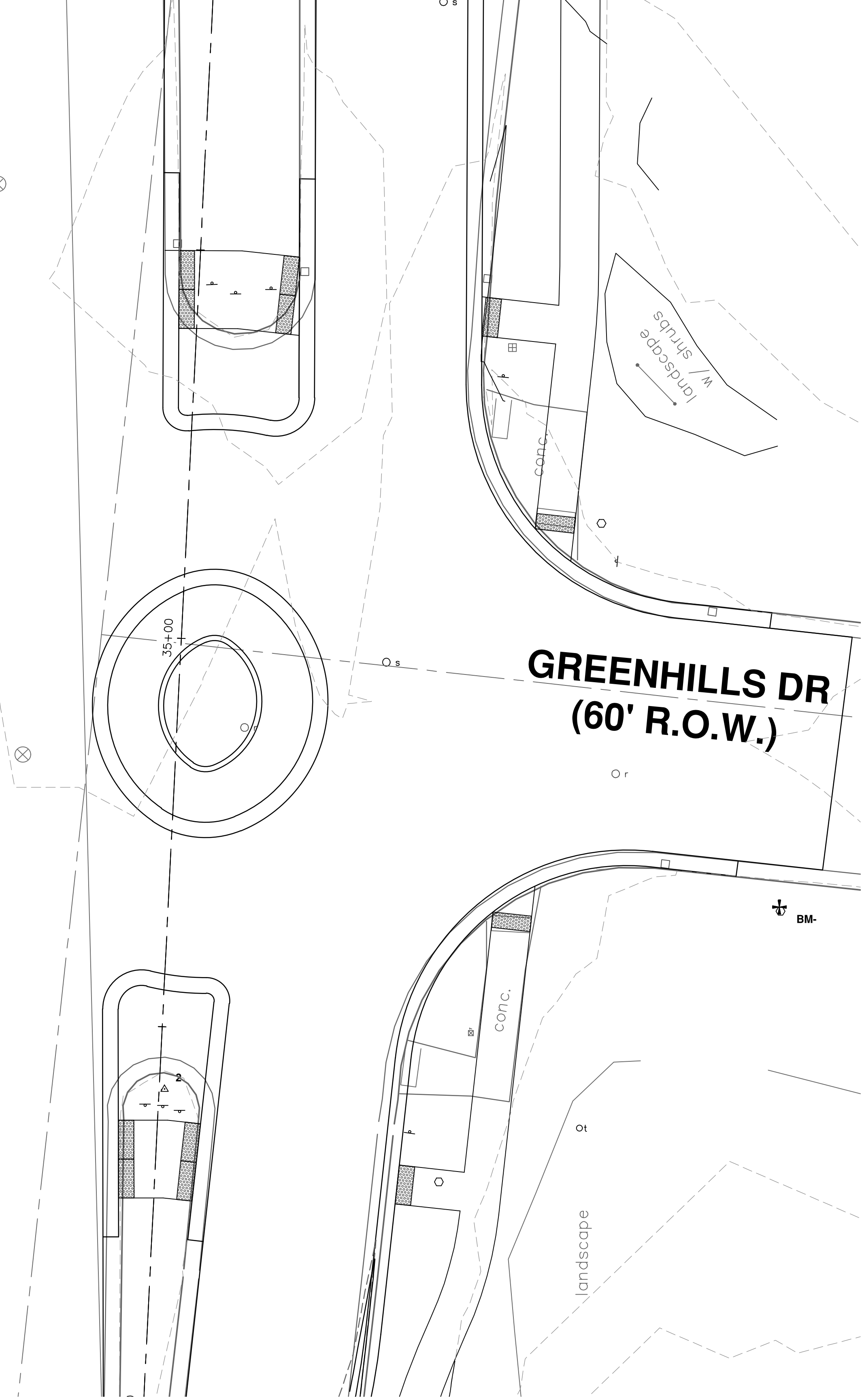


**EARHART RD (120' R.O.W.)**

**EARHART RD (120' R.O.W.)**

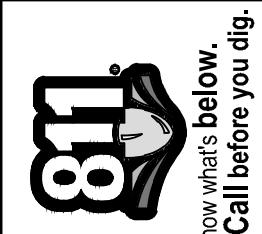


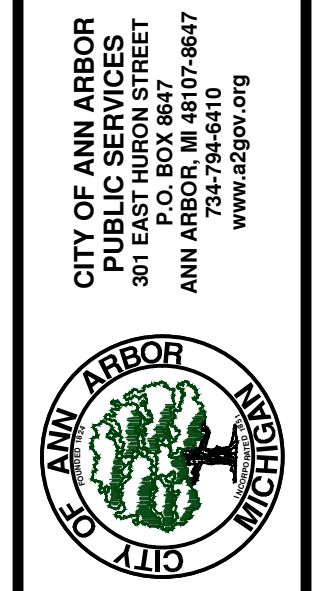
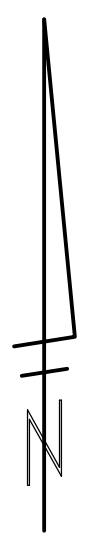
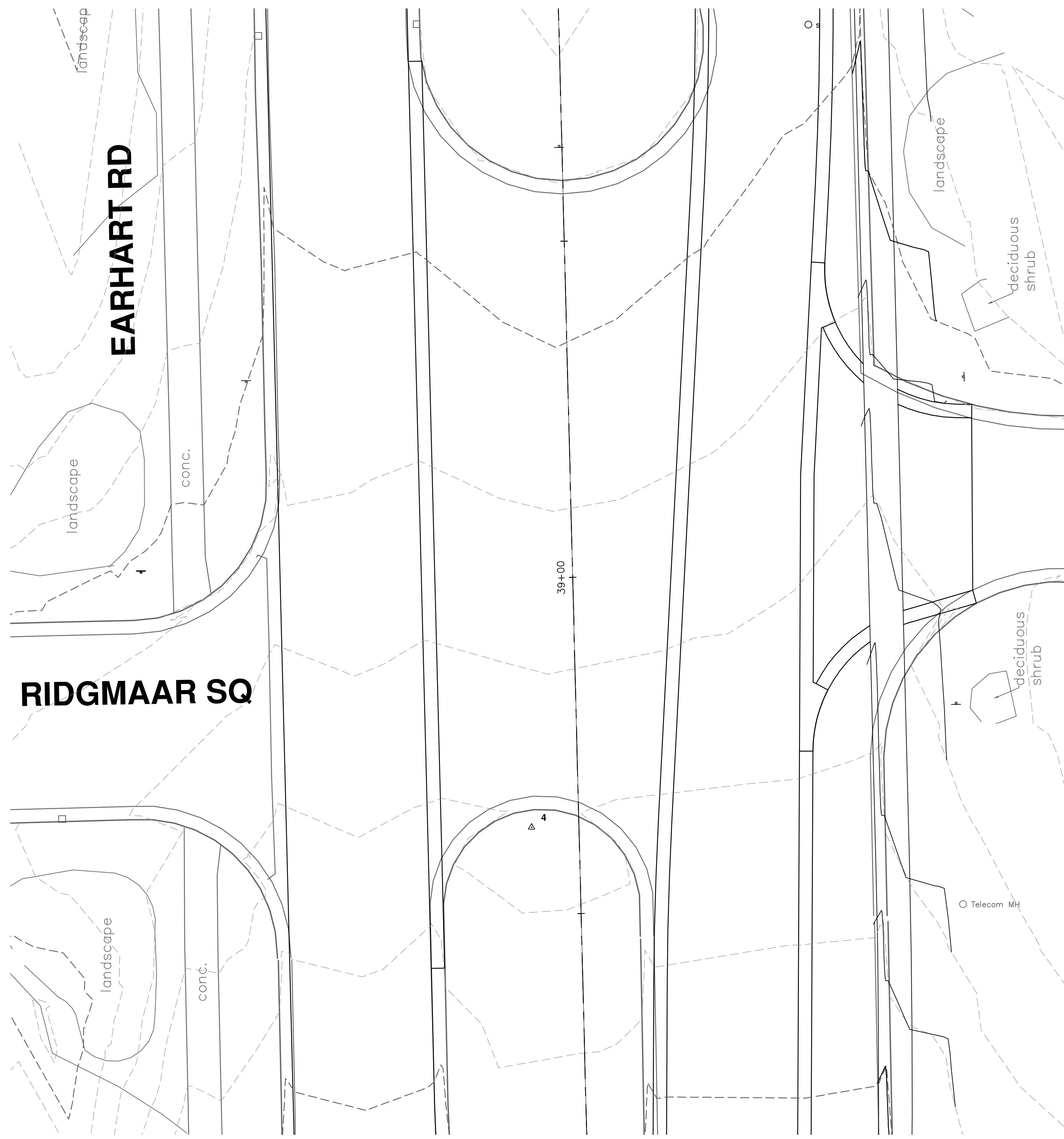
**GREENHILLS DR  
(60' R.O.W.)**



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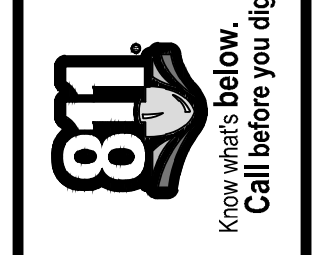
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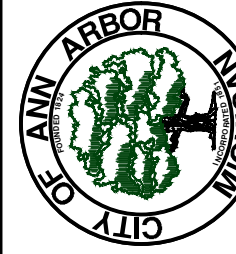
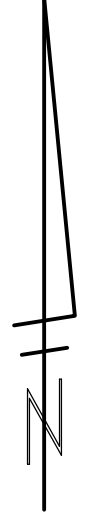
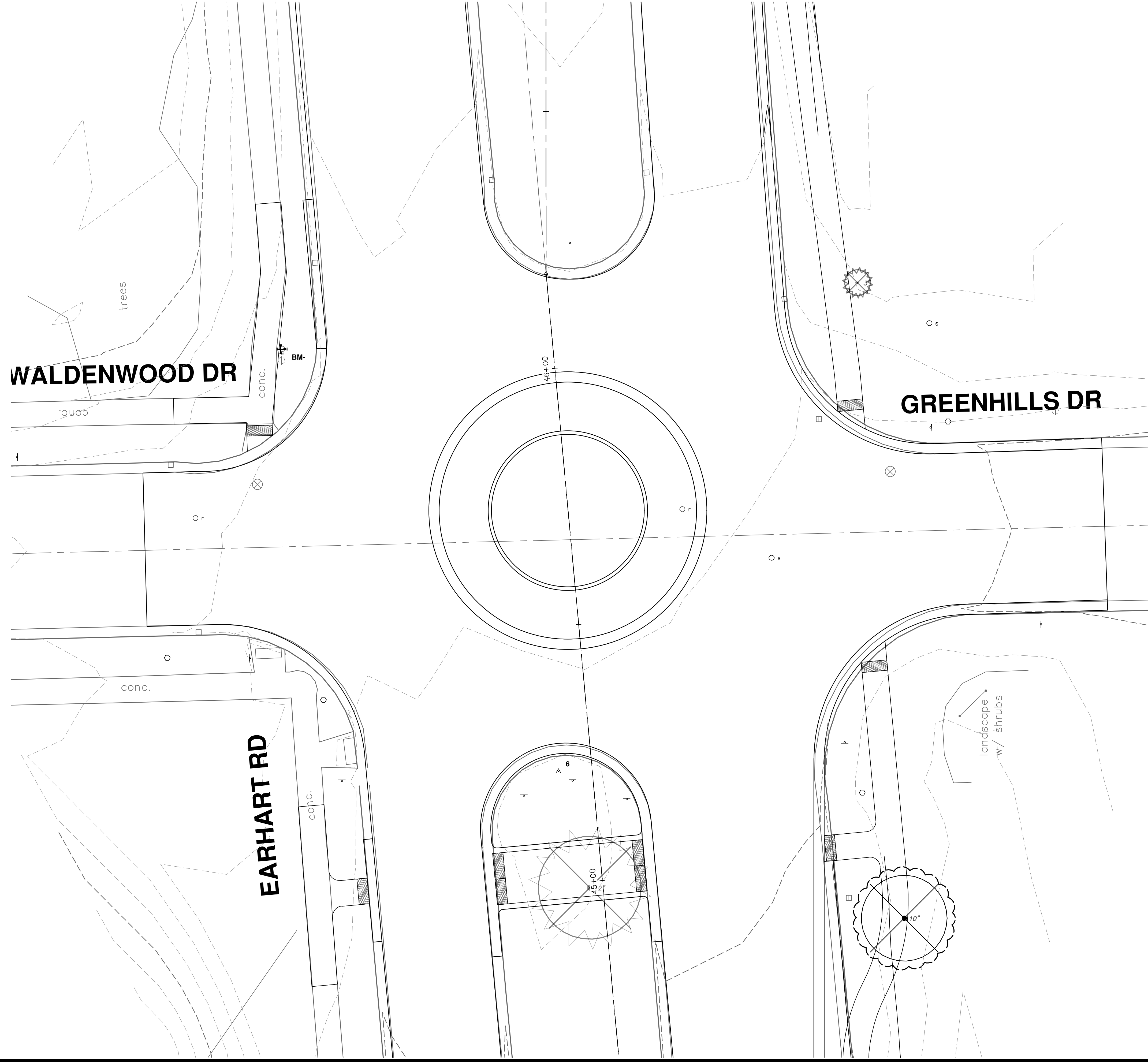




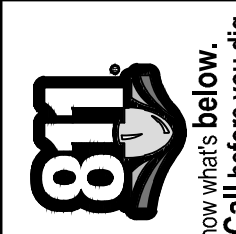
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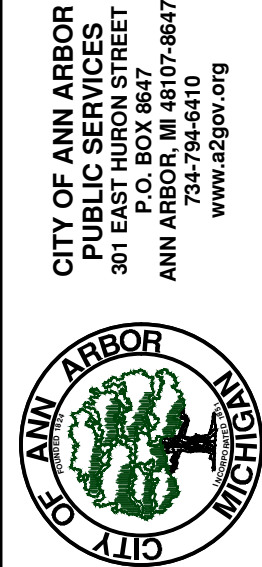
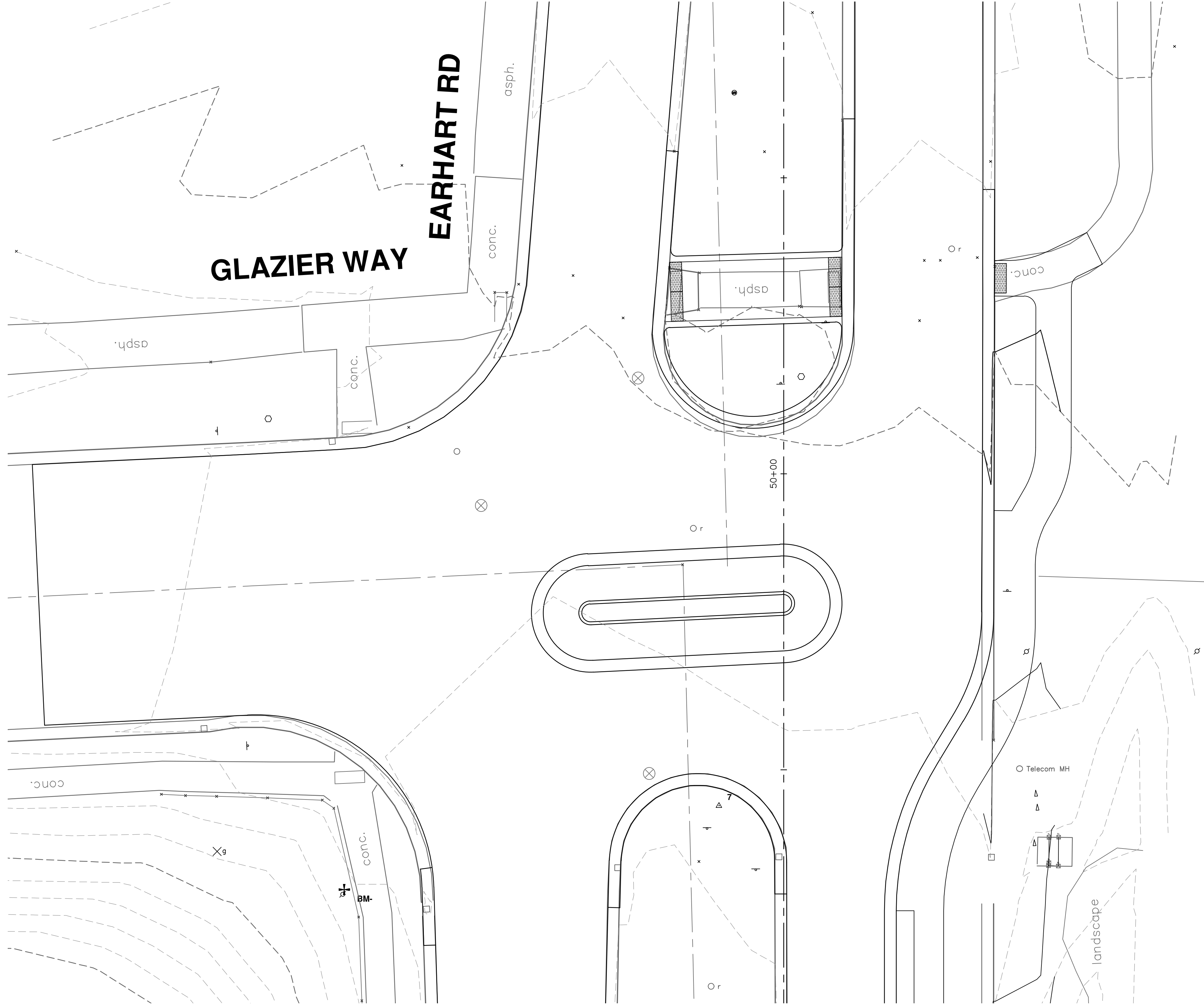
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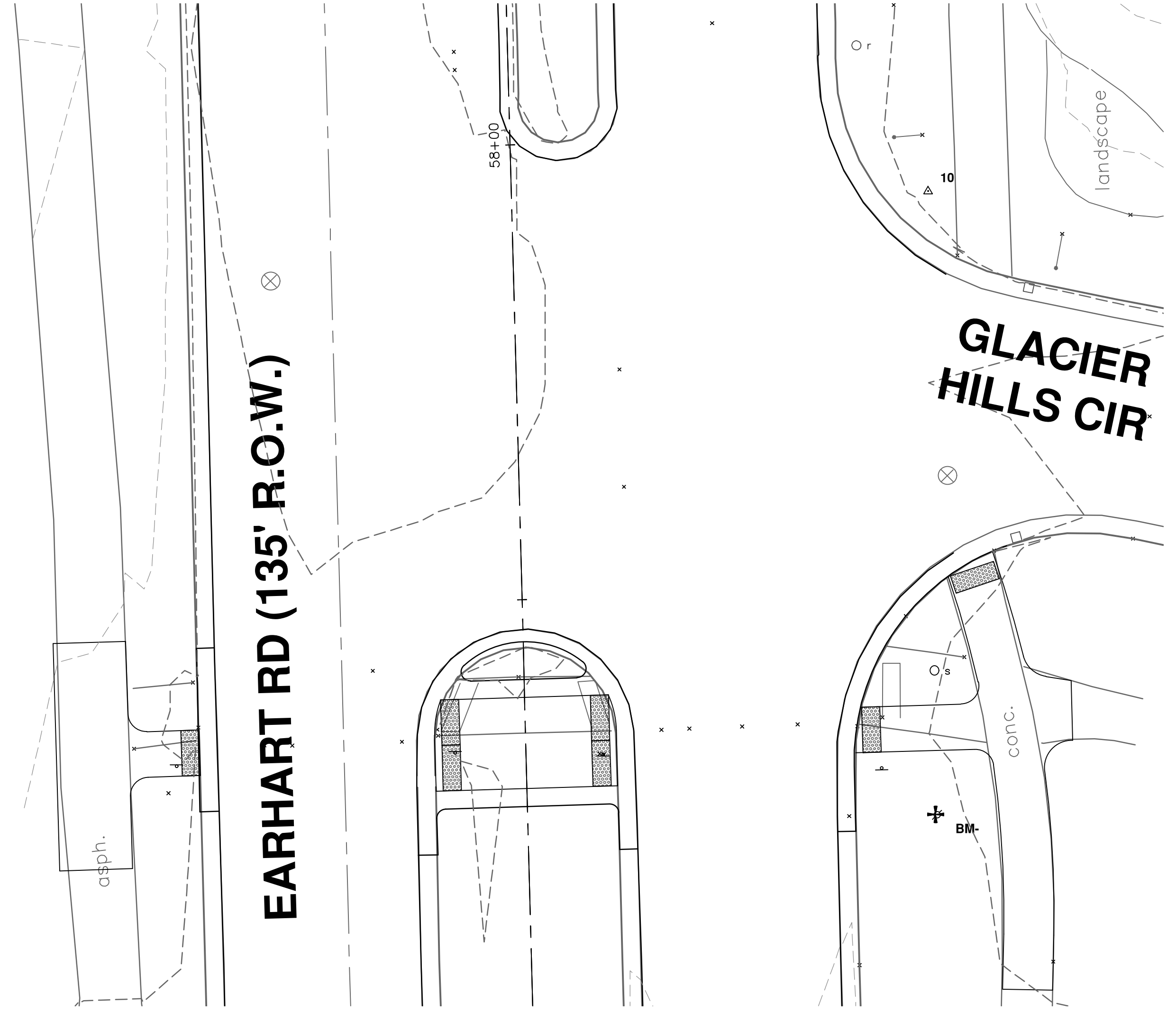




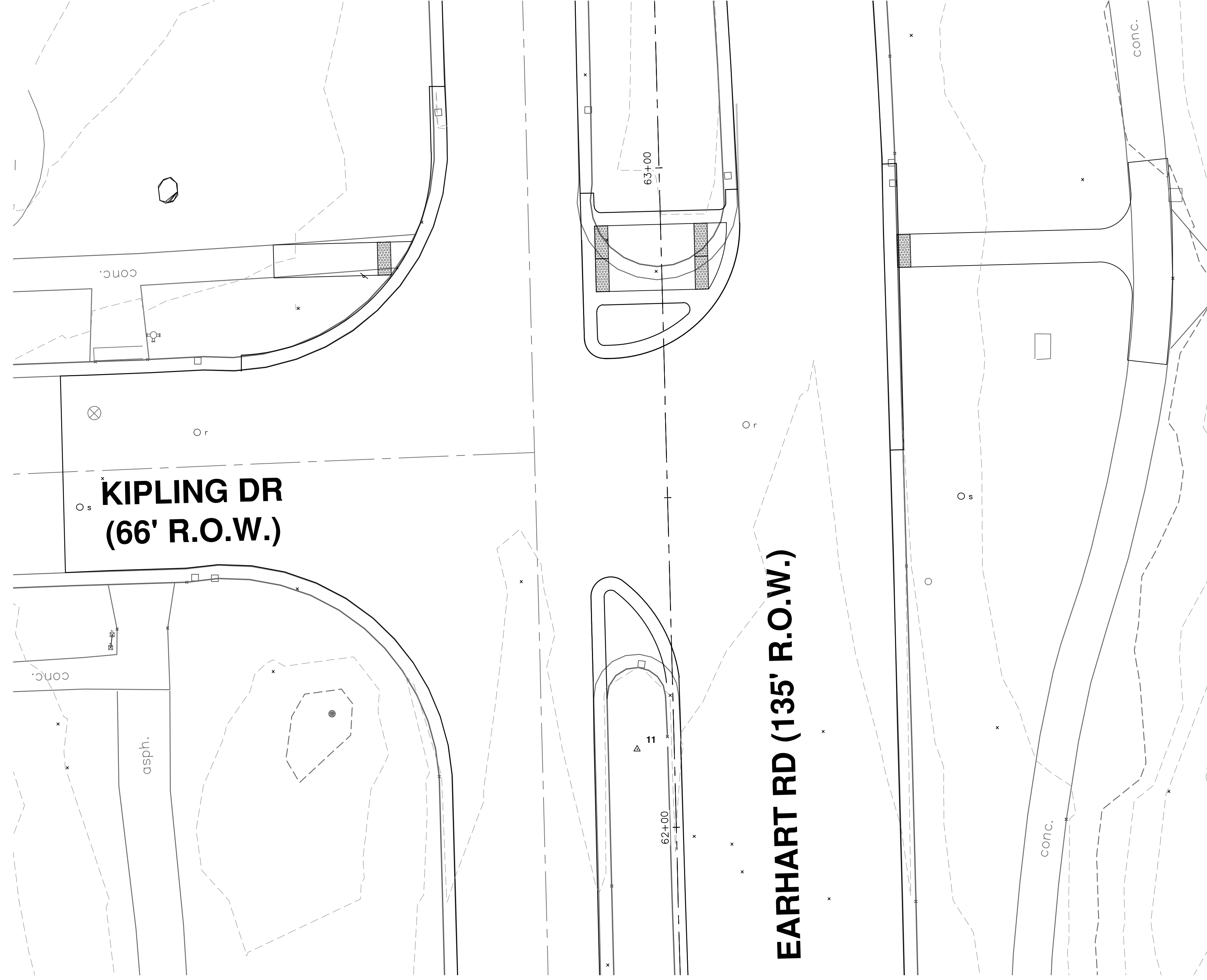
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REV.	DESCRIPTION	DATE	CC/DF	DRAWN	CHECKED
00	OUT TO BID	4-27-23			



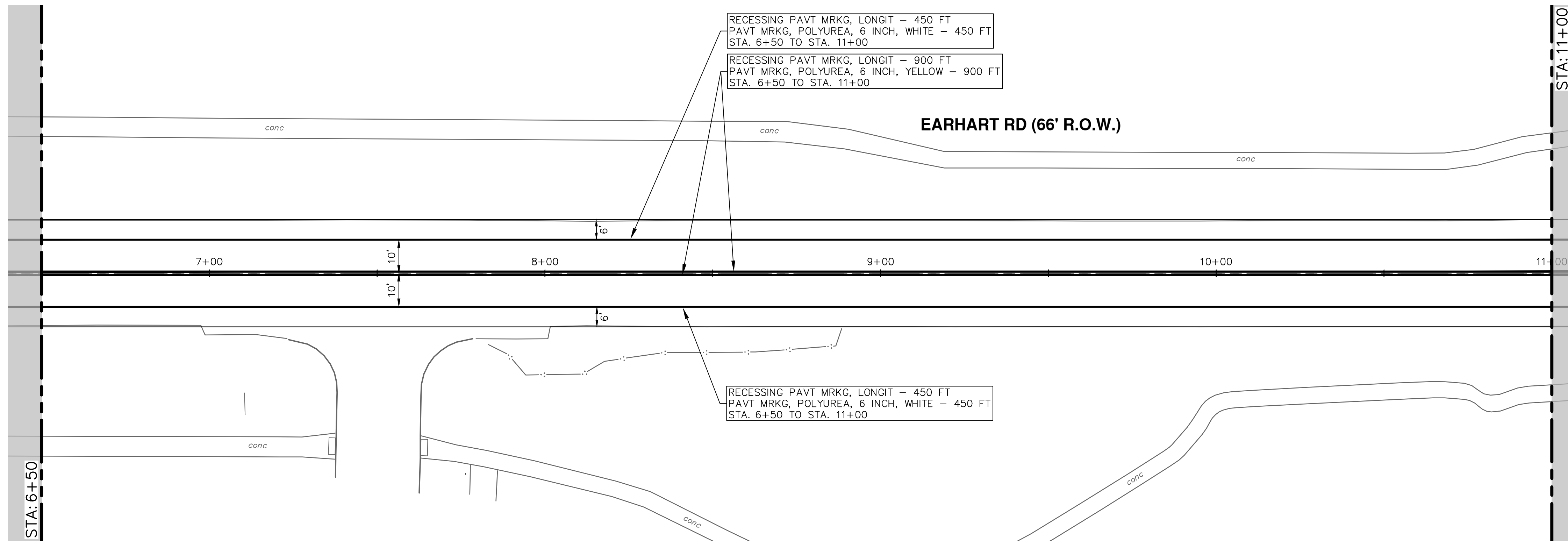
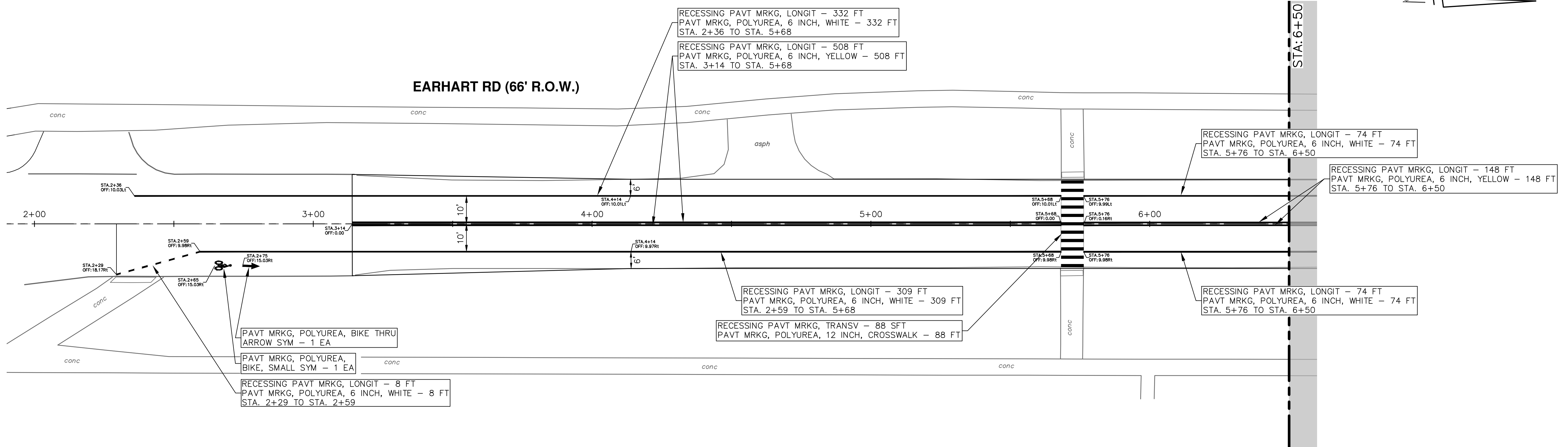



REV.	DESCRIPTION	DATE	CC/DF	NB	CHECKED
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REV.	DESCRIPTION	DATE	CC/DF	DRAWN	CHECKED
00	OUT TO BID	4-27-23			

R:\2021023 Earhart (Geddes to US23)\Plan Production\2021023Pmk.dwg Dwg Created: 25-Apr-23 - \_o2\_standard bw.stb - Plot Date: 27-Apr-23





**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
 PAVEMENT MARKINGS

STA. 2+29 - STA. 11+00

SCALE: 1" = 20'

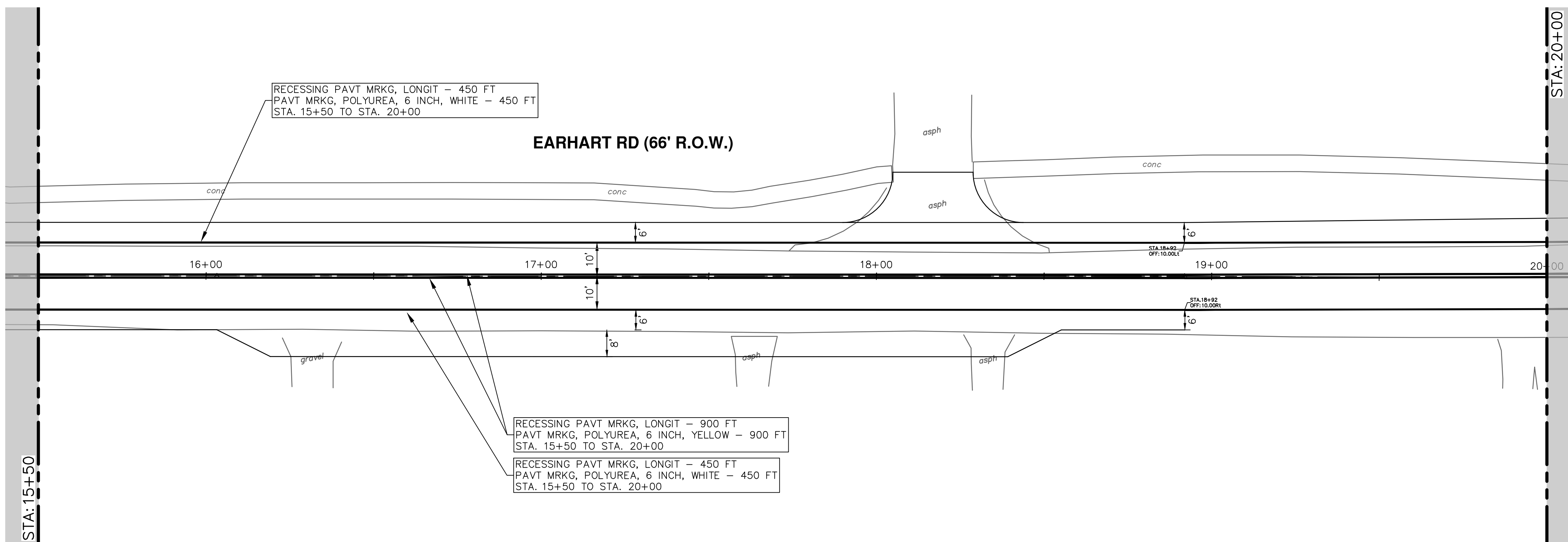
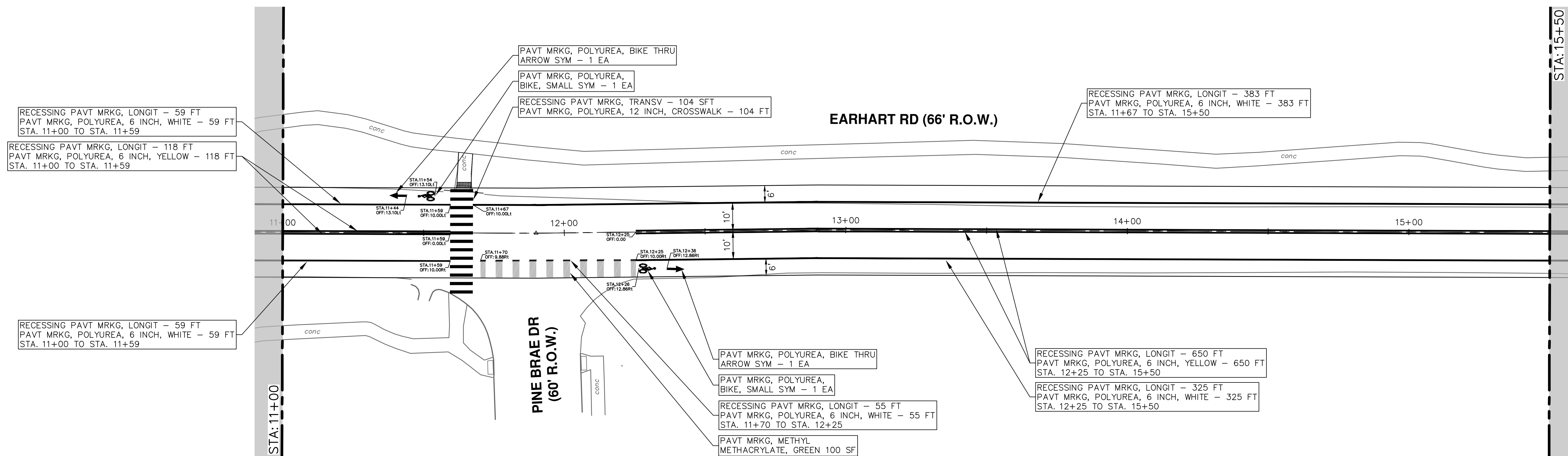
DRAWING No. **2021023-55**

REV.	DATE	DESCRIPTION
00	4-27-23	OUT TO BID
01		CC/DF
02		NB
03		CHECKED

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**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**

PAVEMENT MARKINGS

STA. 11+00 - STA. 20+00

SCALE: 1" = 20'

DRAWING No. 2021023-56

SHEET No. 56 OF 74

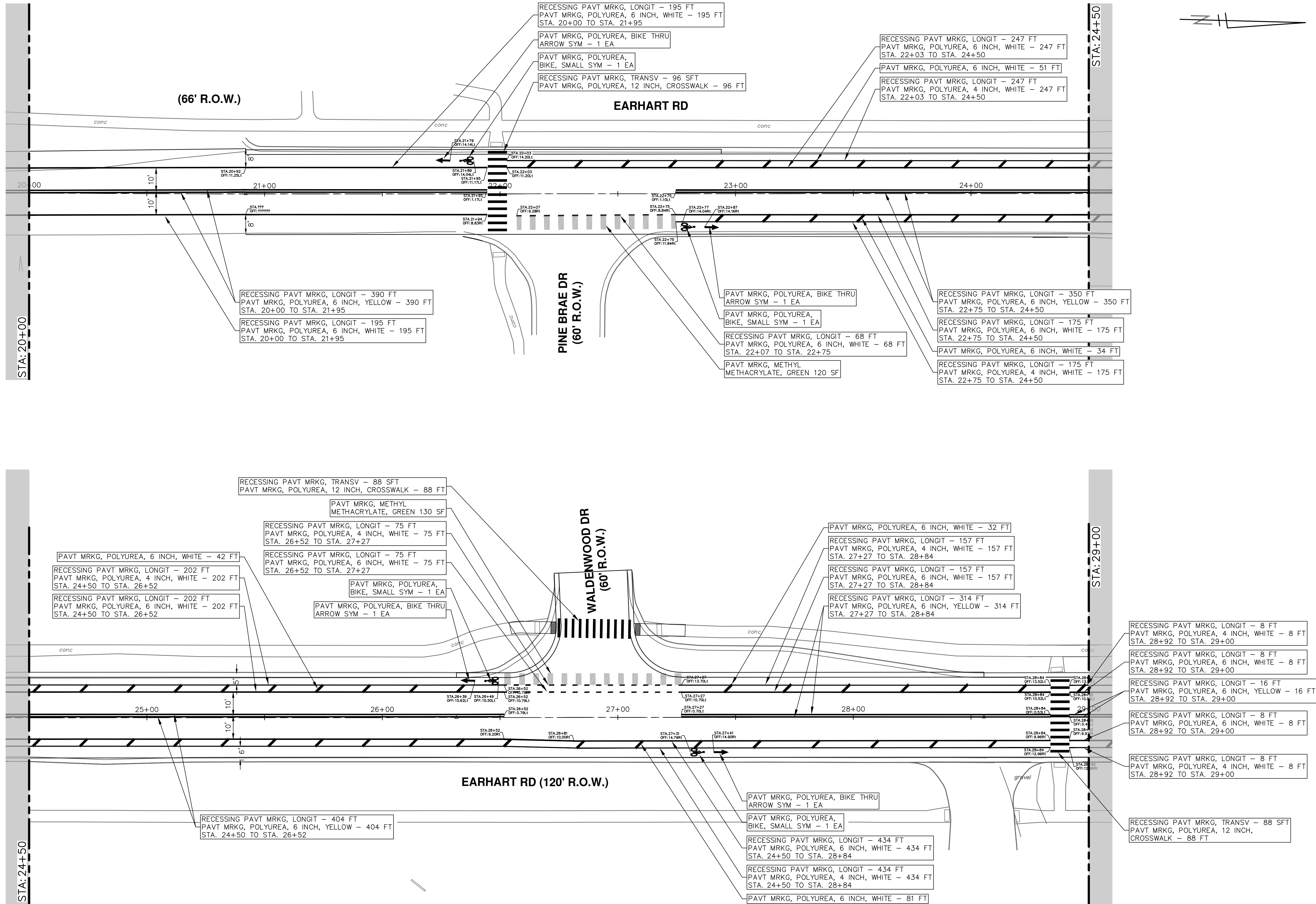
**811**  
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REV.	DESCRIPTION	DATE	DRAWN	CHECKED
00	OUT TO BID	4-27-23	CC/DF	NB

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NO	DATE	DESCRIPTION
4-27-23		

CC/DF DRAWN  
NB CHECKED

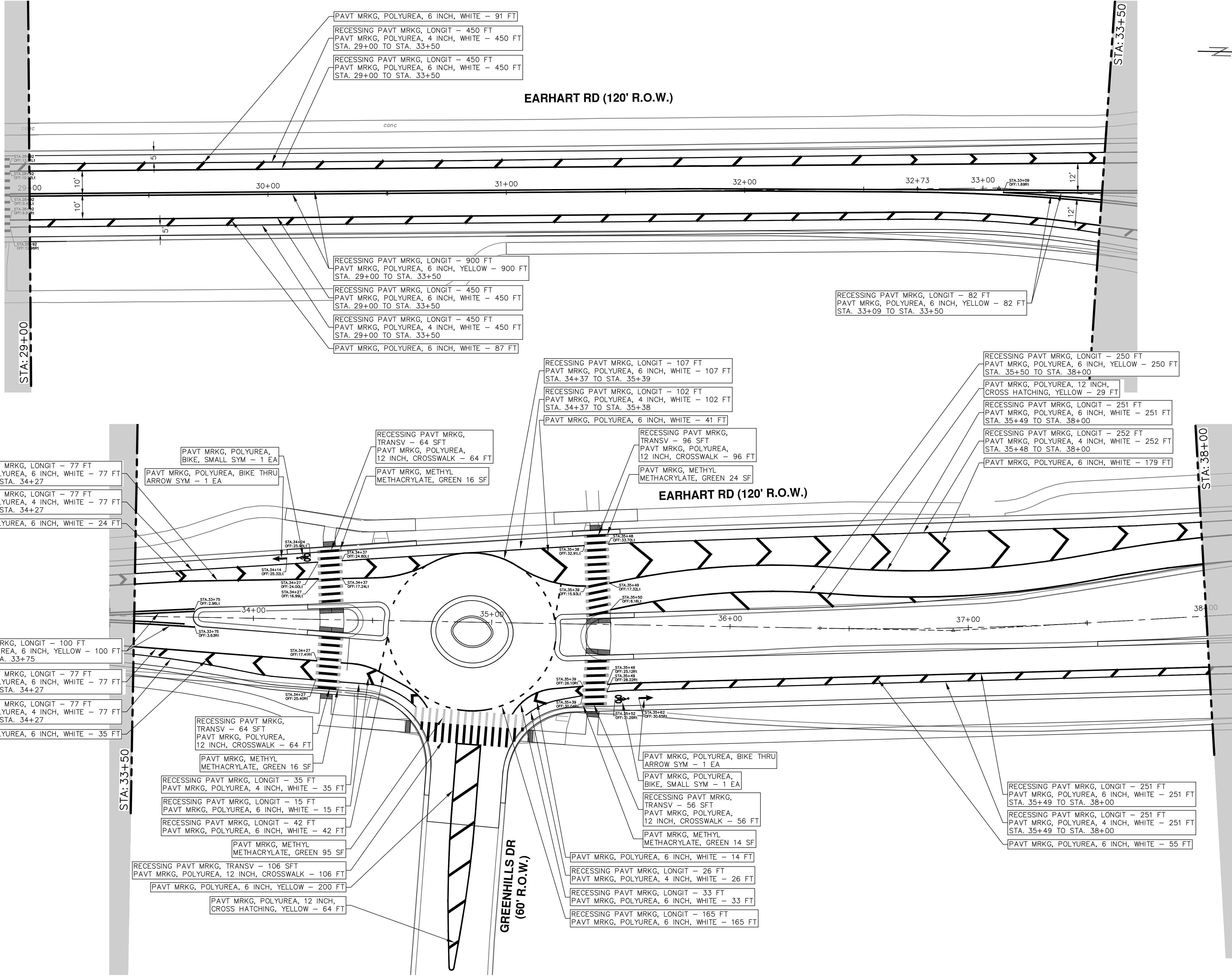
REV. 00 OUT TO BID

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**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
**PAVEMENT MARKINGS**  
STA. 20+00 - STA. 29+00

SCALE: 1" = 20'  
DRAWING No. 2021023-57  
SHEET No. 57 OF 74

R:\2021023 Earhart (Geddes to US23)\Plan Production\2021023Pmk.dwg Dwg Created: 25-Apr-23 - \_o2 standard bw.stb - Plot Date: 27-Apr-23



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00	OUT TO BID	4-27-23		

**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
PAVEMENT MARKINGS  
STA. 29+00 - STA. 38+00

SCALE: 1" = 20'  
DRAWING No. 2021023-58  
SHEET No. 58 OF 74

R:\2021023 Earhart (Geddes to US23)\Plan Production\2021023Pmk.dwg Dwg Created: 25-Apr-23 - \_o2 standard bw.stb - Plot Date: 27-Apr-23

RECESSING PAVT MRKG, LONGIT - 72 FT  
PAVT MRKG, POLYUREA, 4 INCH, WHITE - 72 FT  
STA. 38+45 TO STA. 39+16

RECESSING PAVT MRKG, LONGIT - 72 FT  
PAVT MRKG, POLYUREA, 6 INCH, WHITE - 72 FT  
STA. 38+44 TO STA. 39+16

PAVT MRKG, POLYUREA,  
BIKE, SMALL SYM - 1 EA

PAVT MRKG, POLYUREA, BIKE THRU  
ARROW SYM - 1 EA

PAVT MRKG, POLYUREA, 6 INCH, WHITE - 17 FT

RECESSING PAVT MRKG, LONGIT - 44 FT  
PAVT MRKG, POLYUREA, 4 INCH, WHITE - 44 FT  
STA. 38+00 TO STA. 38+44

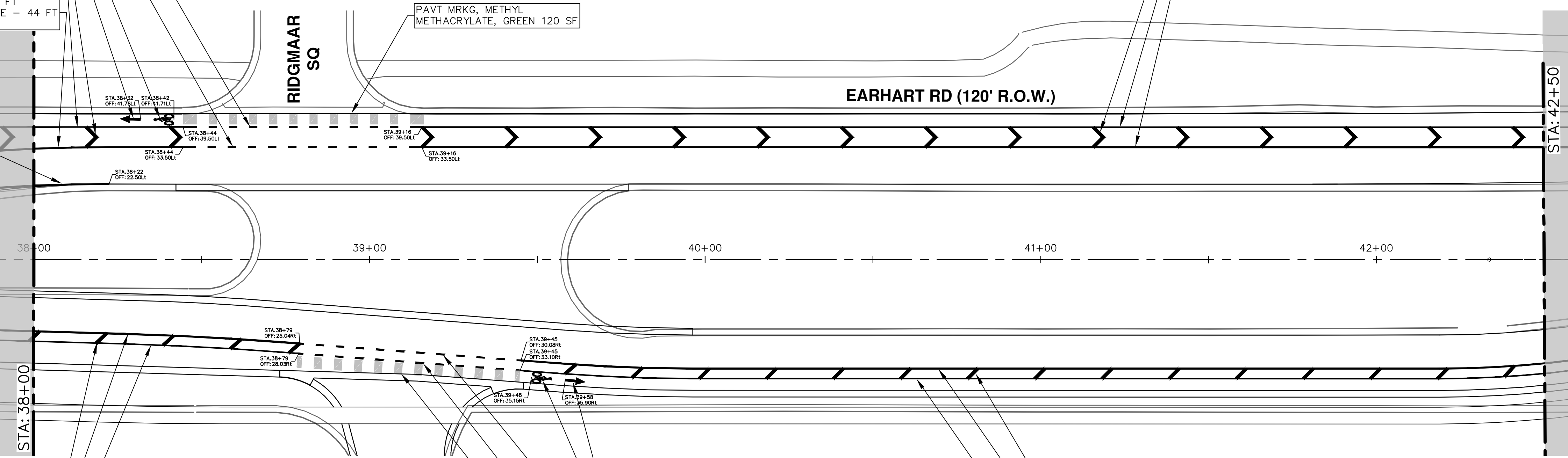
RECESSING PAVT MRKG, LONGIT - 44 FT  
PAVT MRKG, POLYUREA, 6 INCH, WHITE - 44 FT  
STA. 38+00 TO STA. 38+44

RECESSING PAVT MRKG, LONGIT - 22 FT  
PAVT MRKG, POLYUREA, 6 INCH,  
YELLOW - 22 FT  
STA. 38+00 TO STA. 38+22

PAVT MRKG, POLYUREA, 6 INCH, WHITE - 21 FT

RECESSING PAVT MRKG, LONGIT - 79 FT  
PAVT MRKG, POLYUREA, 6 INCH, WHITE - 79 FT  
STA. 38+00 TO STA. 38+79

RECESSING PAVT MRKG, LONGIT - 79 FT  
PAVT MRKG, POLYUREA, 4 INCH, WHITE - 79 FT  
STA. 38+00 TO STA. 38+79



REV.	DATE	DESCRIPTION
00	4-27-23	OUT TO BID
		CC/DF
		DRAWN
		CHECKED
		NB

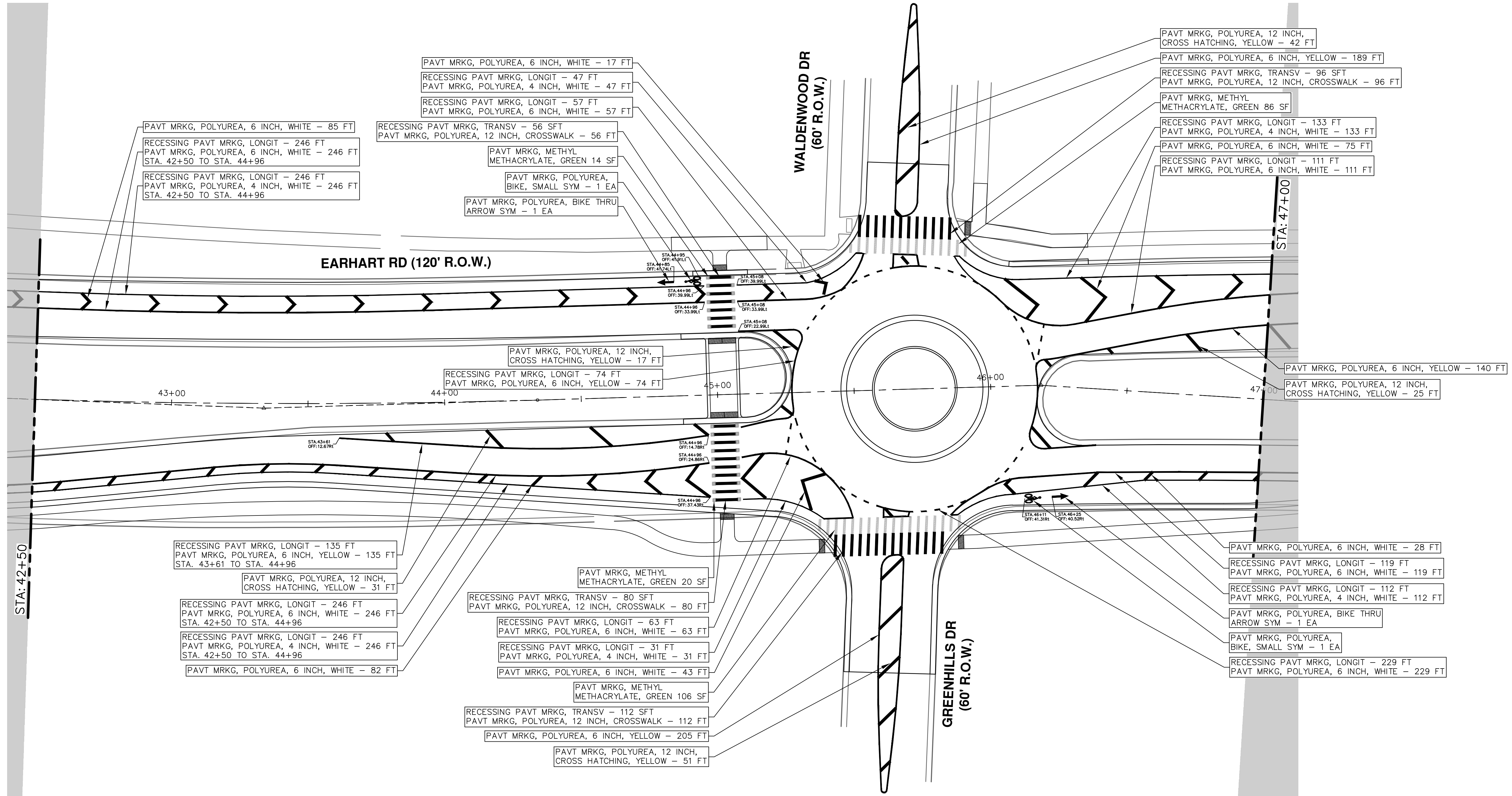
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EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
PAVEMENT MARKINGS  
STA. 38+00 - STA. 42+50

SCALE: 1" = 20'  
DRAWING No. 2021023-59  
SHEET No. 59 OF 74

R:\2021023 Earnhart (Geddes to US23)\Plan Production\2021023Pmk.dwg Dwg Created: 25-Apr-23 - \_o2 standard bw.stb - Plot Date: 27-Apr-23



REV.	DATE	DESCRIPTION
00	4-27-23	OUT TO BID
		CC/DF
		DRAWN
		CHECKED
		NB

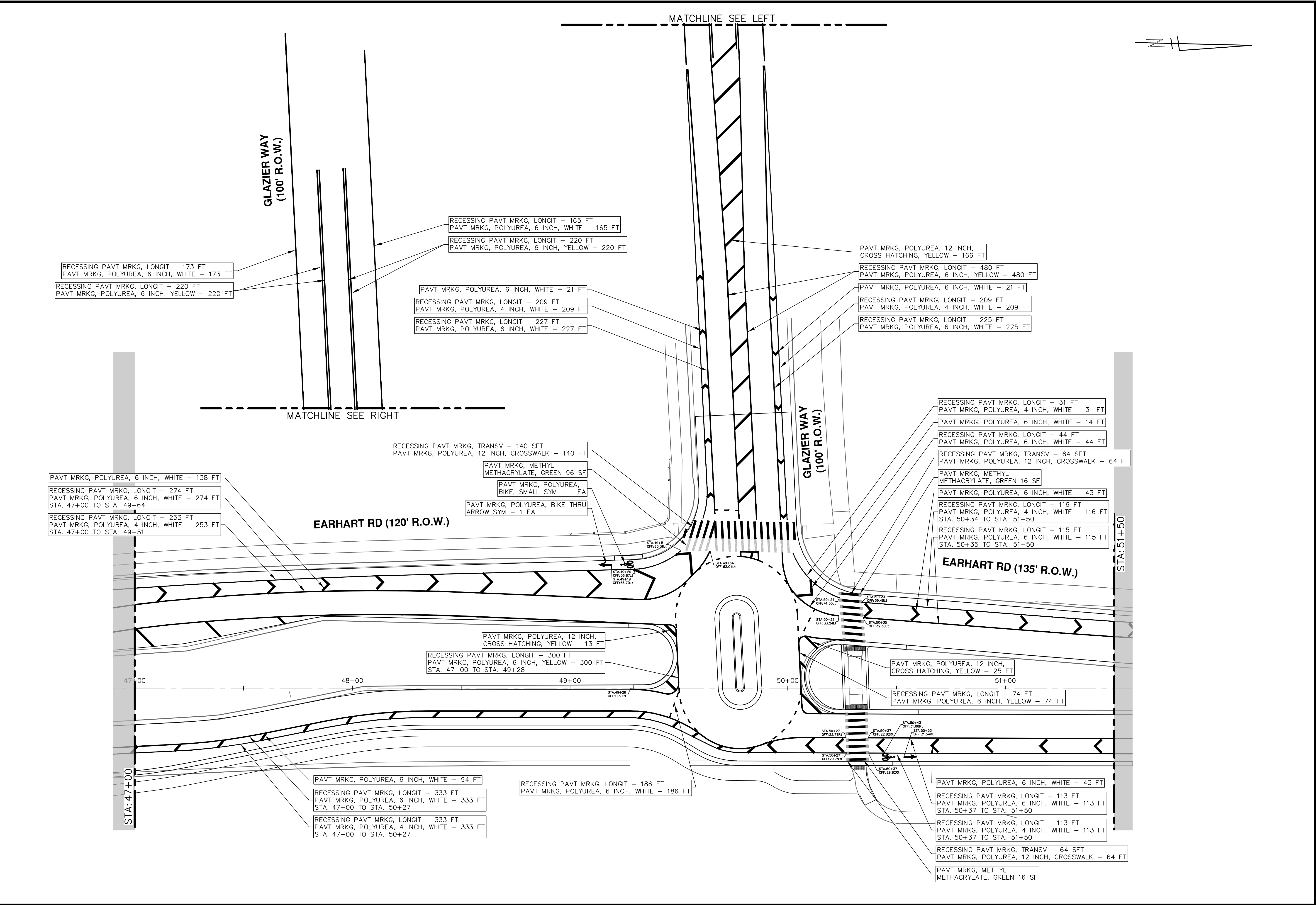
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EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
PAVEMENT MARKINGS  
49 STA. 42+50 - STA. 47+00

SHEET No. 60 OF 74  
DRAWING No. 2021023-60  
SCALE: #####

R:\2021023 Earhart (Geddes to US23)\Plan Production\2021023Pmk.dwg Dwg Created: 25-Apr-23 - s02 standard bw.stb - Plot Date: 27-Apr-23



**811**  
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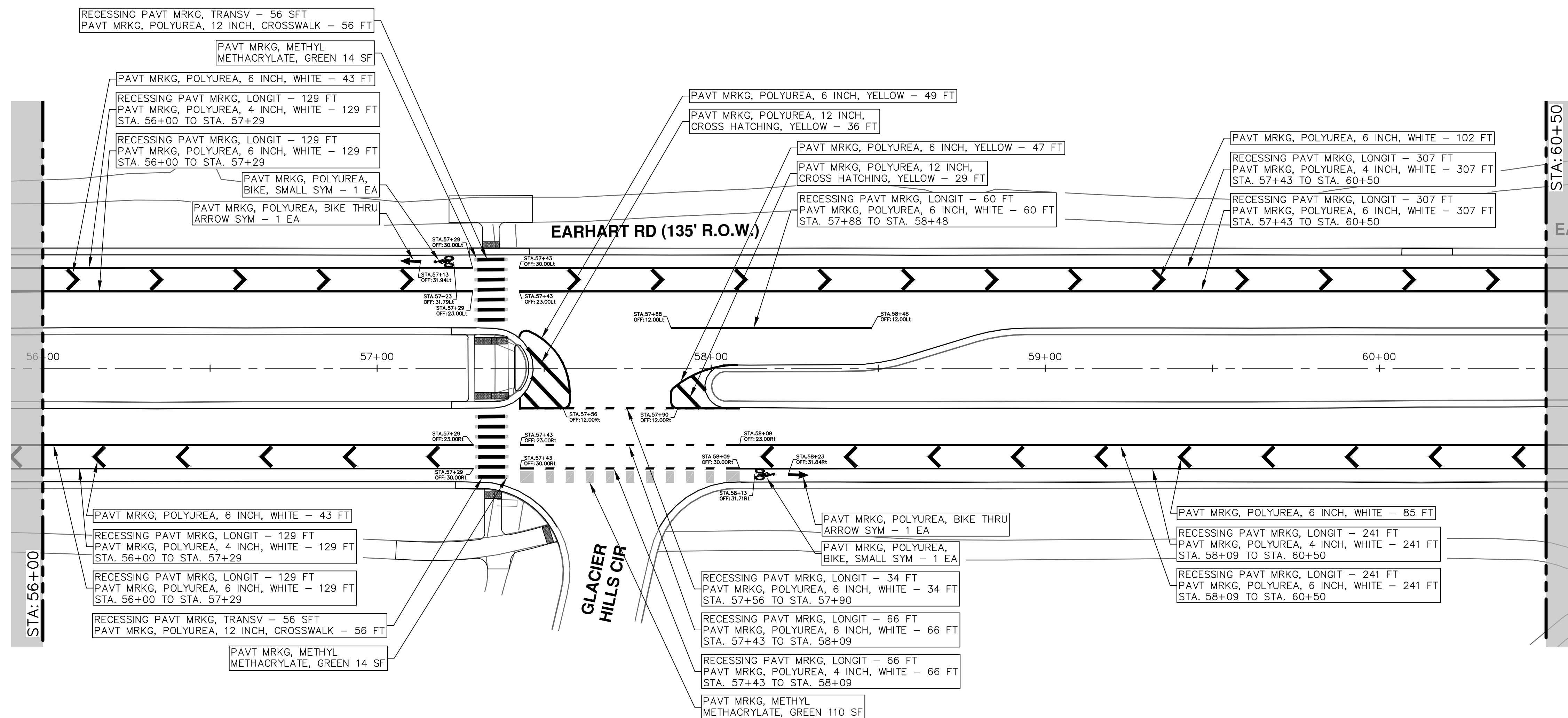
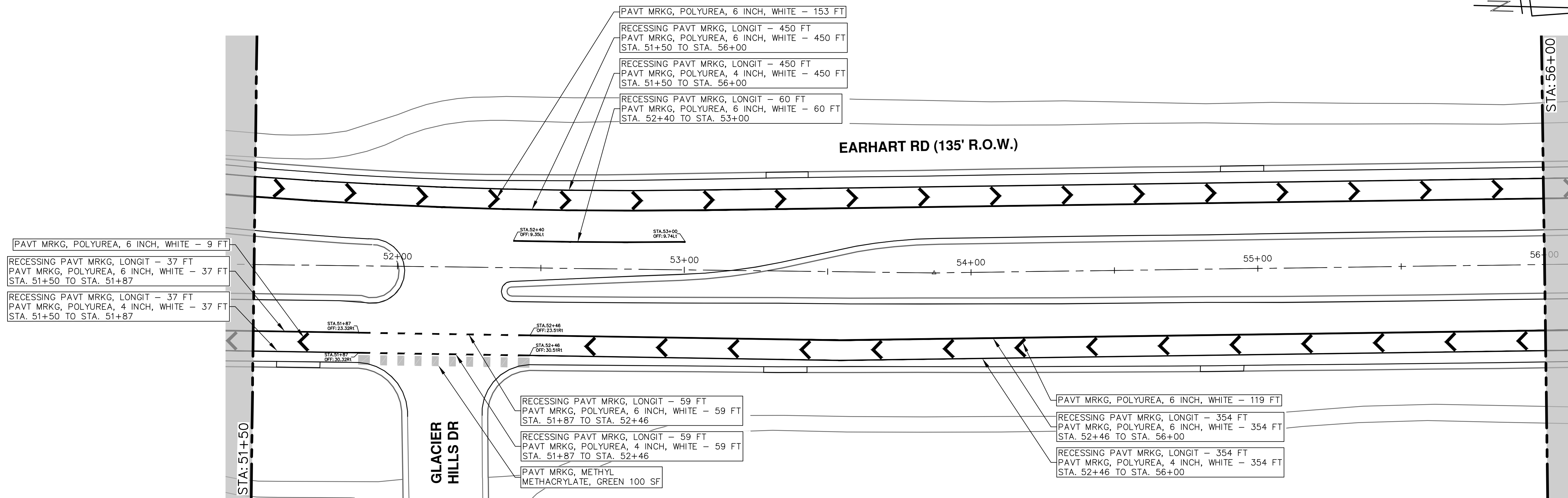
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**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
PAVEMENT MARKINGS

DRAWING No. 2021023-61  
SHEET No. 61 OF 74

R:\2021023 Earhart (Geddes to US23)\Plan Production\2021023Pmk.dwg Dwg Created: 25-Apr-23 - \_o2 standard bw.stb - Plot Date: 27-Apr-23



**811**  
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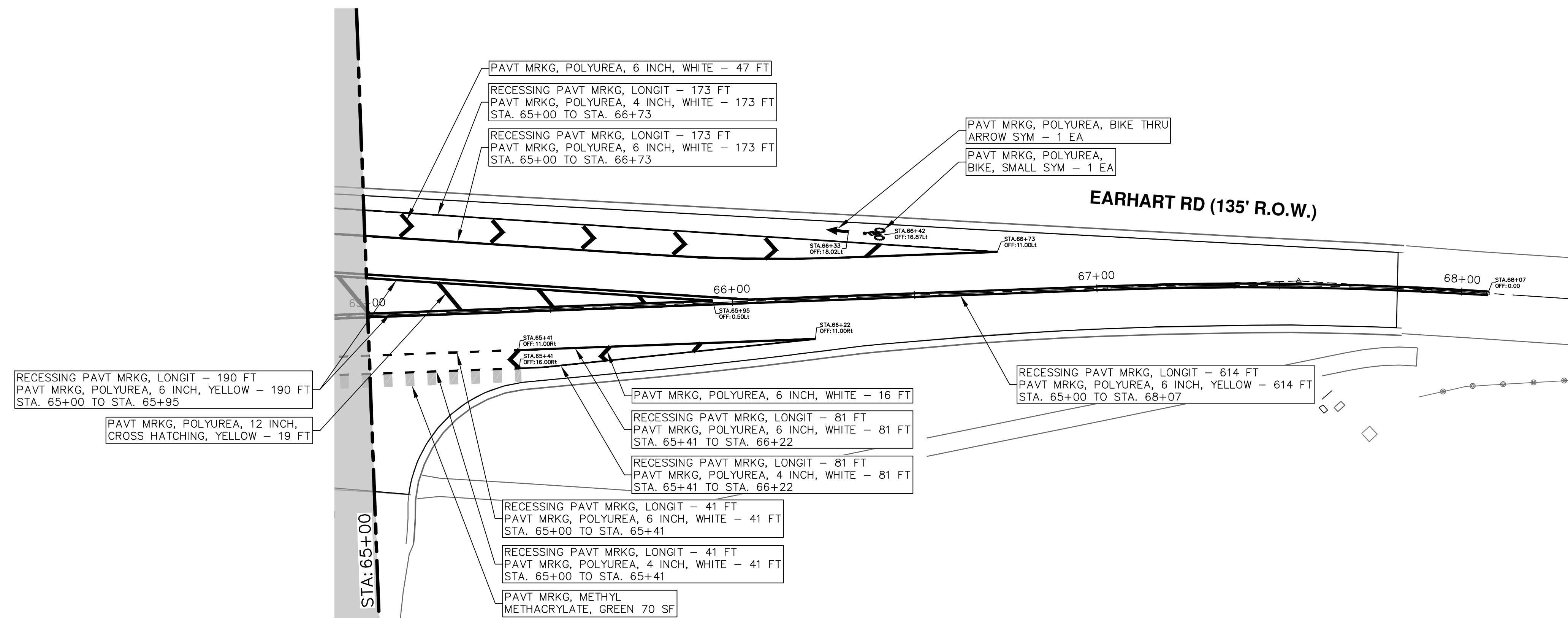
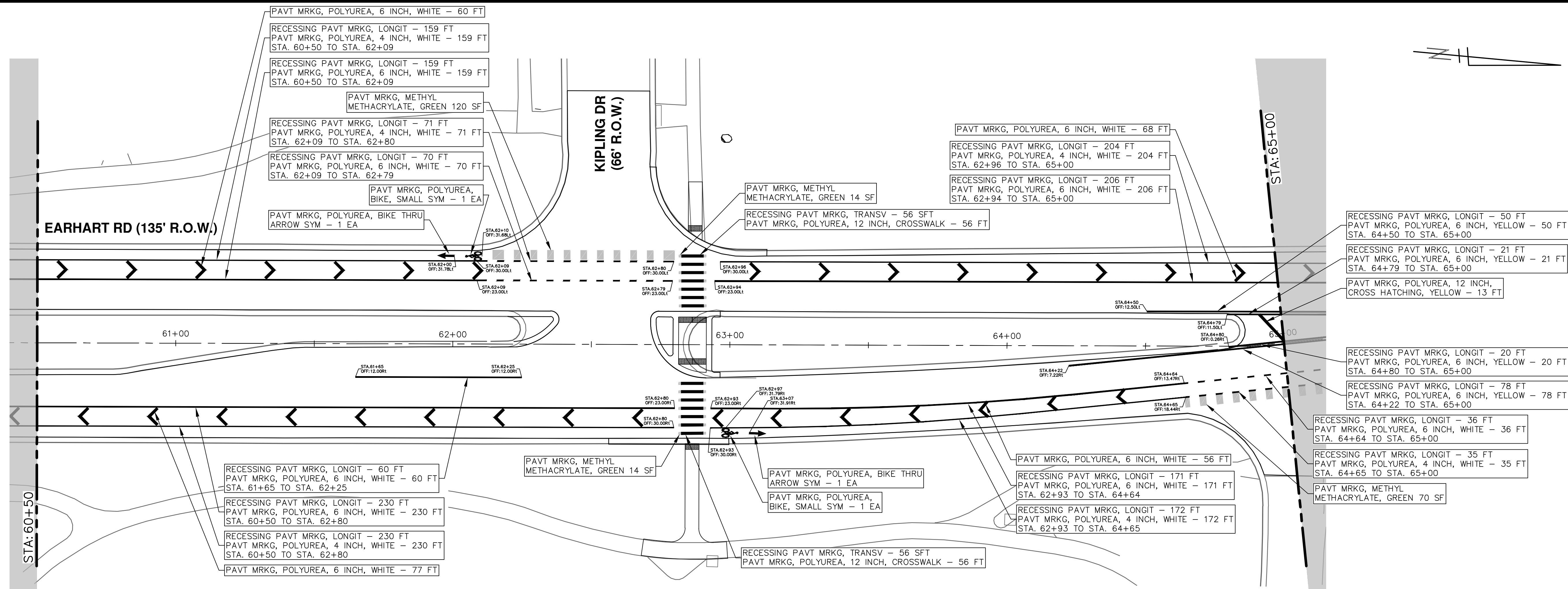
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**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
PAVEMENT MARKINGS

STA. 51+50 - STA. 60+50

SCALE: 1" = 20'

DRAWING No. **2021023-62**  
SHEET No. **62 OF 74**

R:\2021023 Earhart (Geddes to US23)\Plan Production\2021023Pmk.dwg Dwg Created: 25-Apr-23 - \_o2\_standard bw.stb - Plot Date: 27-Apr-23



**811**  
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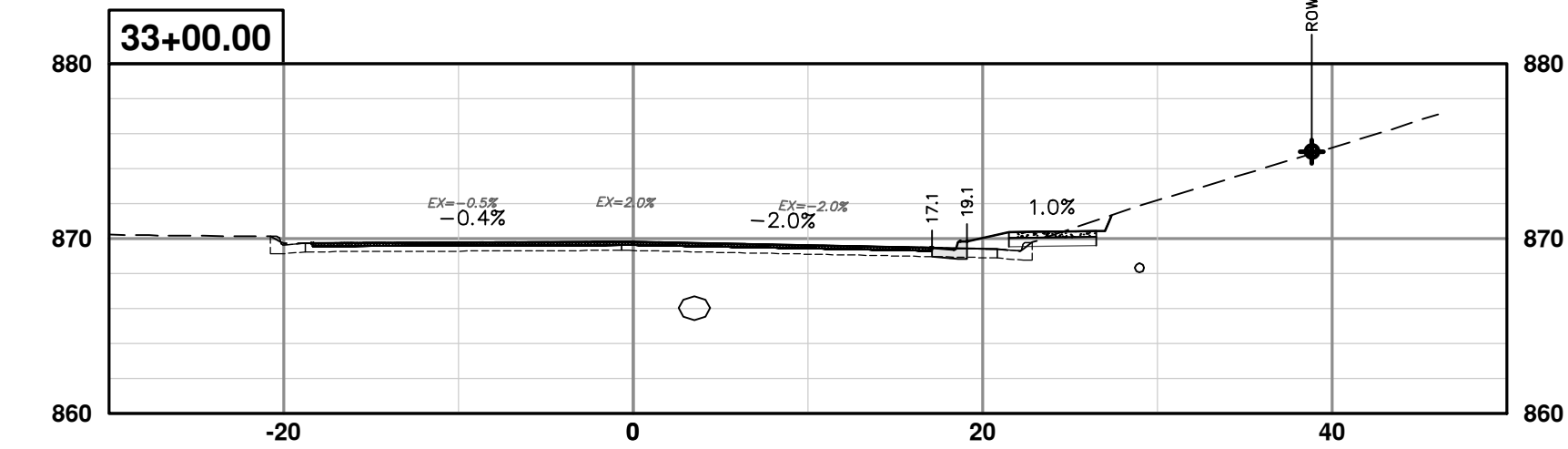
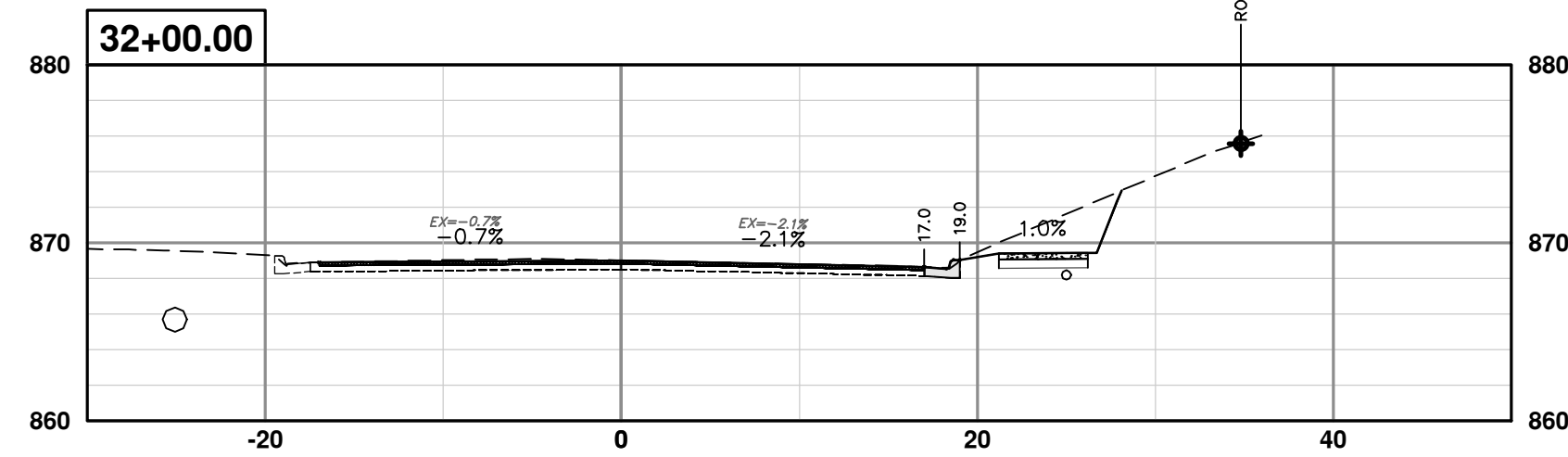
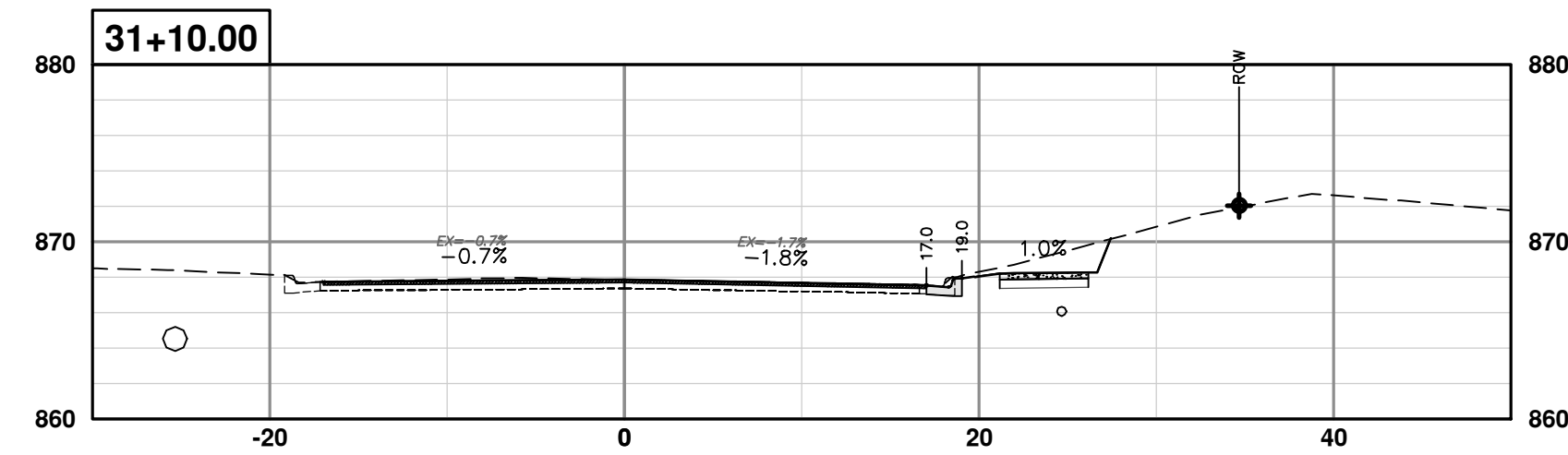
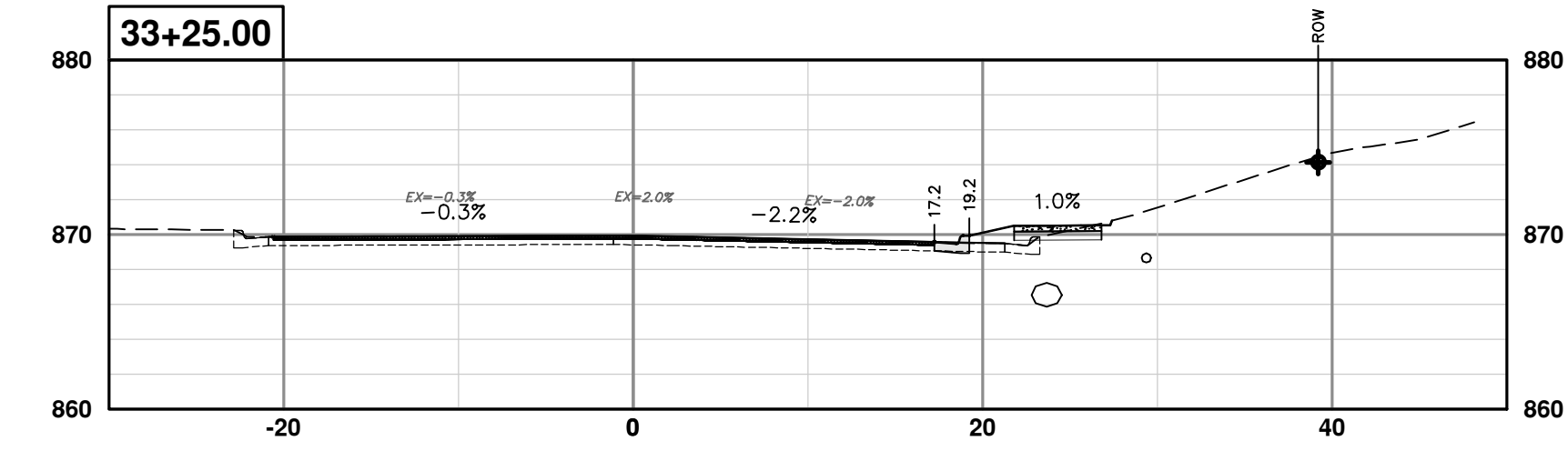
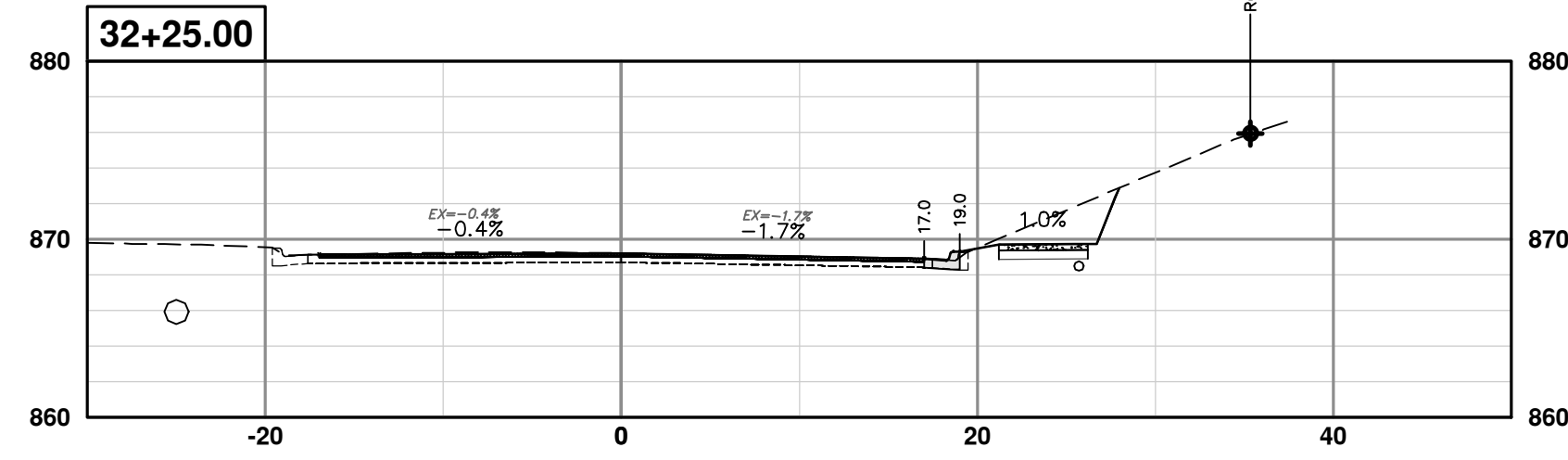
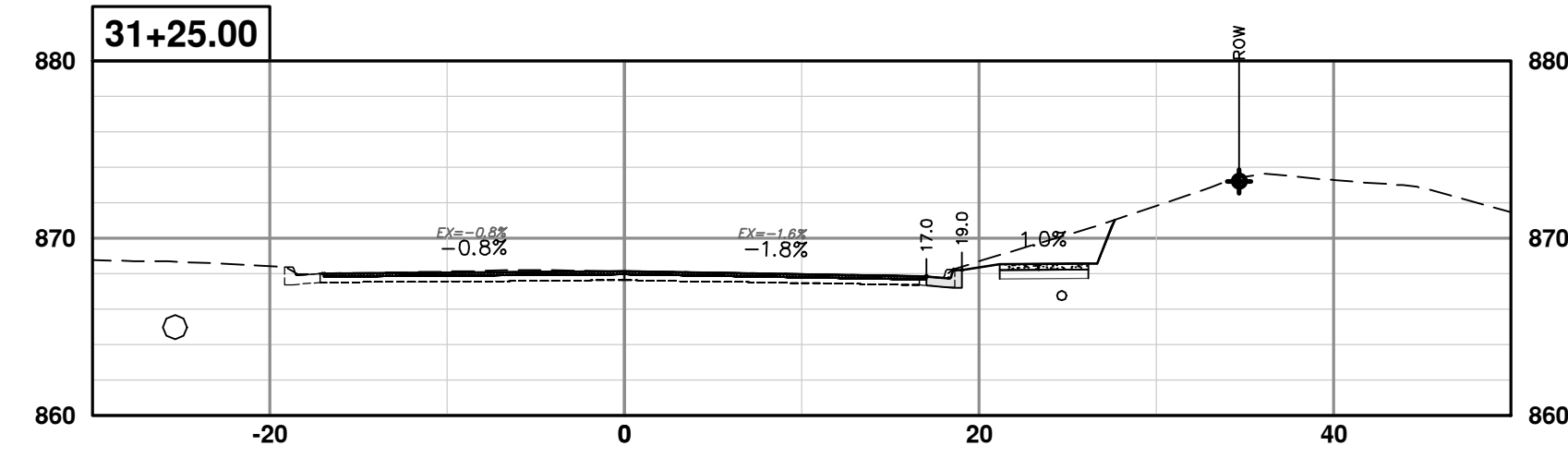
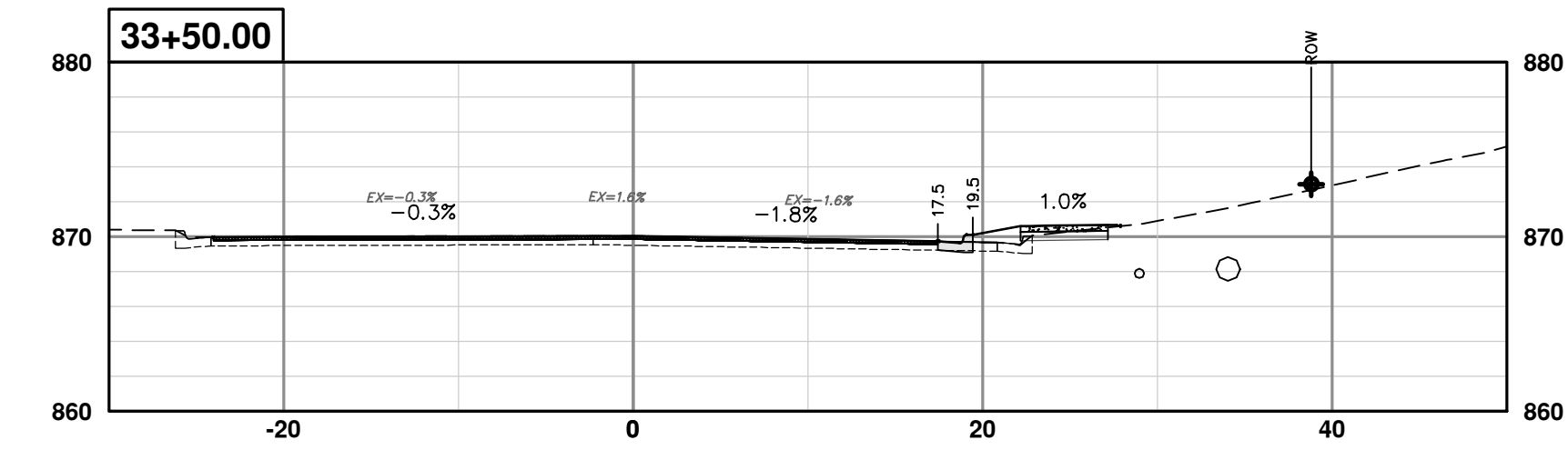
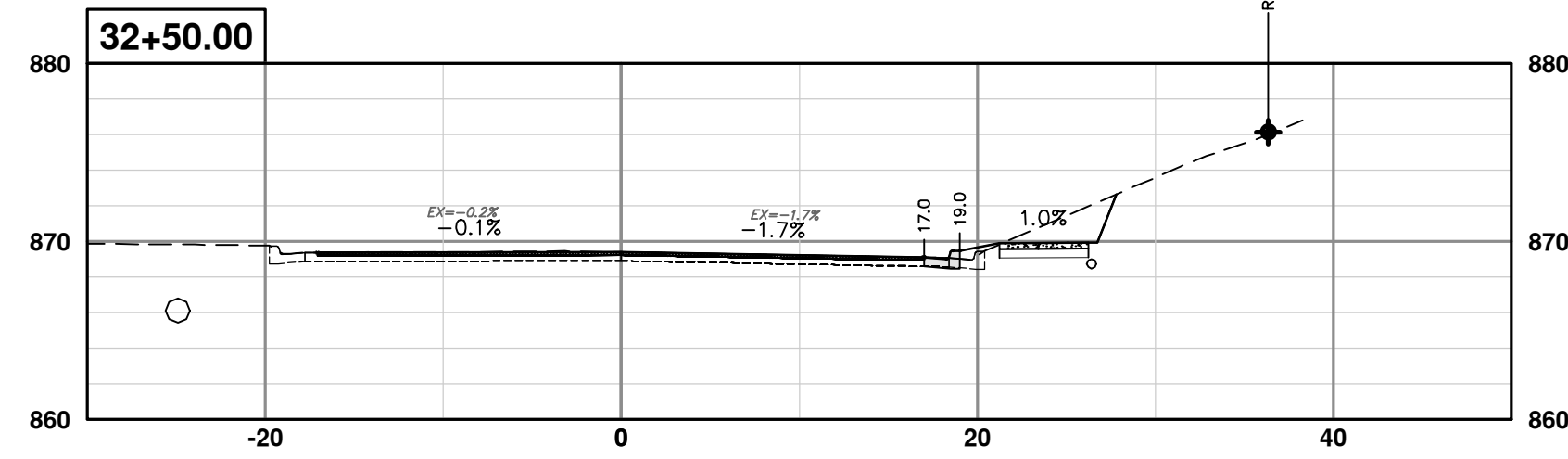
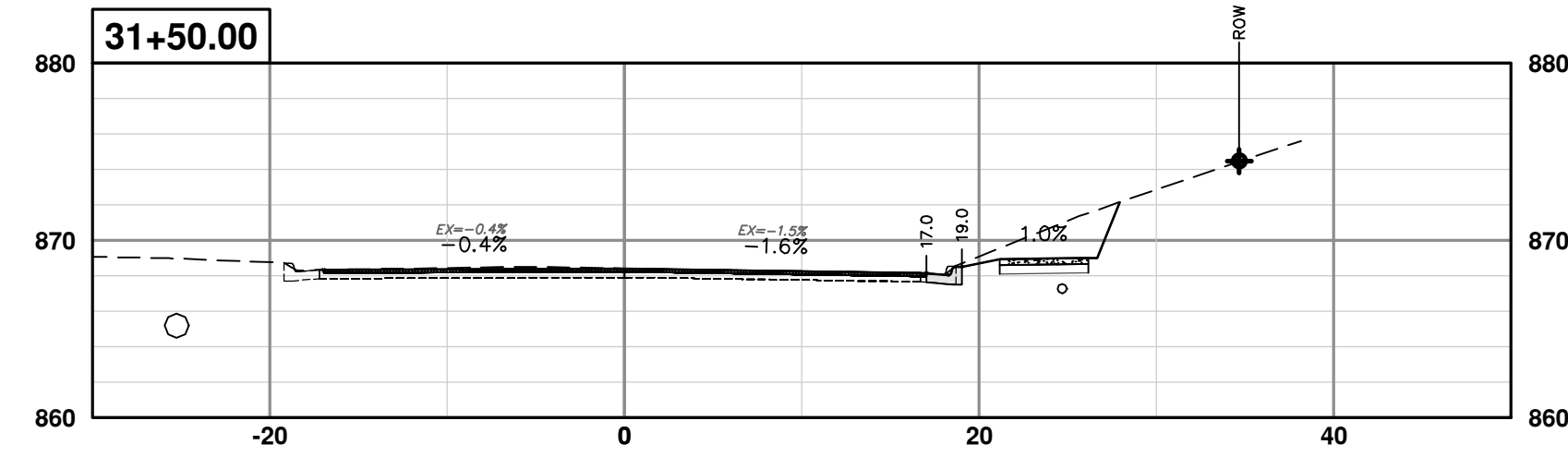
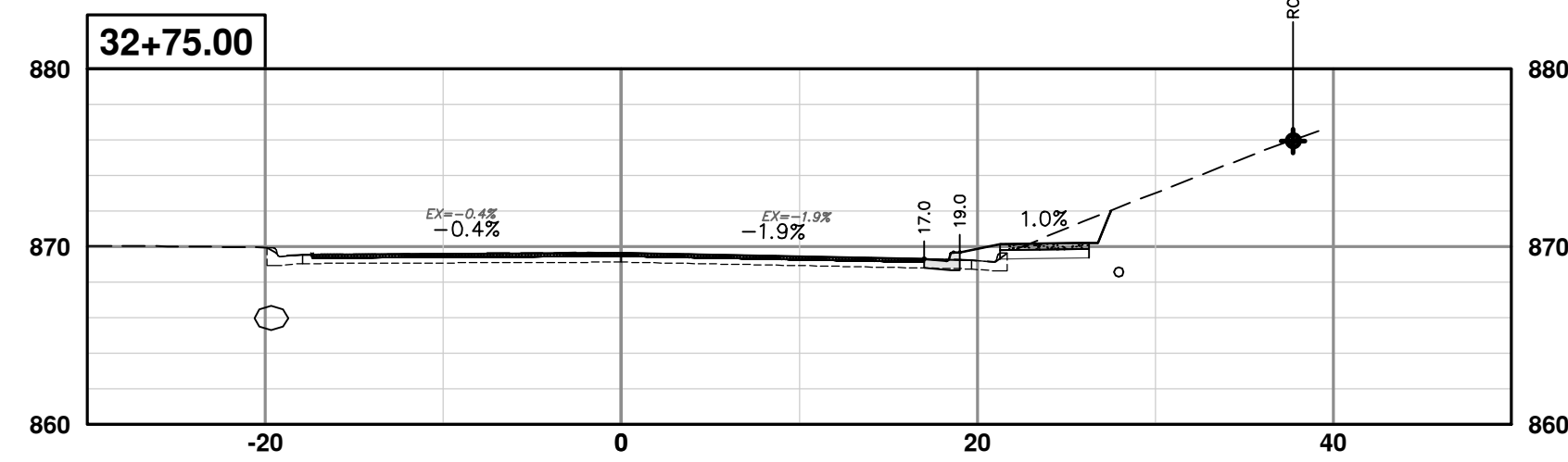
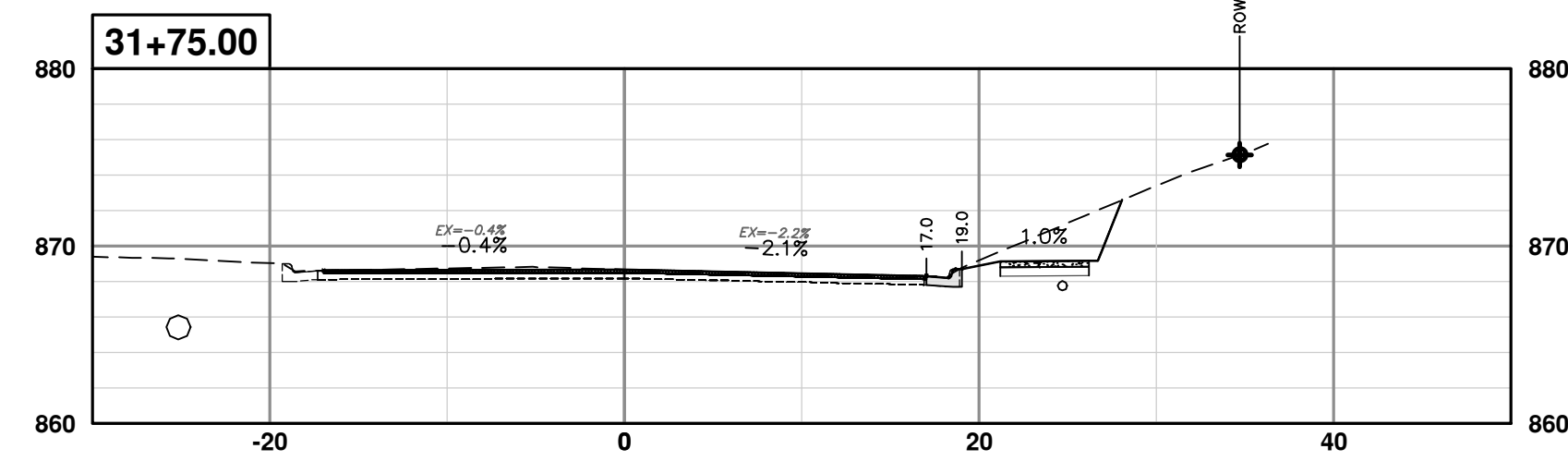
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EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
PAVEMENT MARKINGS  
STA. 60+50 - 68+07

SHEET No.  
**63 OF 74**

SCALE: 1" = 20'  
DRAWING No.  
**2021023-63**

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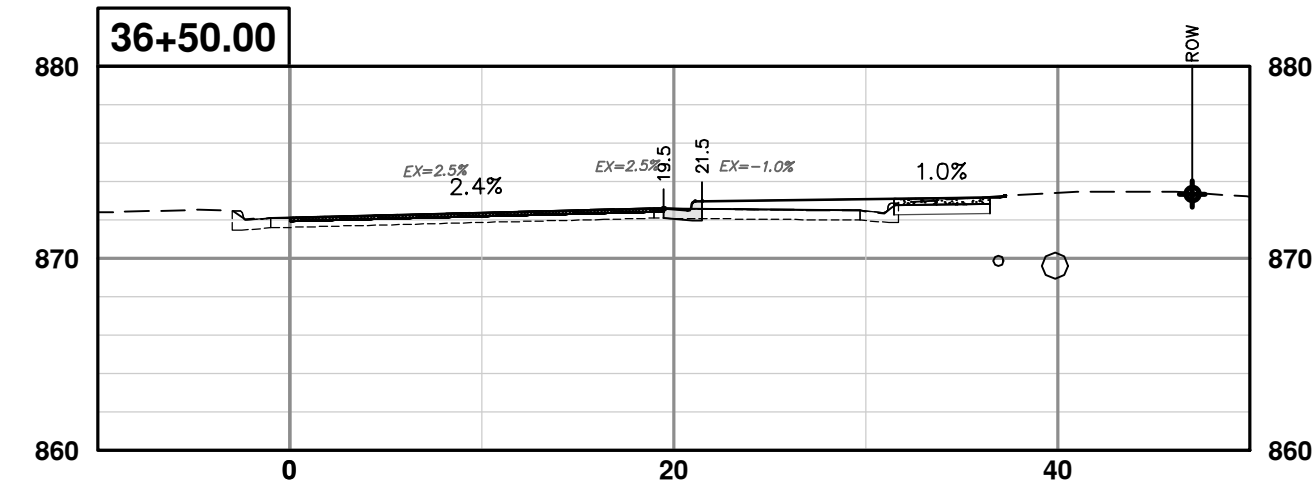
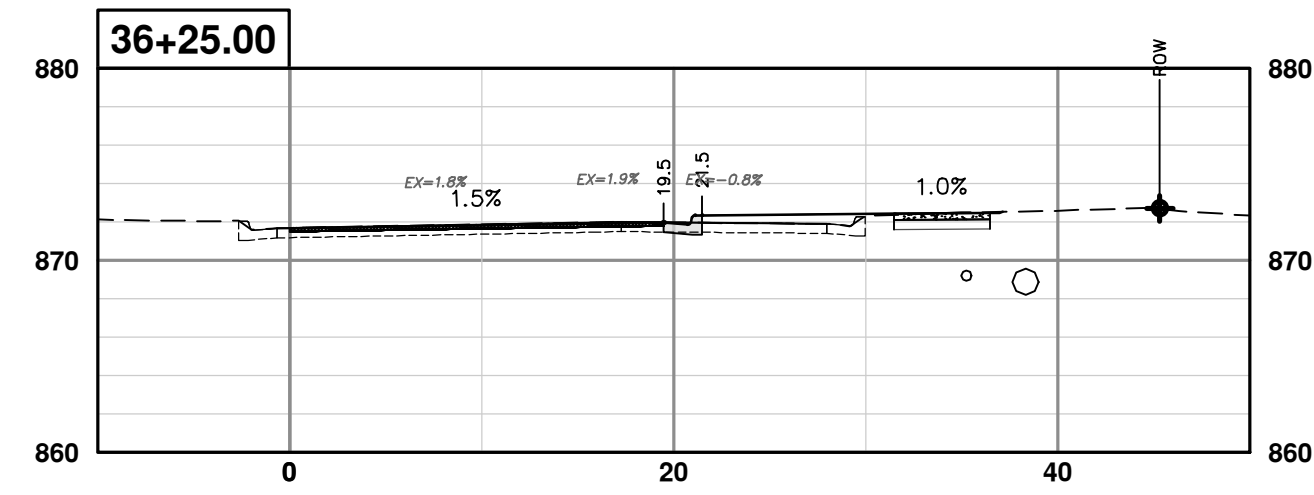
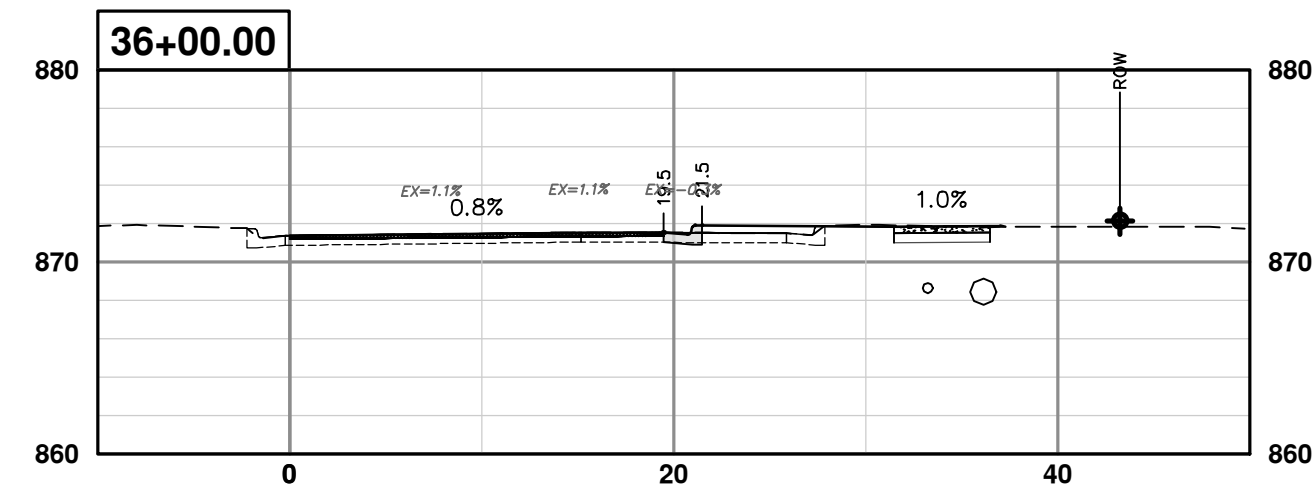
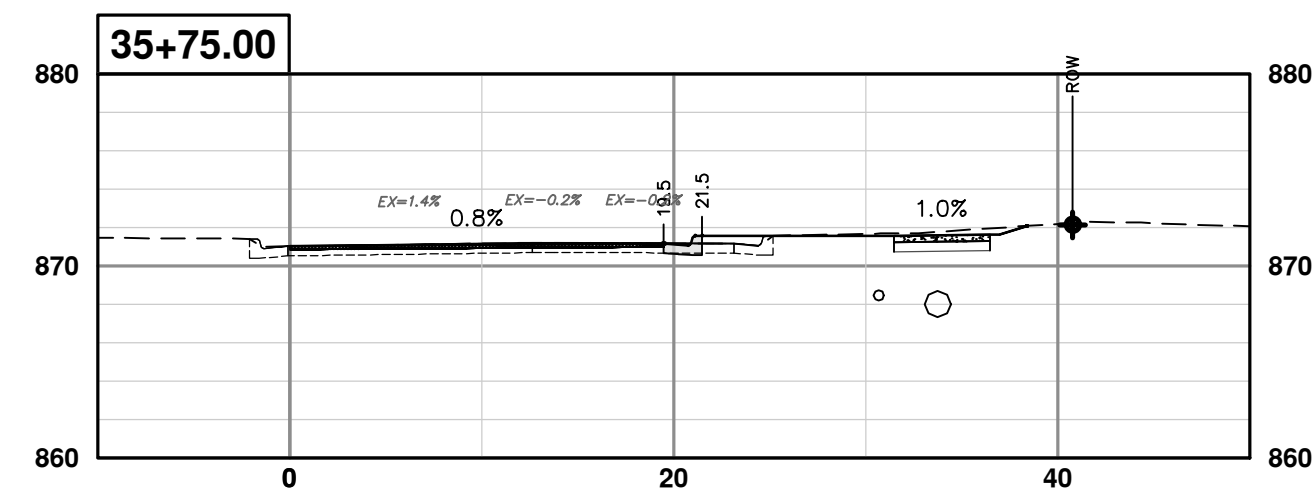
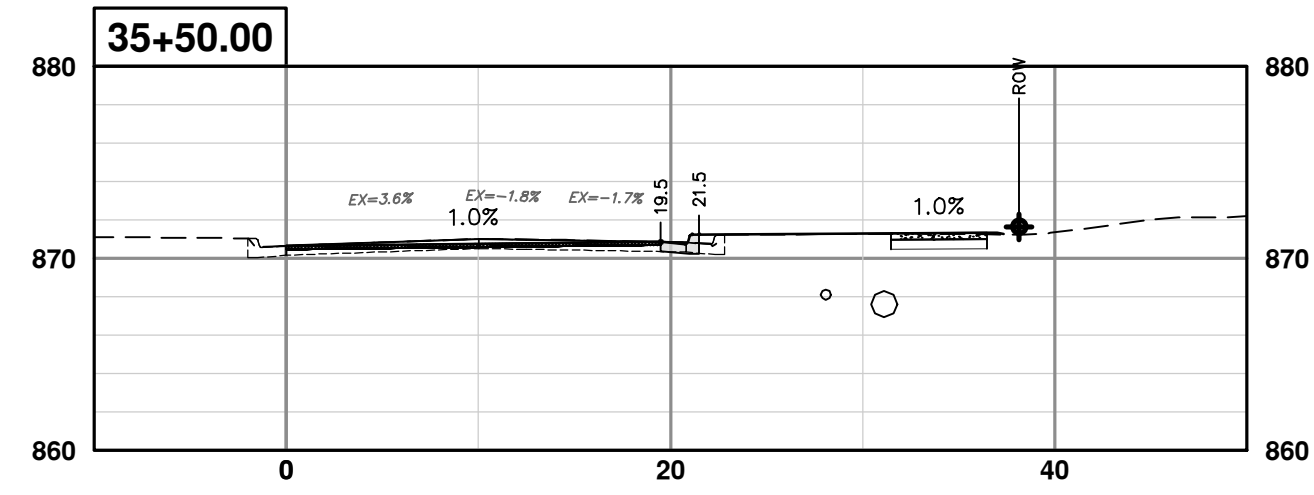
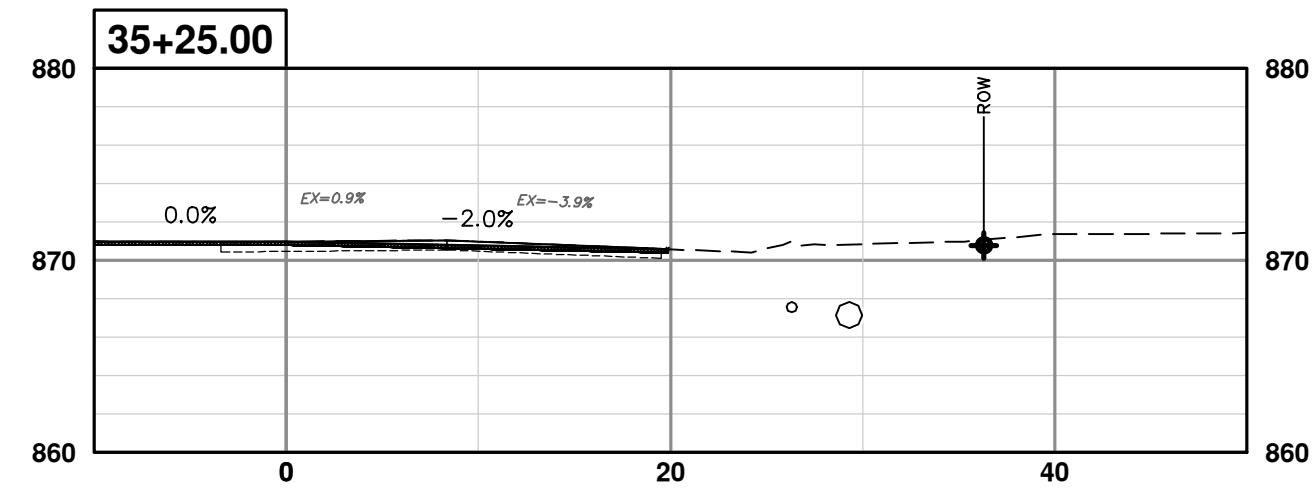
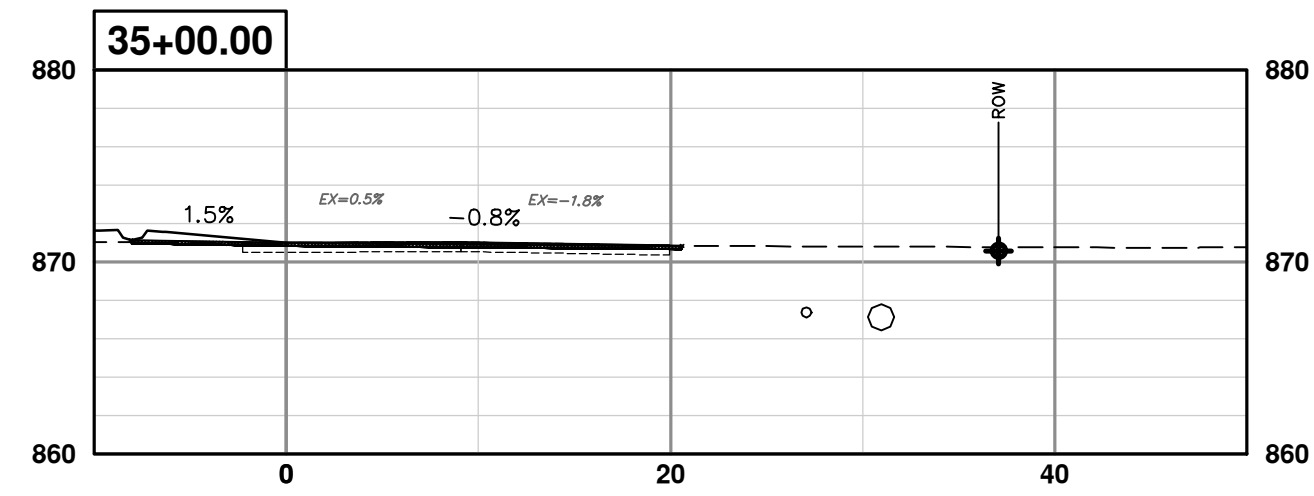
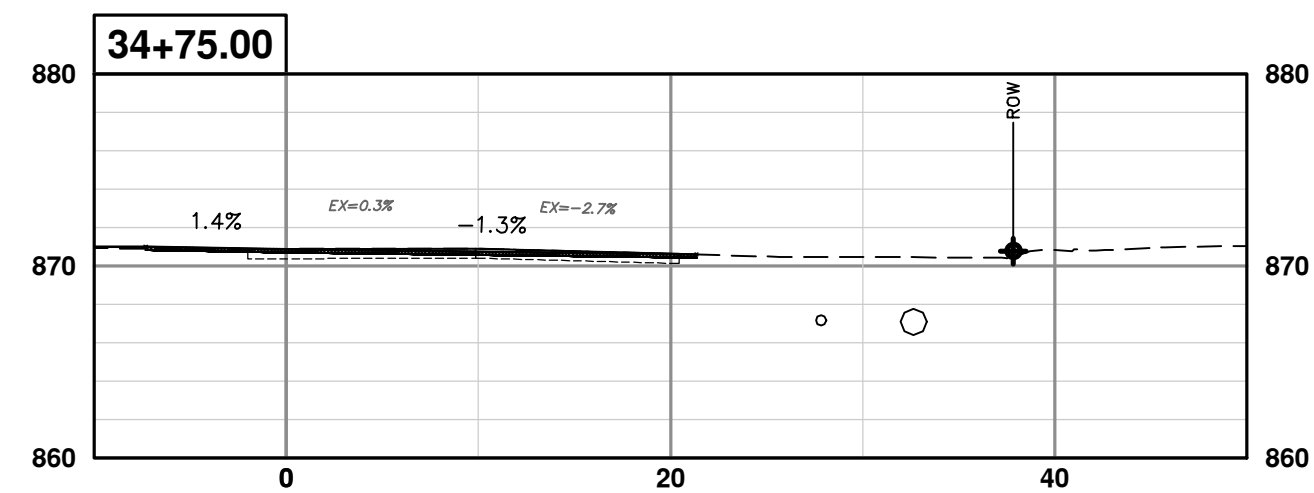
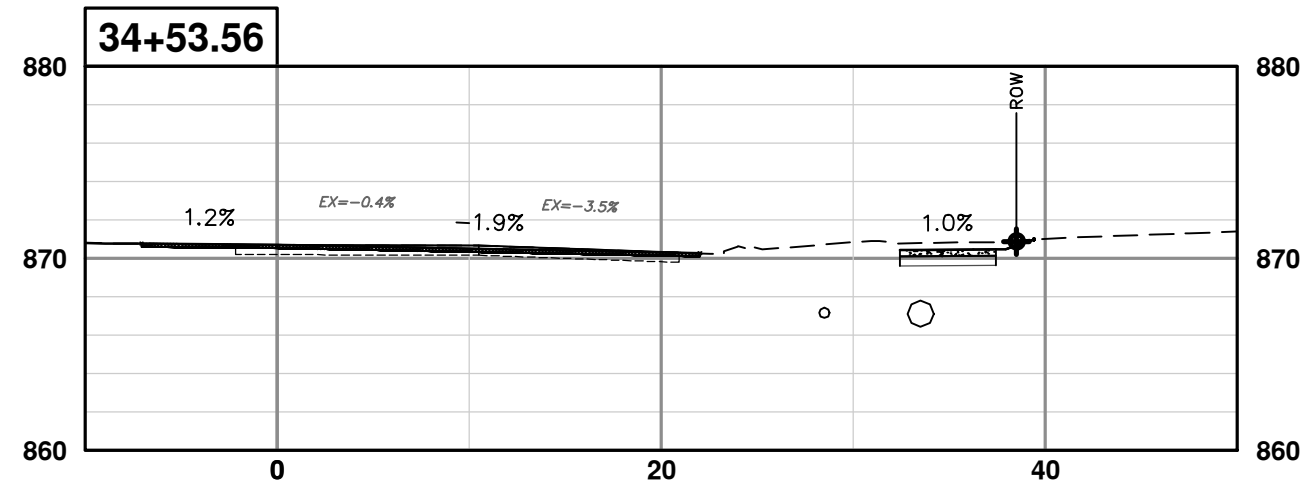
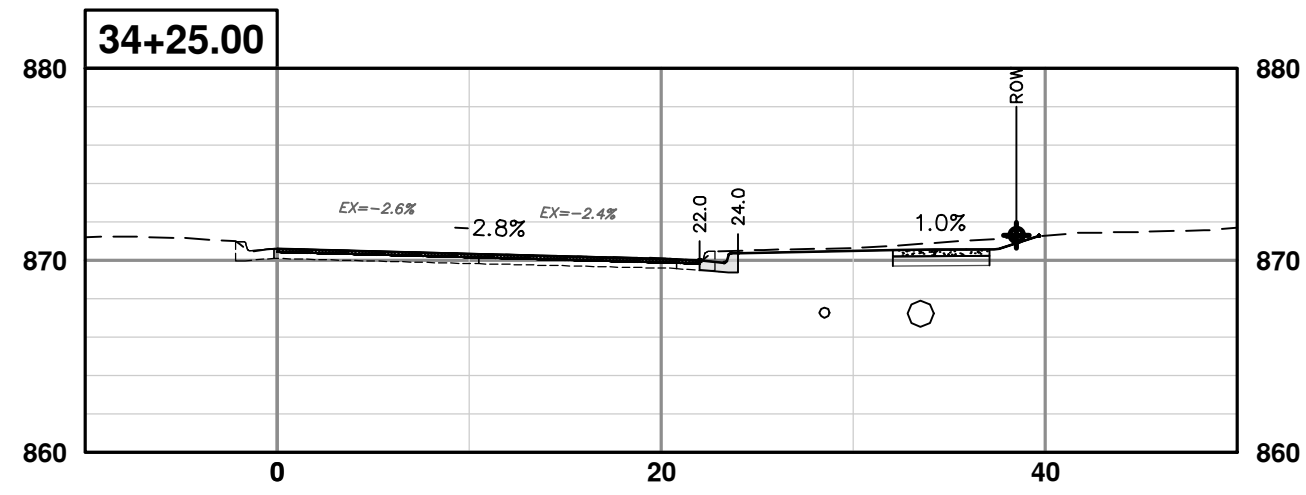
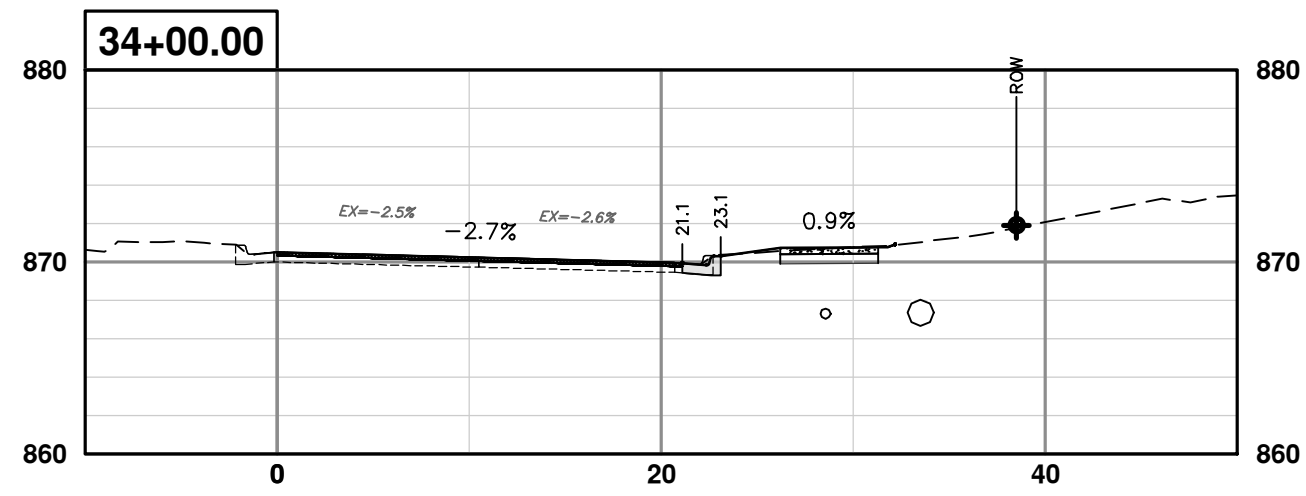
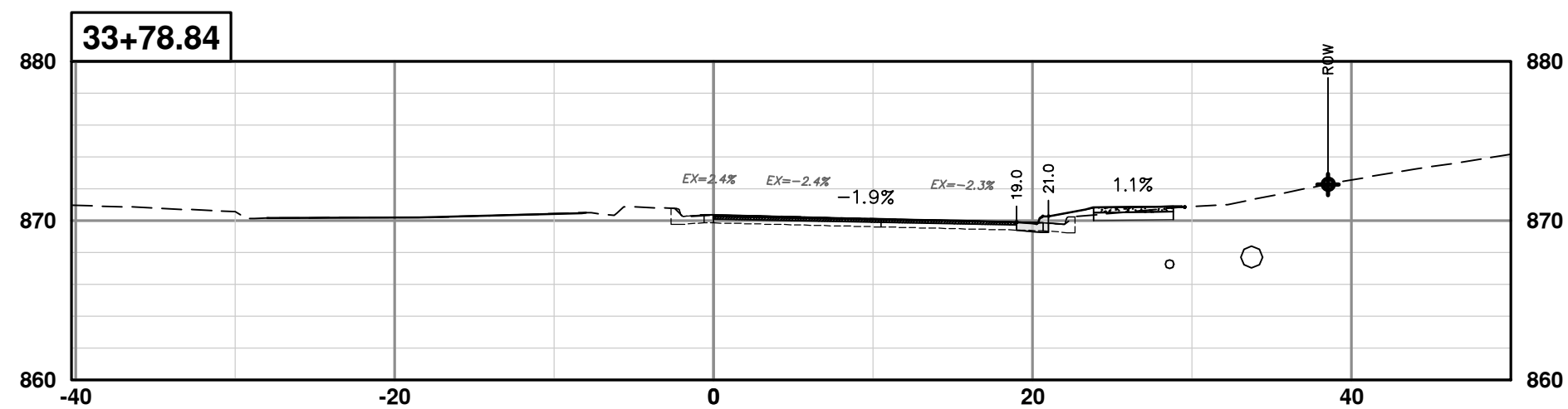
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CROSS SECTIONS  
NORTHBOUND 31+10 TO 33+50

SCALE PLAN: 1" = 10'  
PROFILE: 1" = 10'

DRAWING No. 2021023-64



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SHEET No.  
65 OF 74

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CROSS SECTIONS  
NORTHBOUND 33+75 TO 36+50

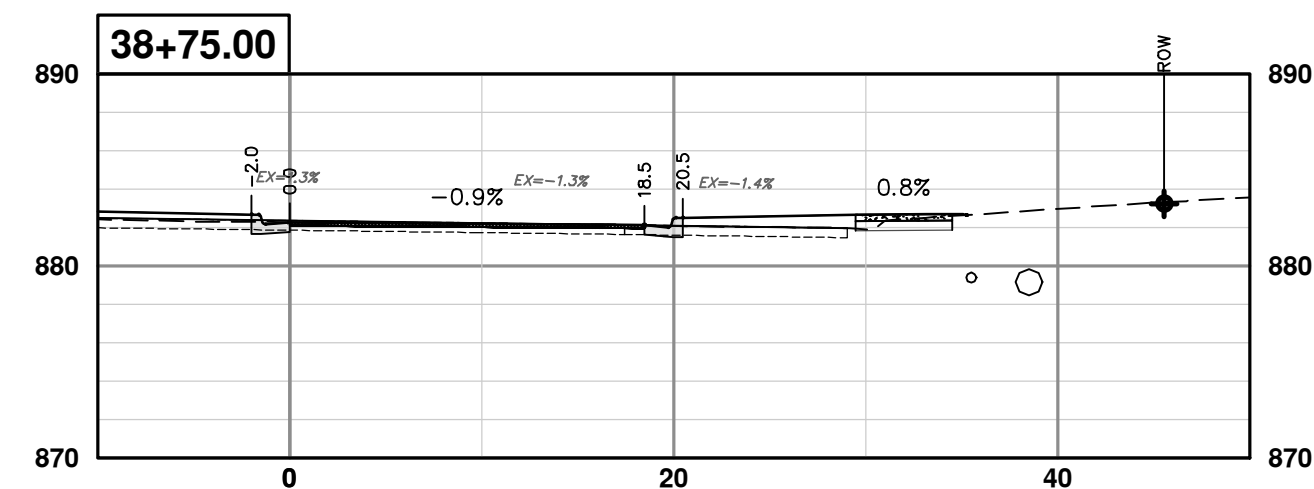
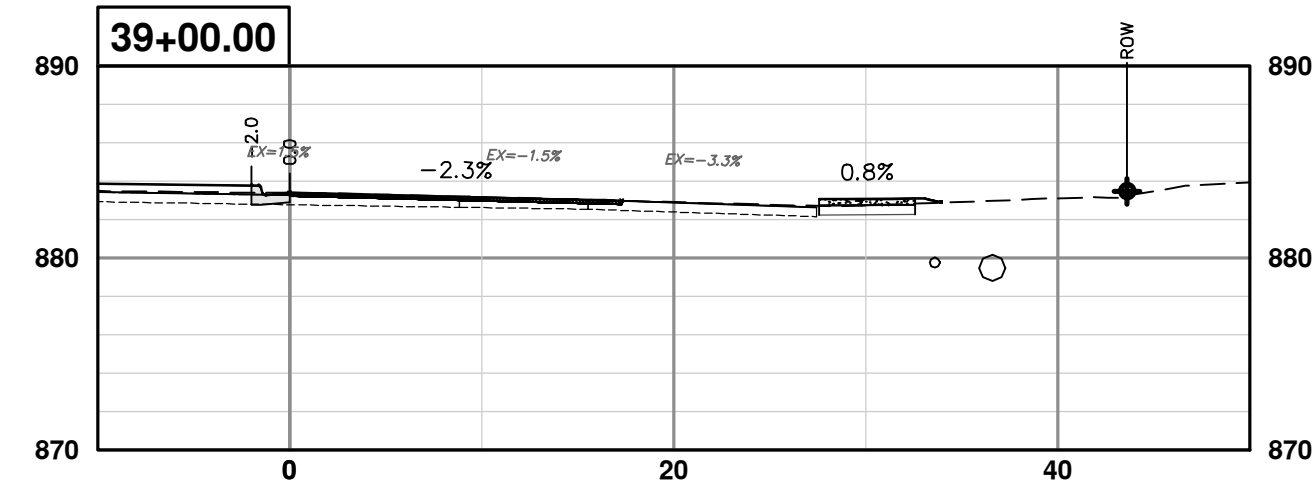
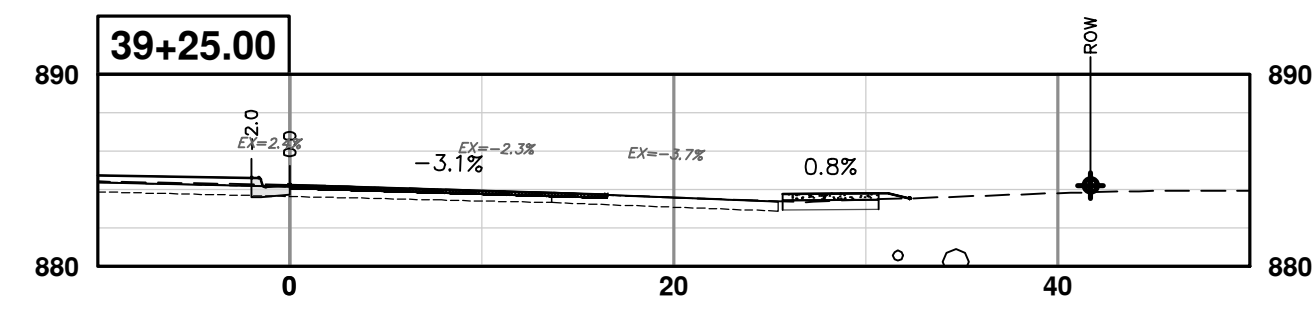
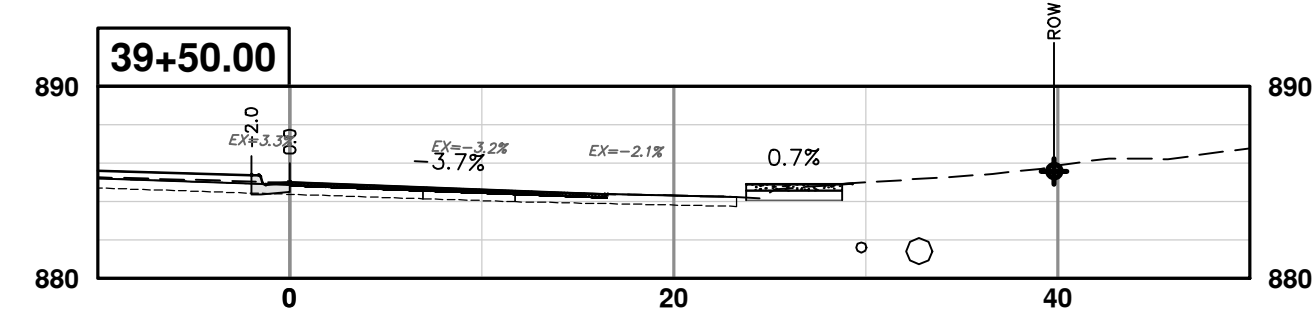
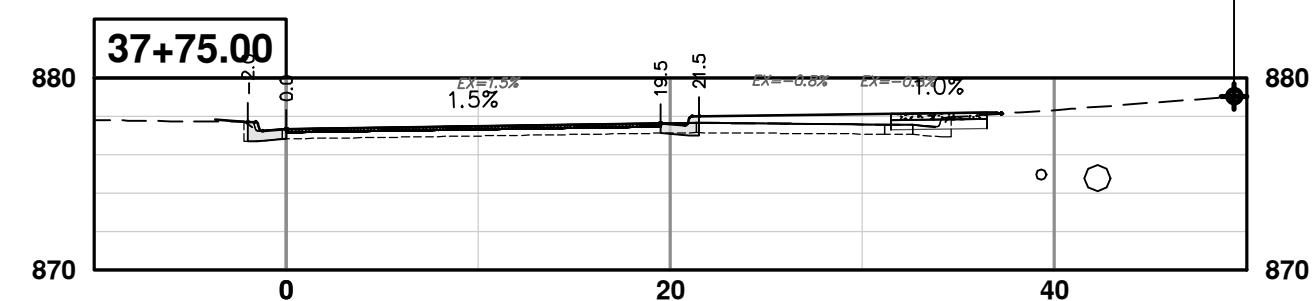
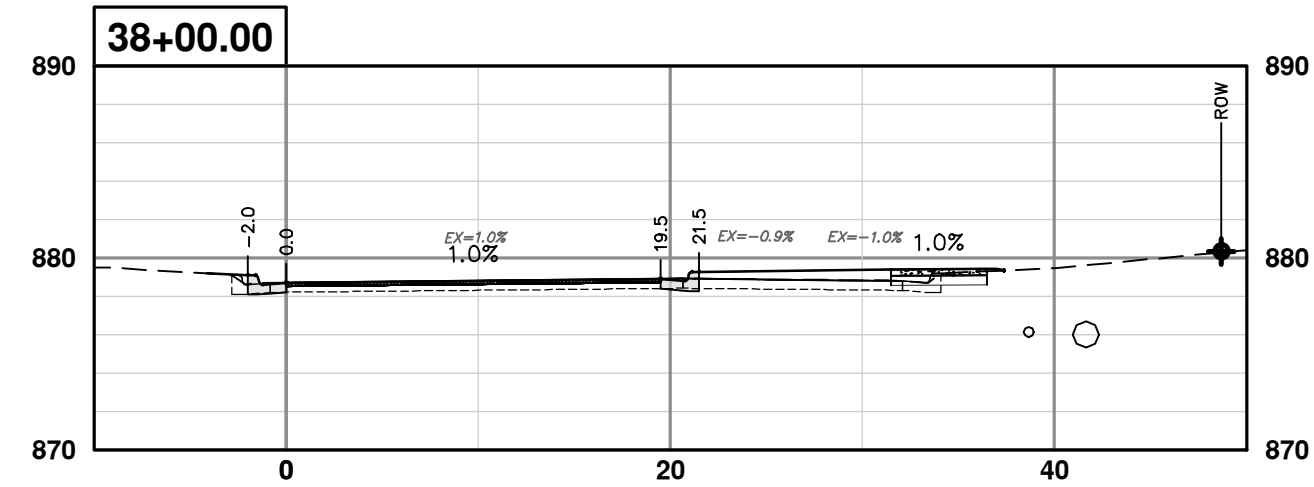
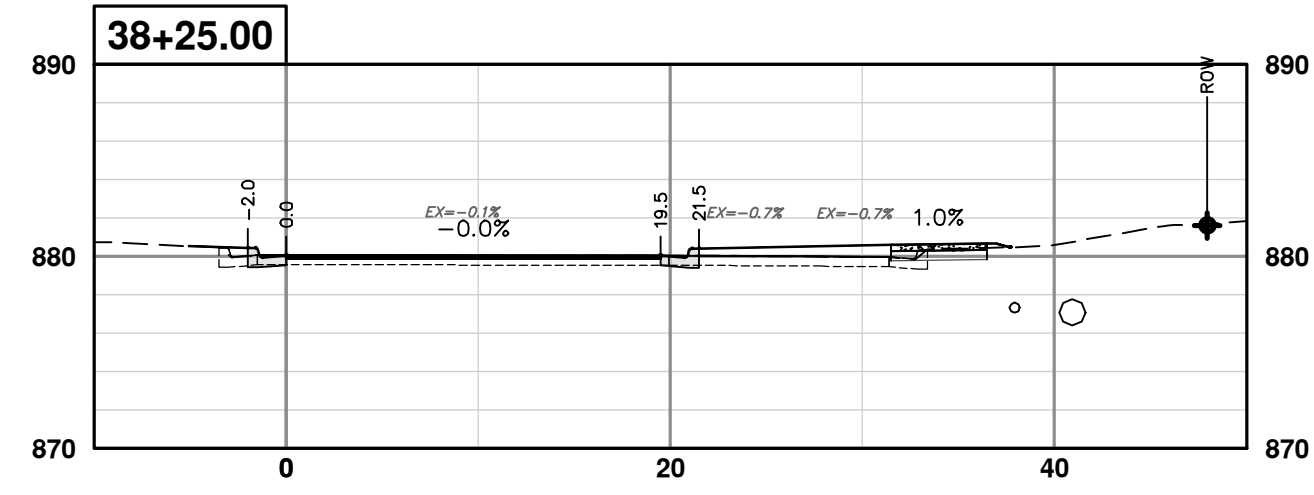
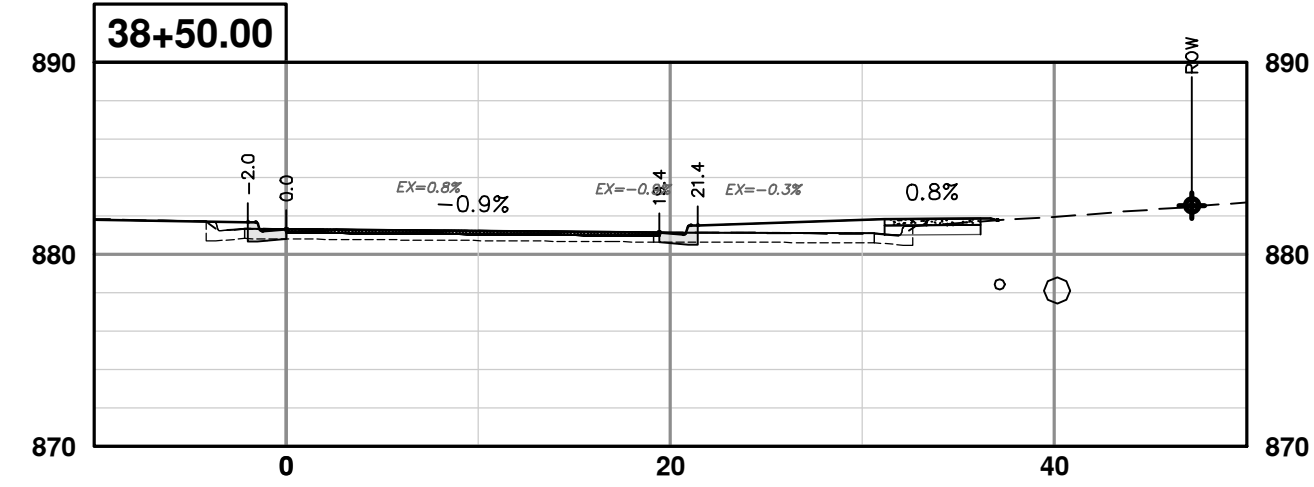
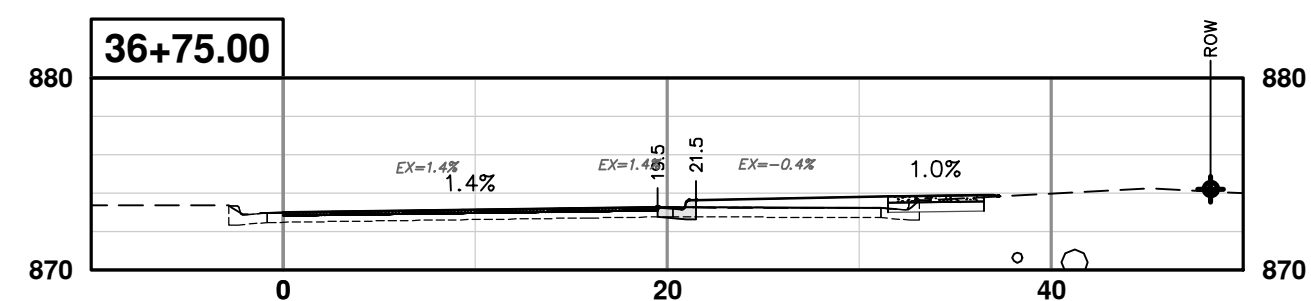
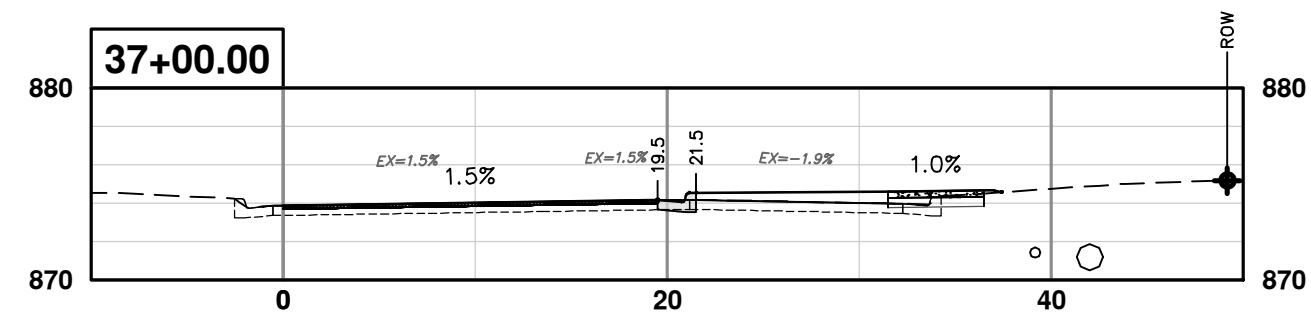
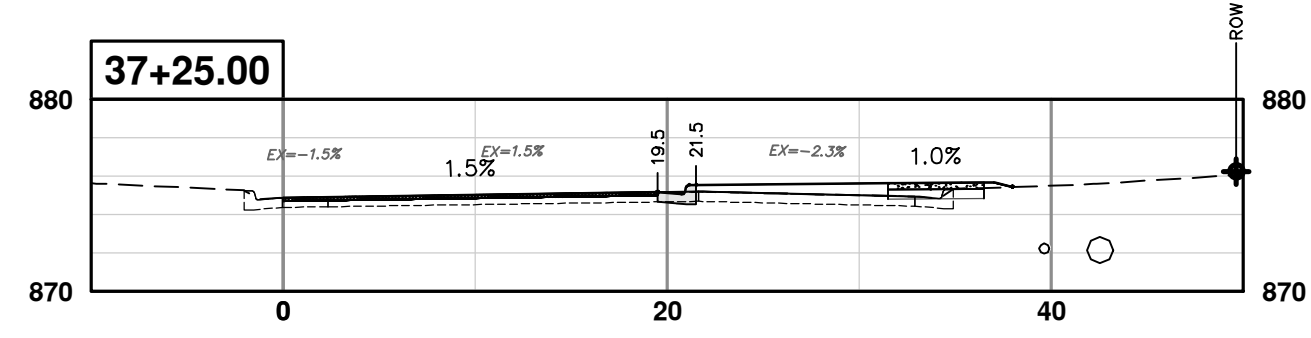
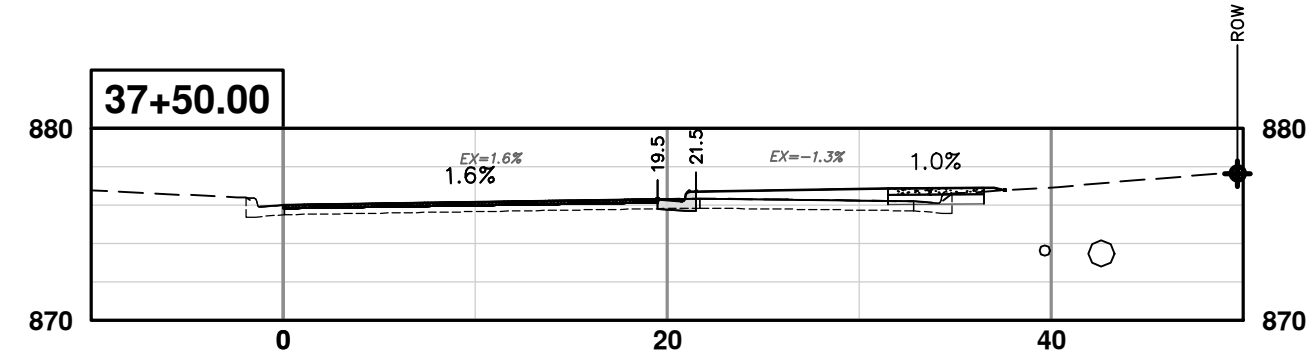


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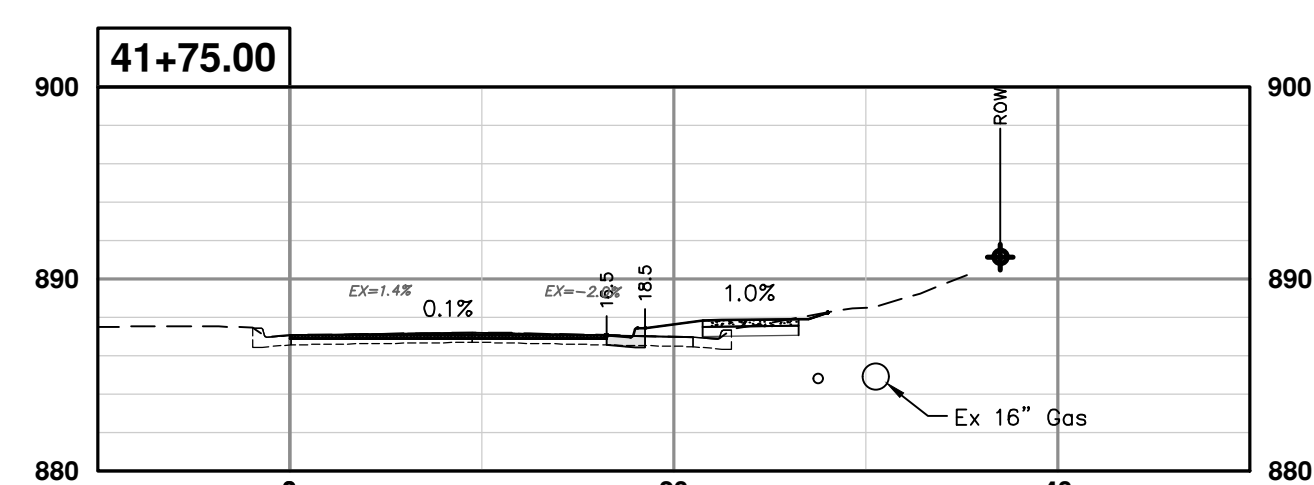
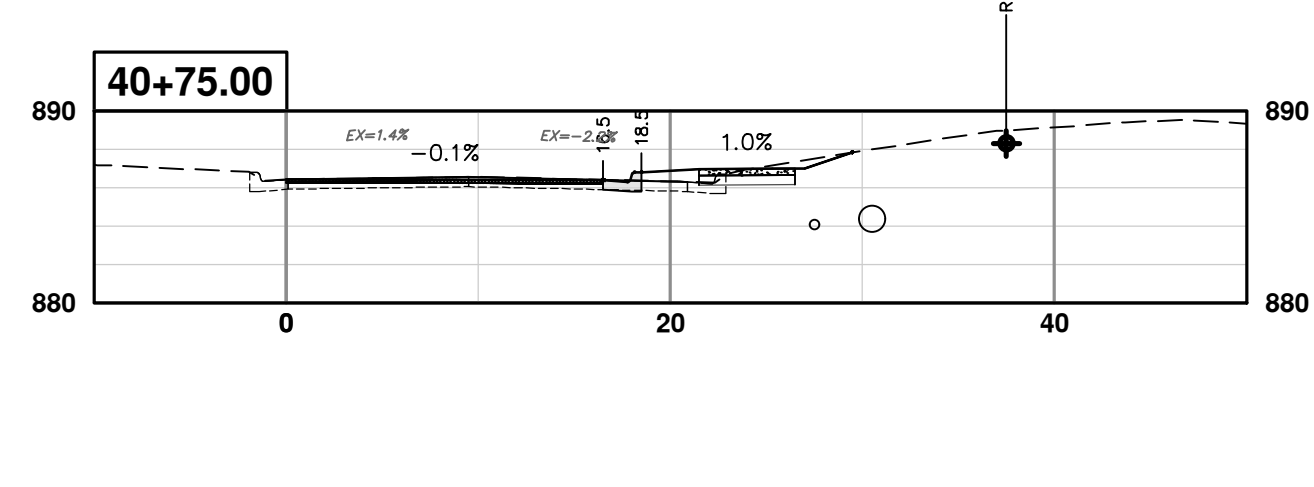
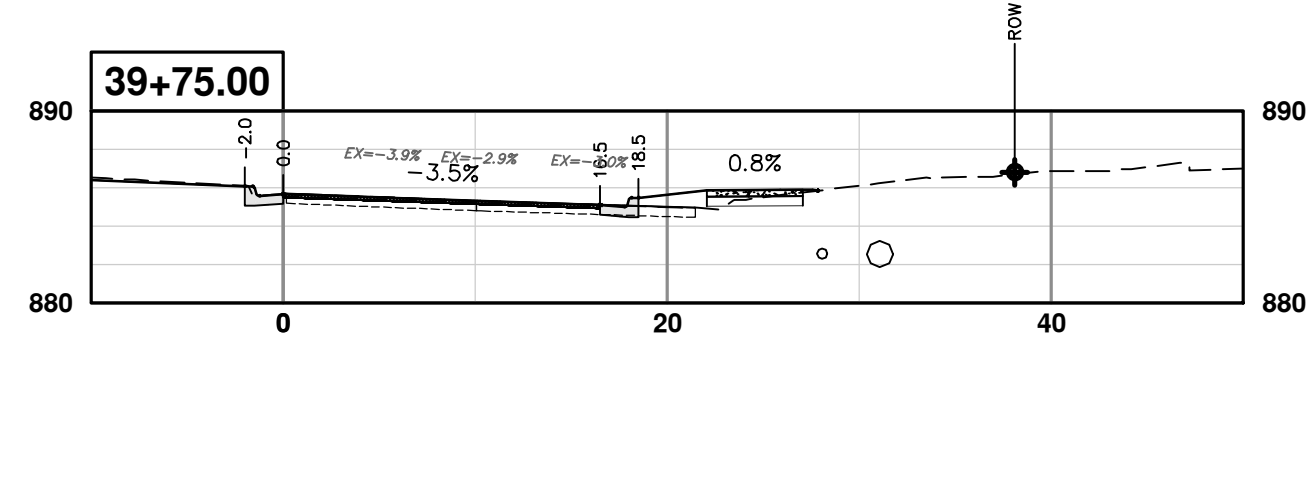
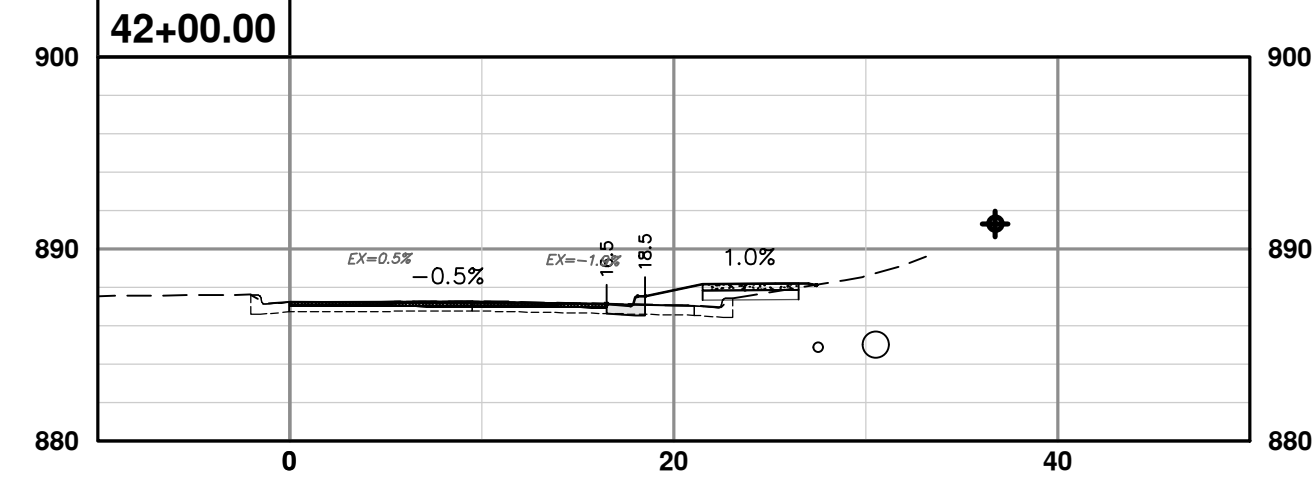
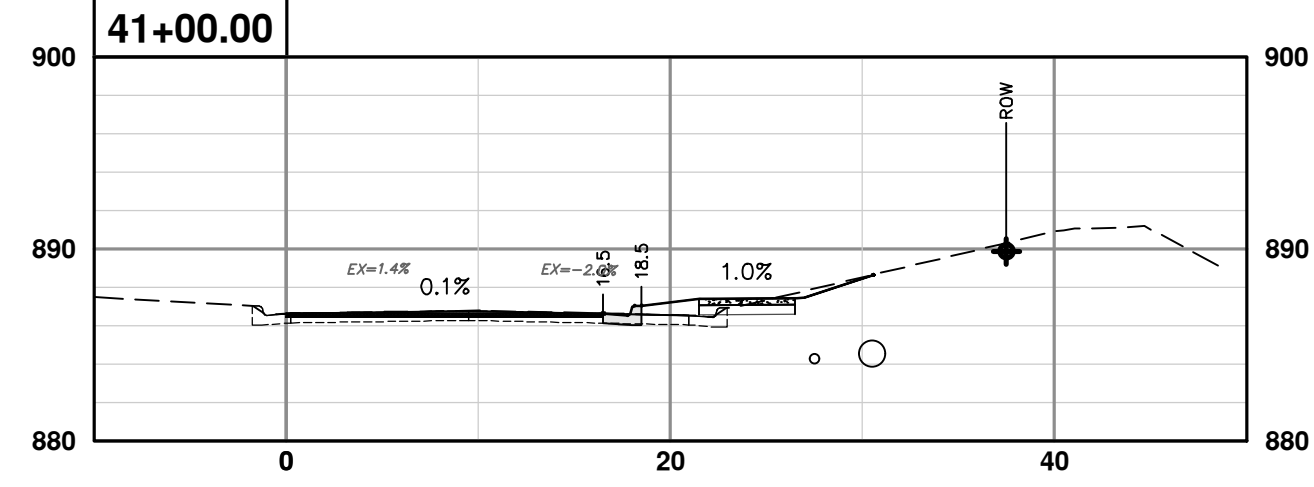
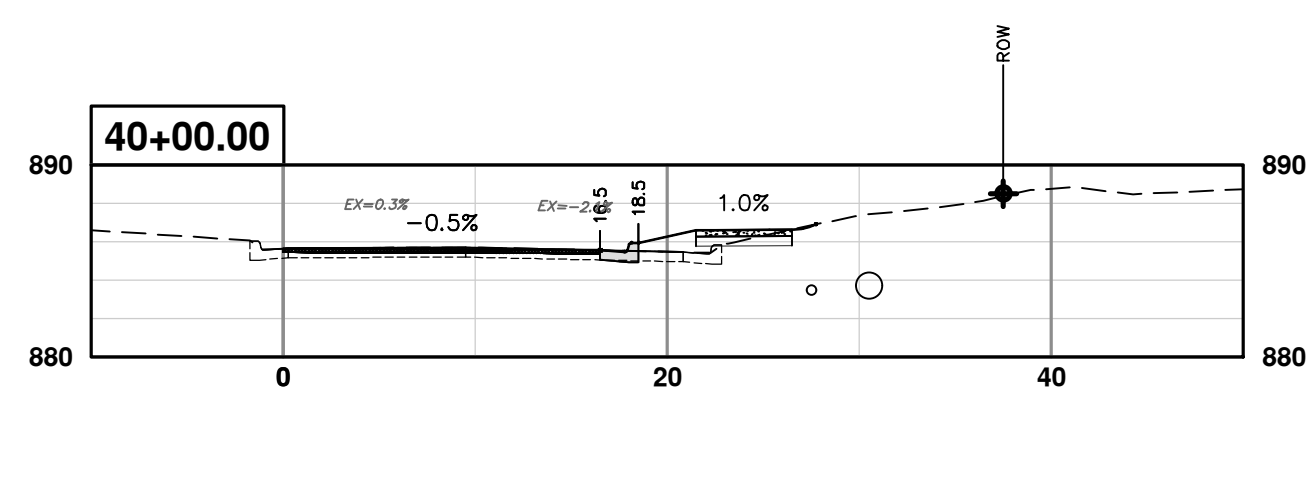
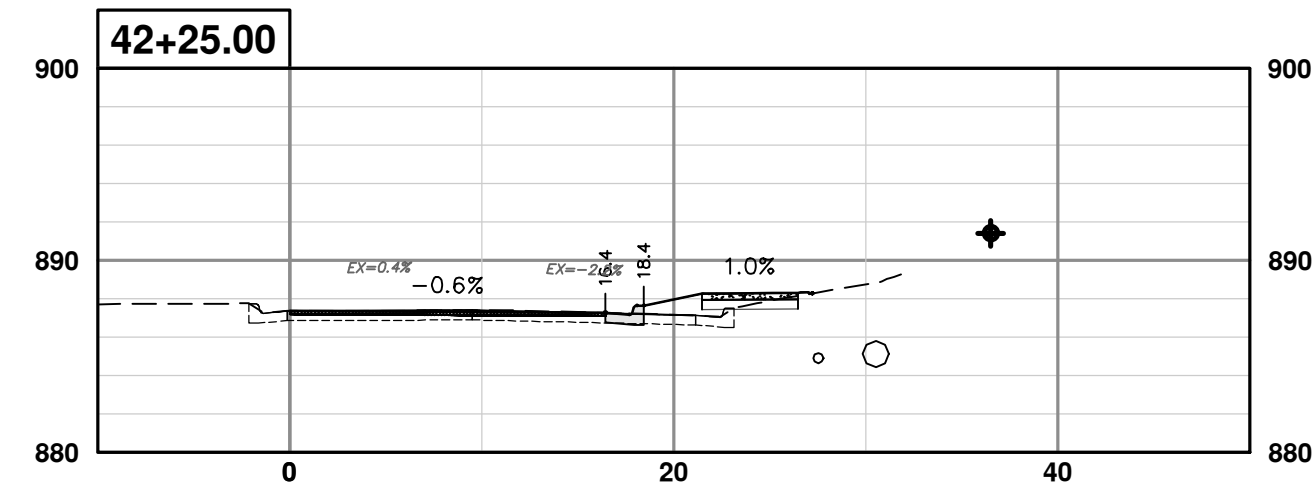
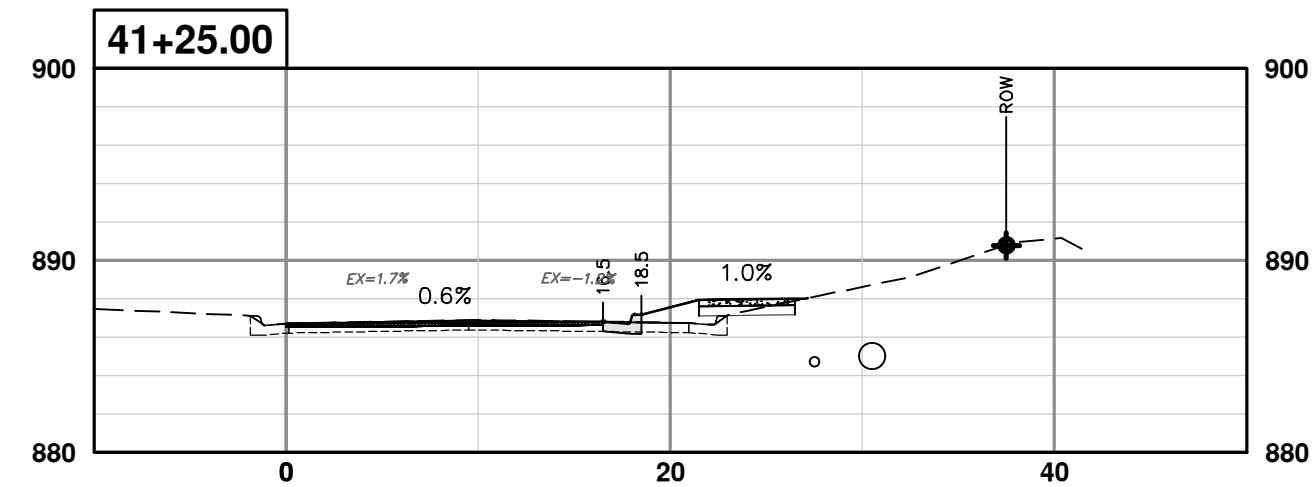
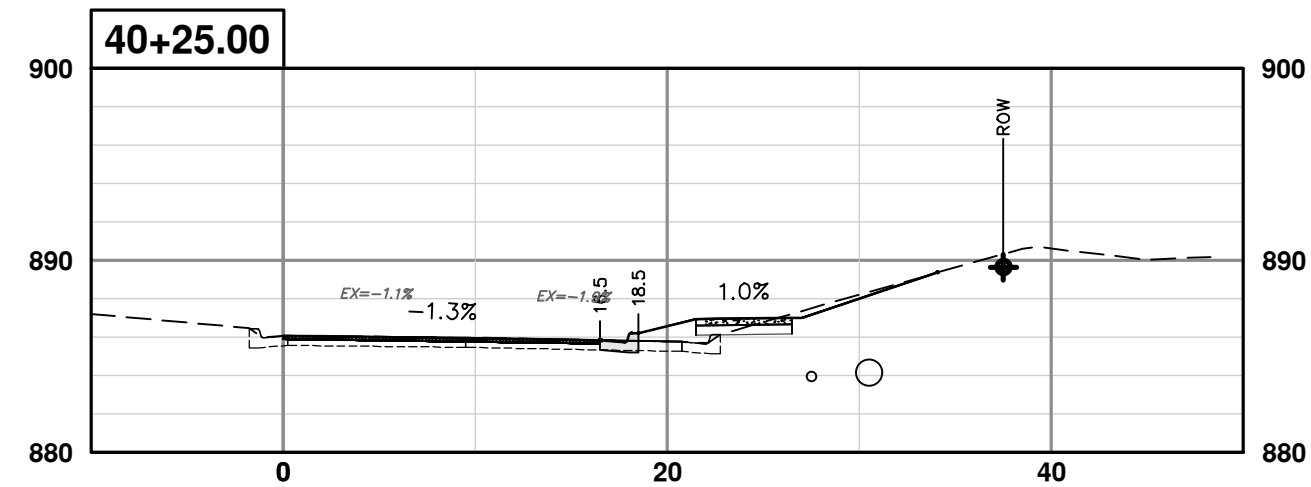
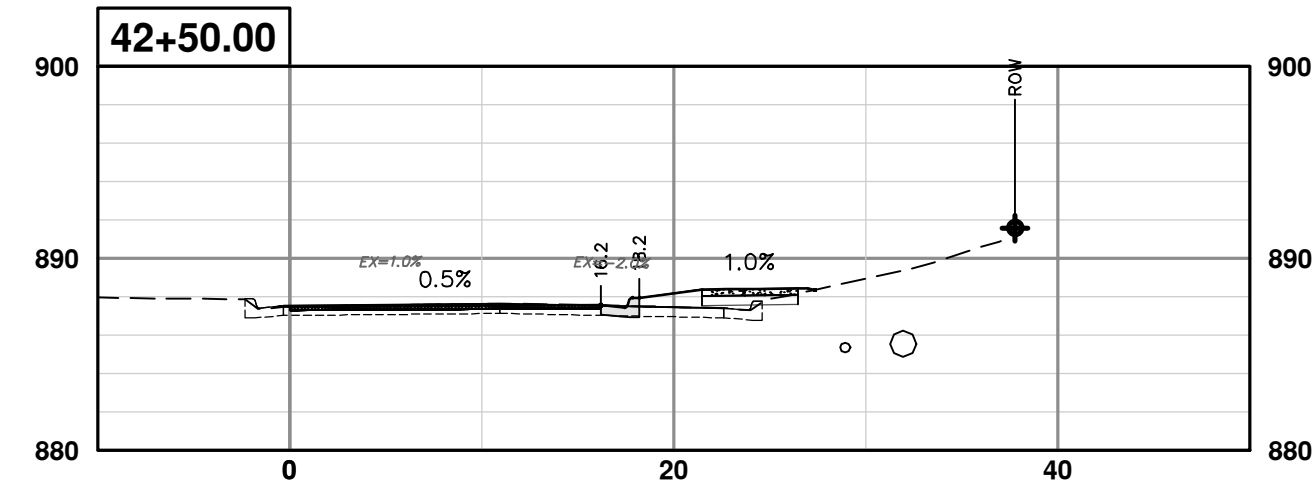
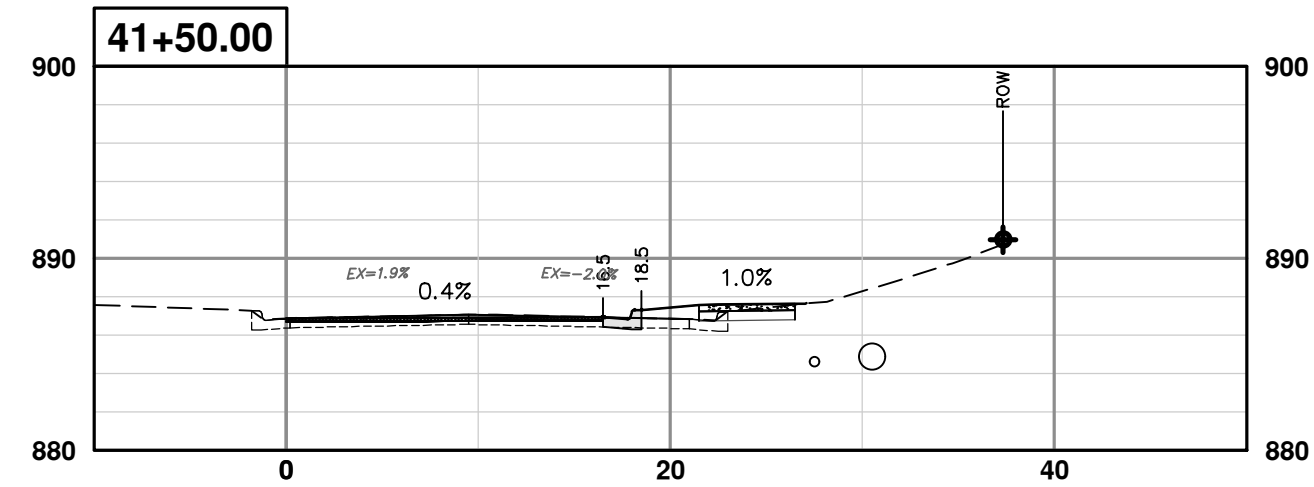
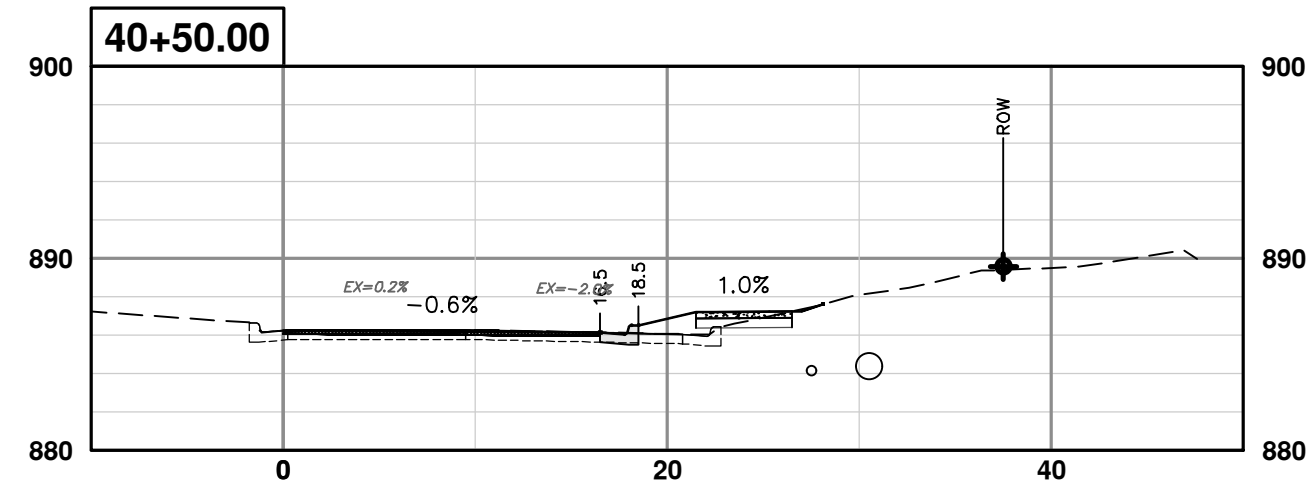
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EARHART ROAD IMPROVEMENTS (GEDDES - US23)  
CROSS SECTIONS  
NORTHBOUND 36+75 TO 39+50

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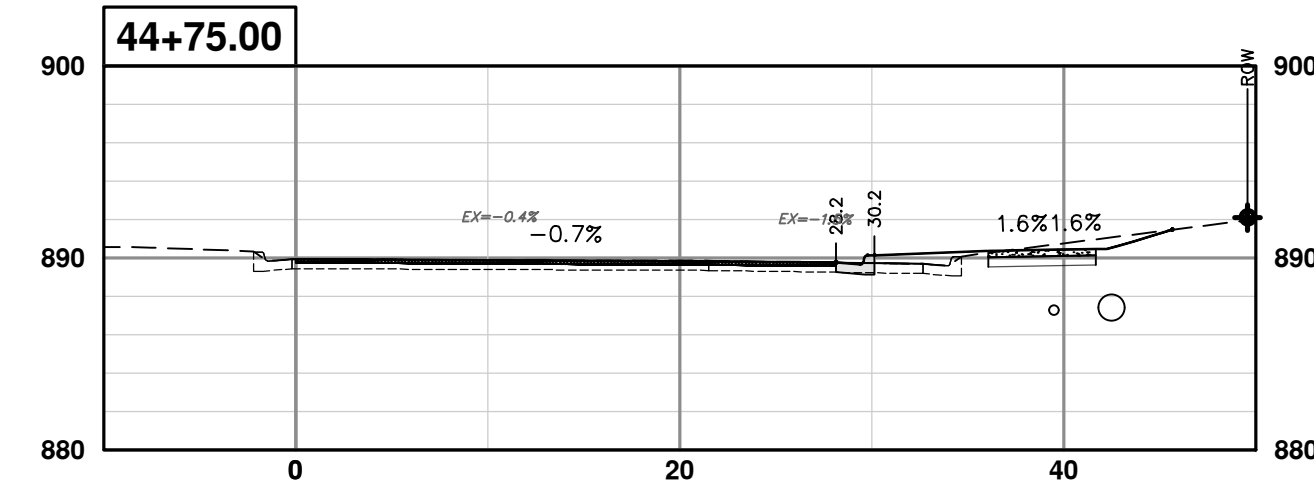
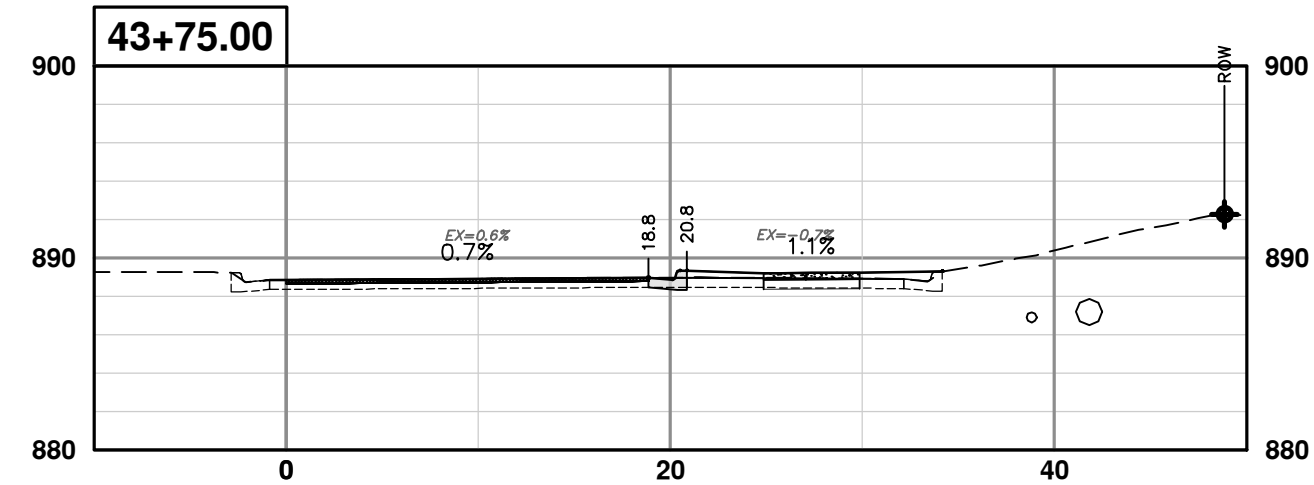
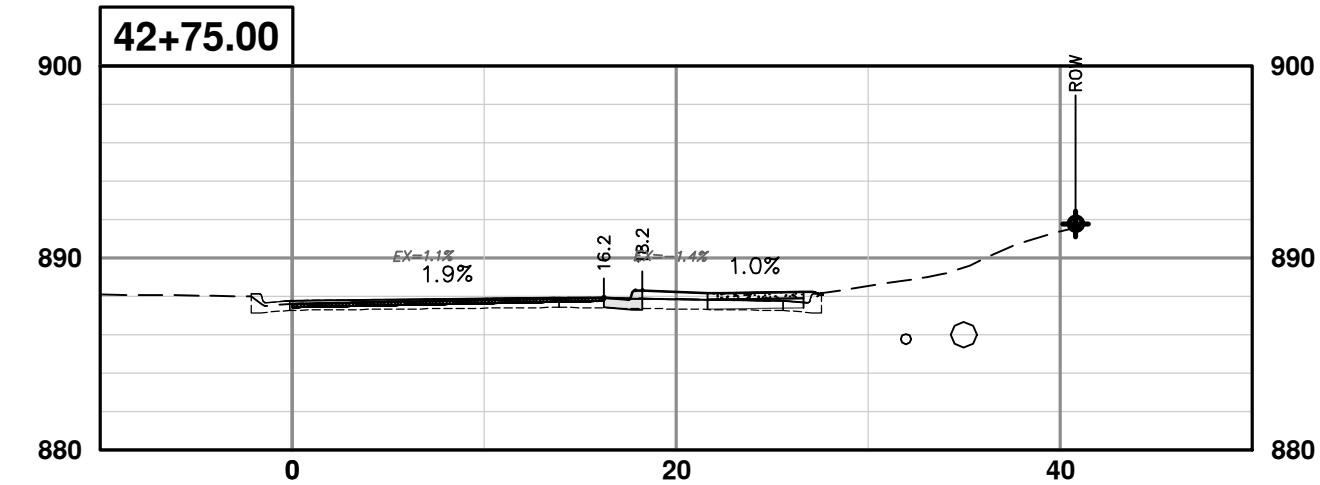
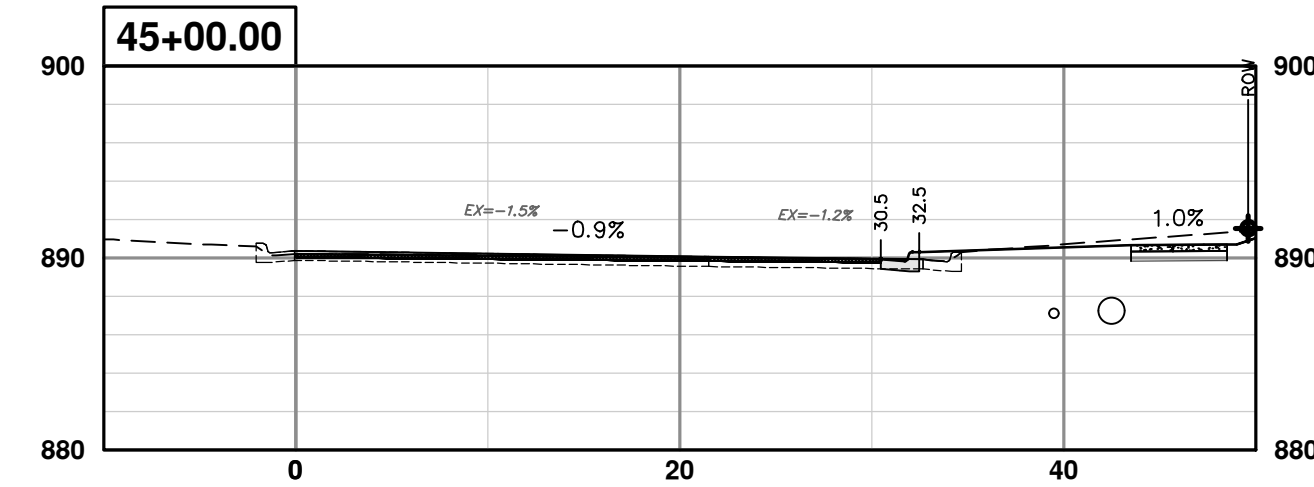
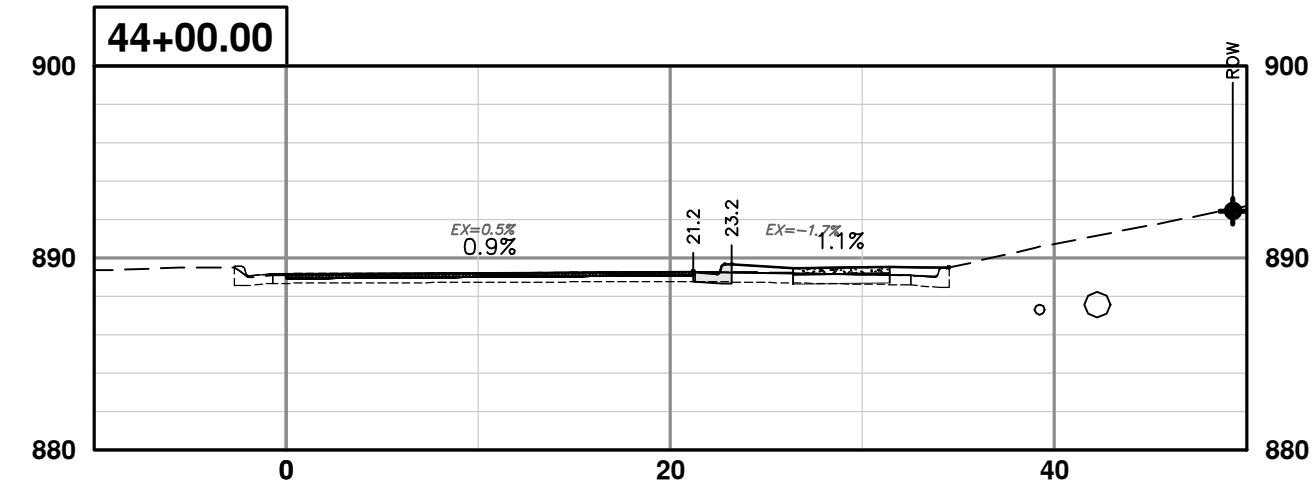
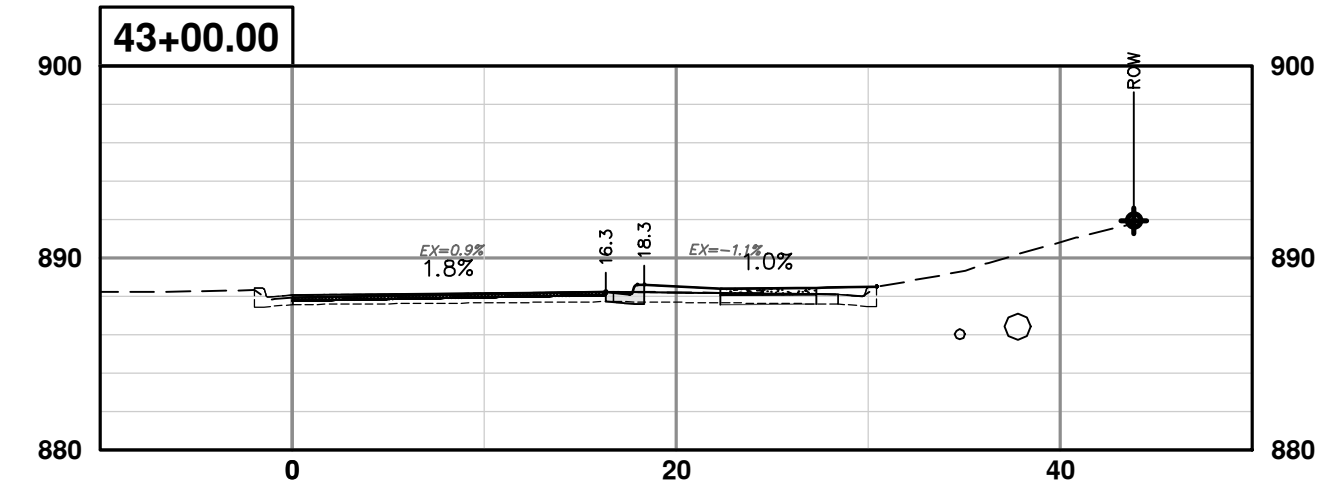
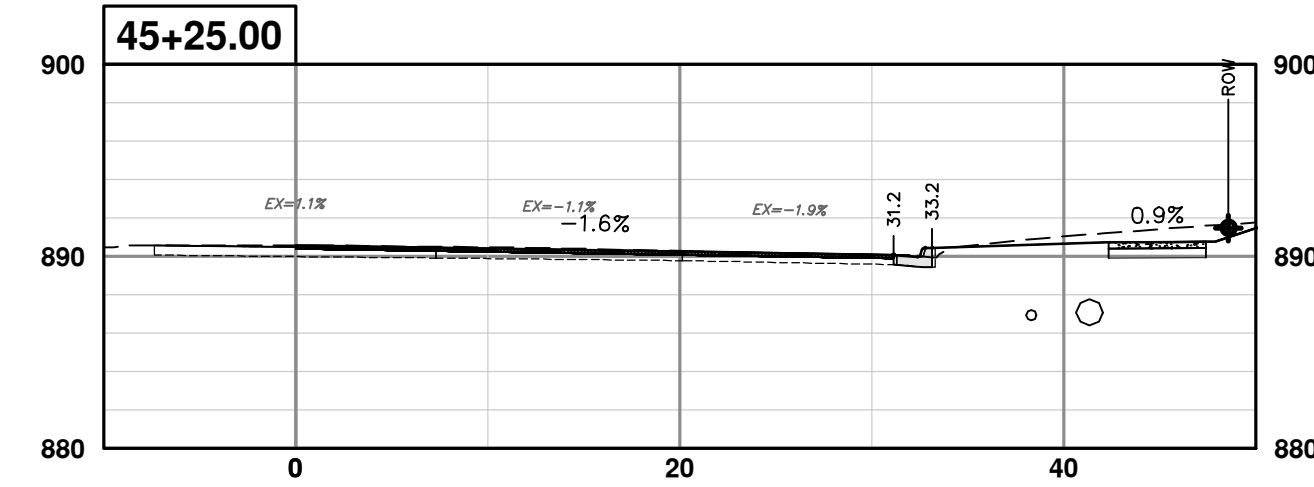
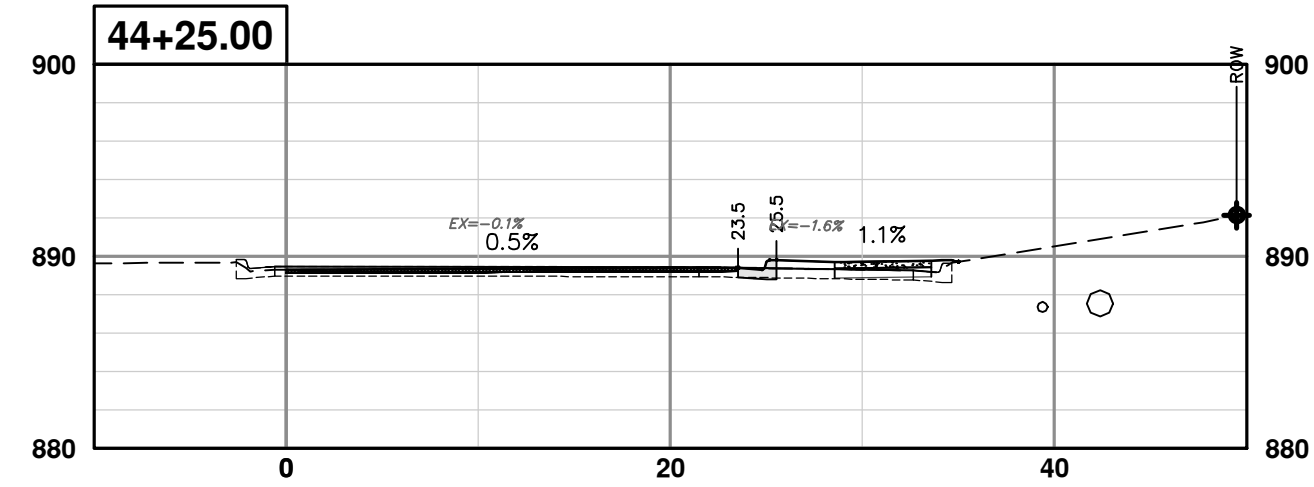
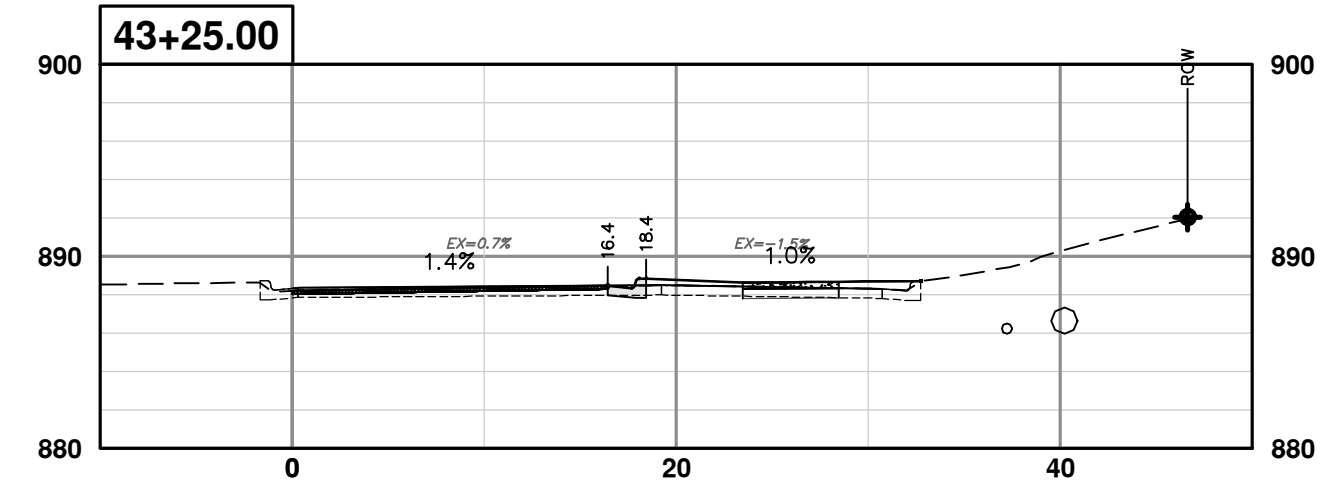
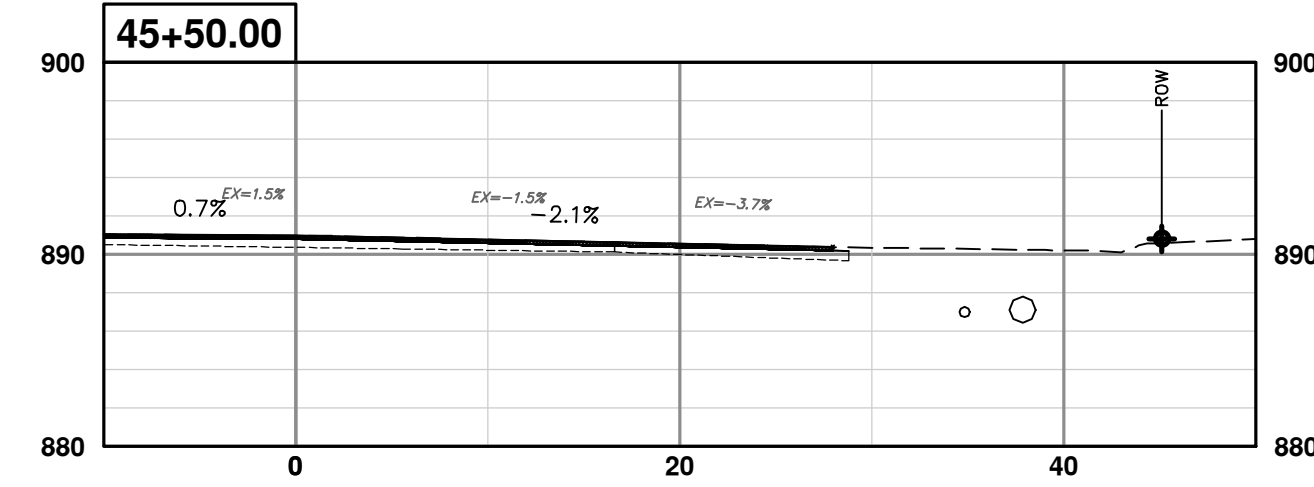
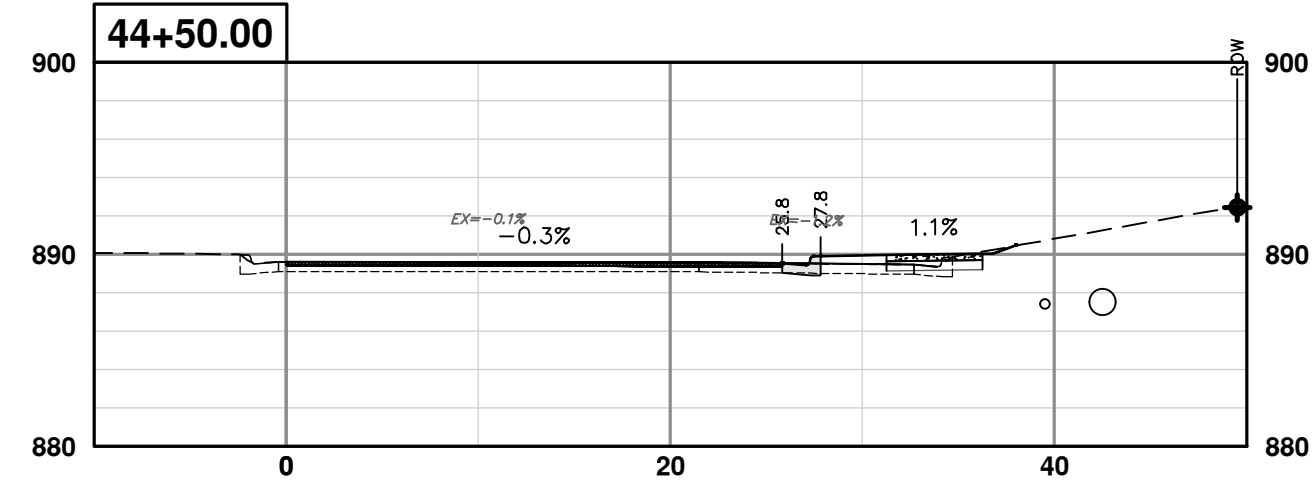
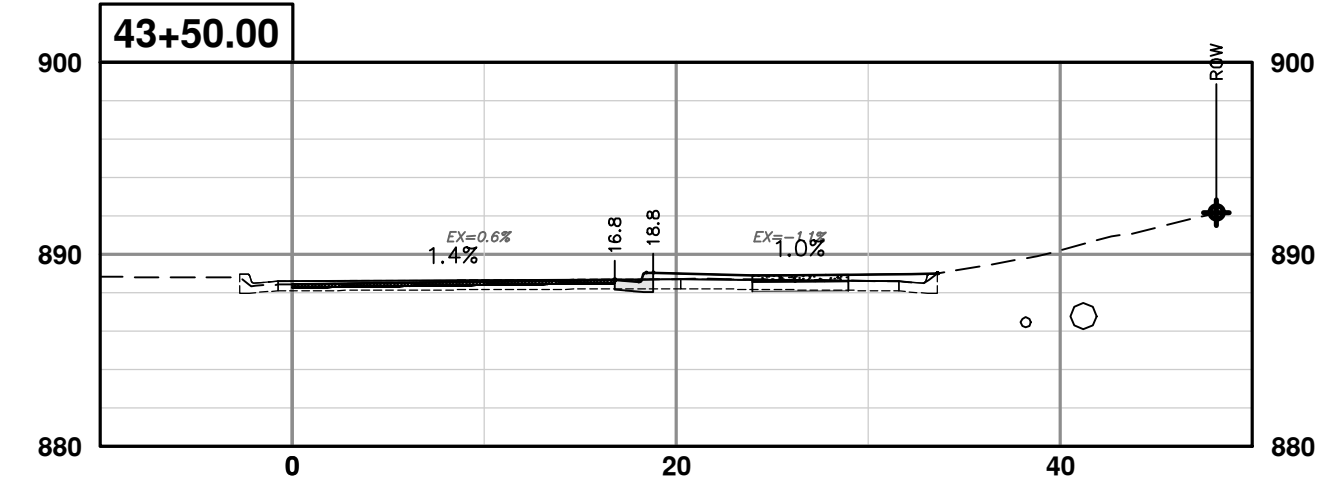
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CROSS SECTIONS  
NORTHBOUND 39+75 TO 42+50

SCALE PLAN: 1" = 10'  
PROFILE: 1" = 10'  
DRAWING No. 2021023-67

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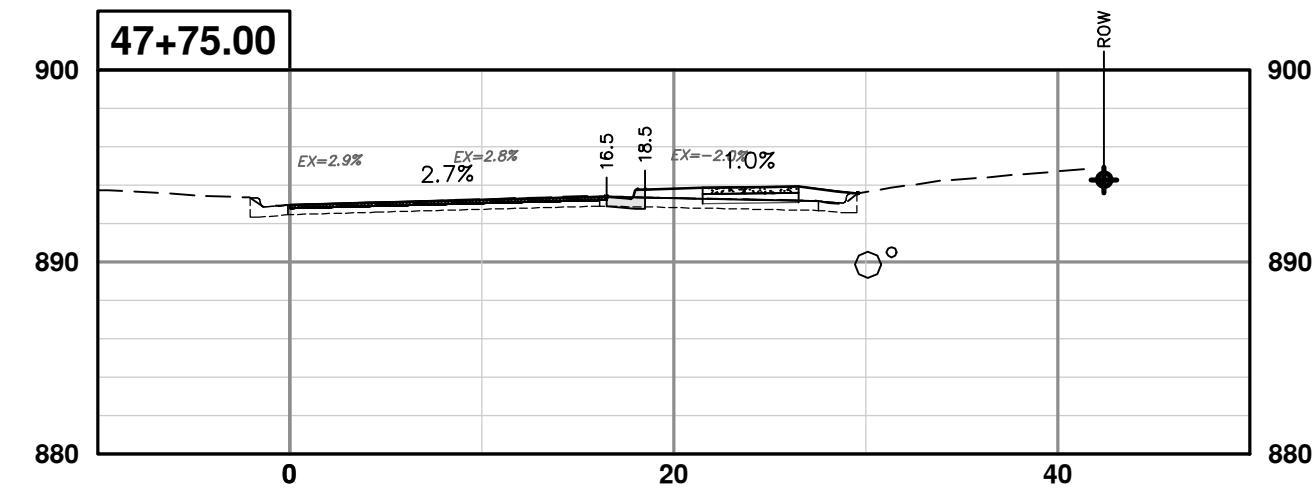
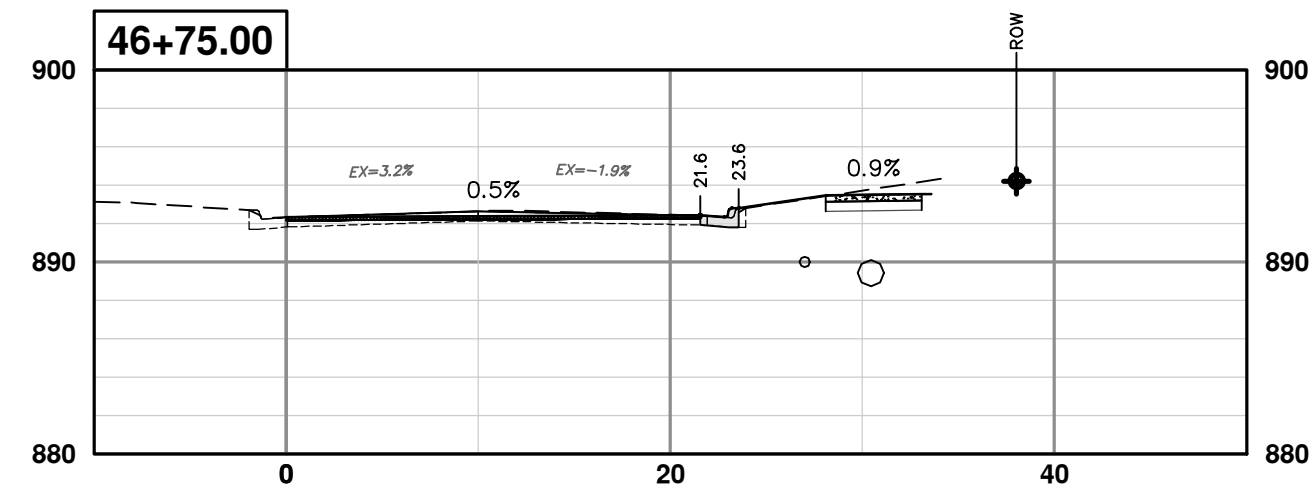
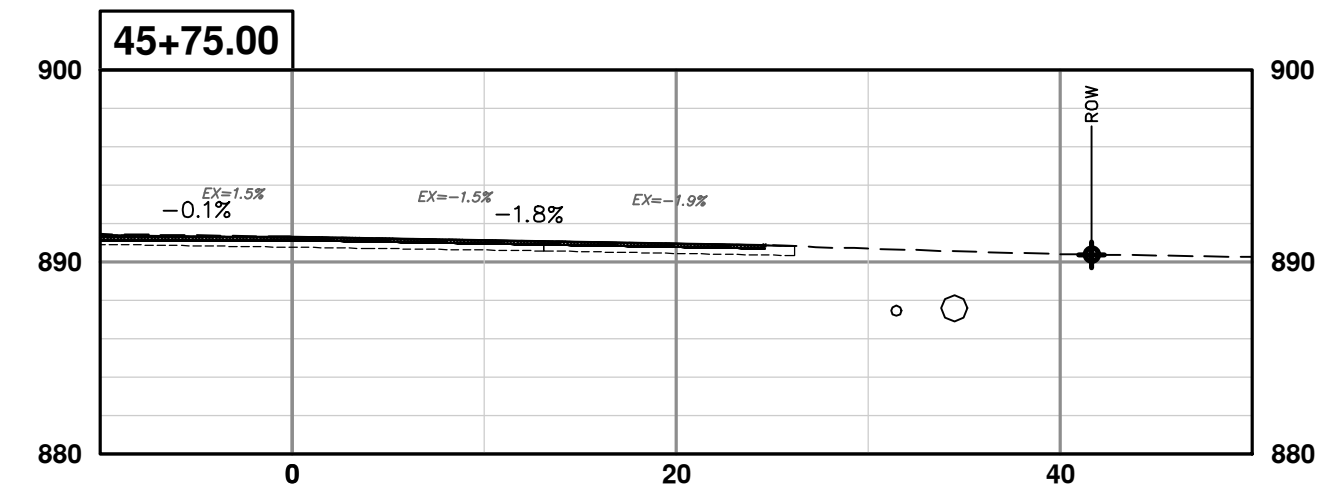
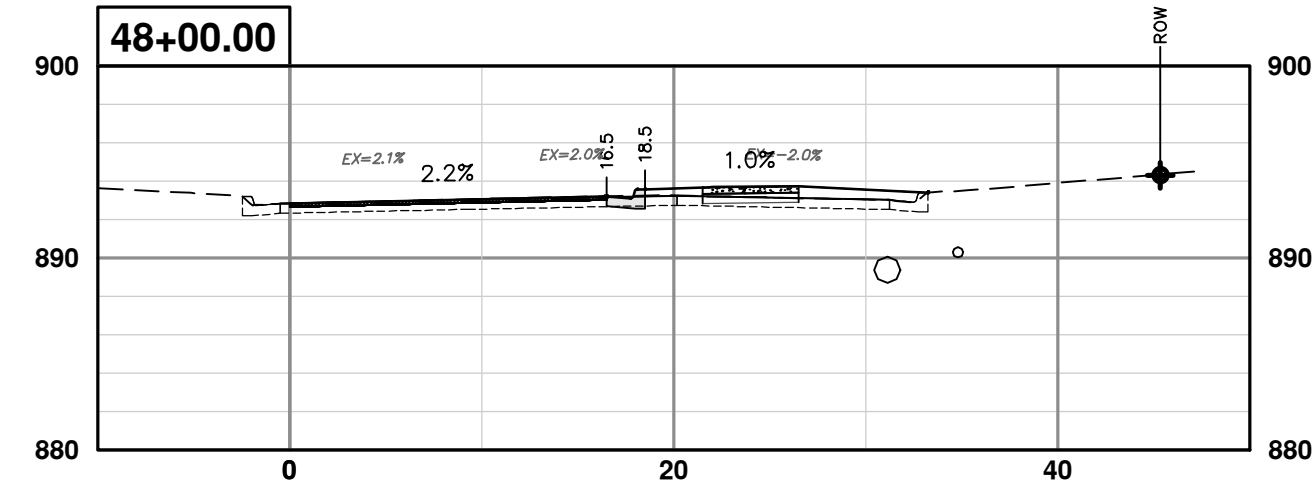
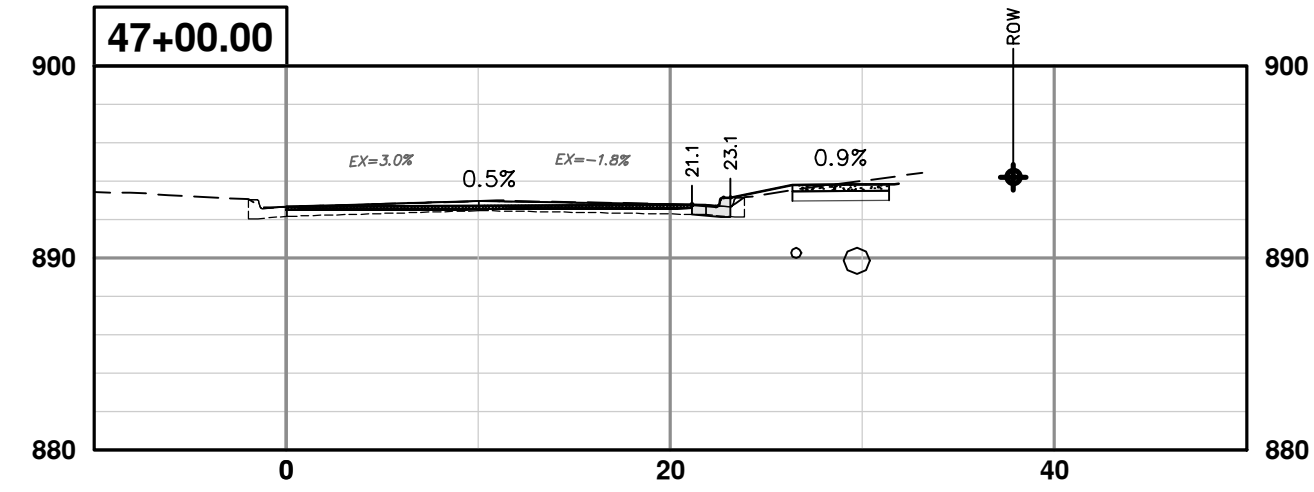
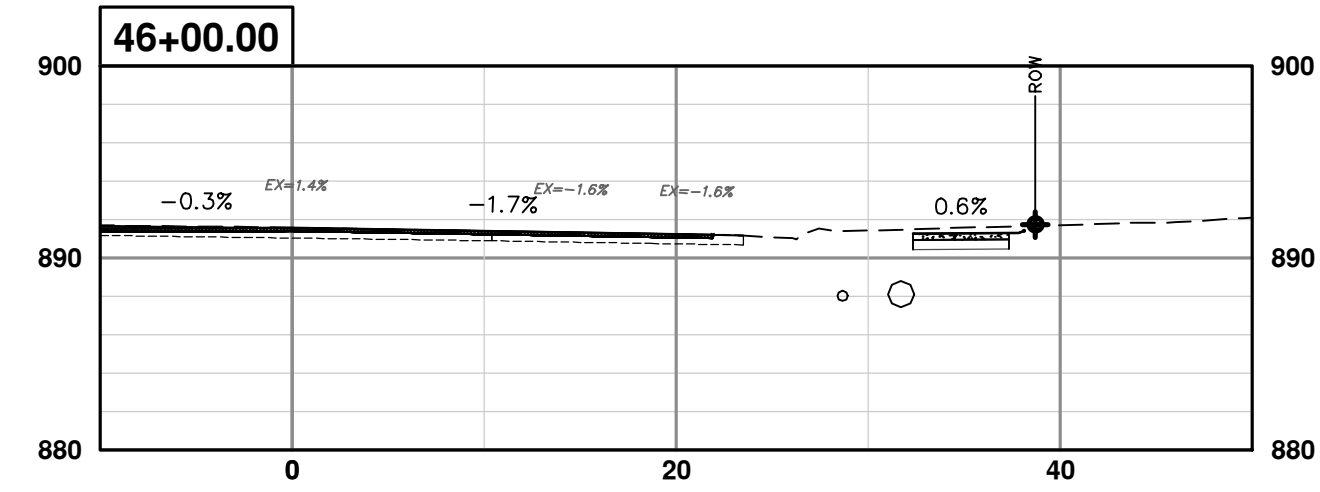
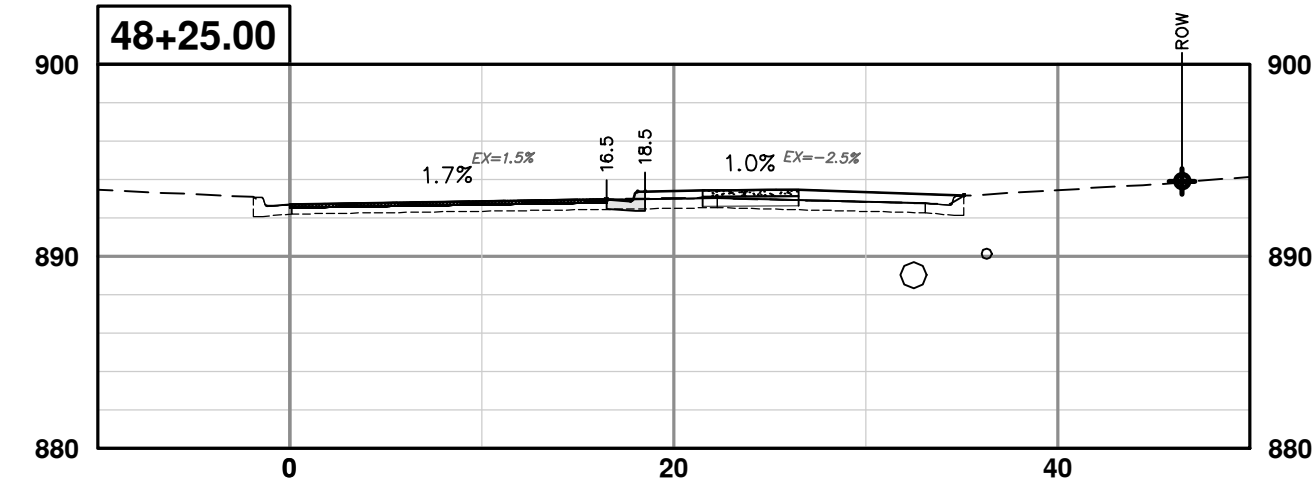
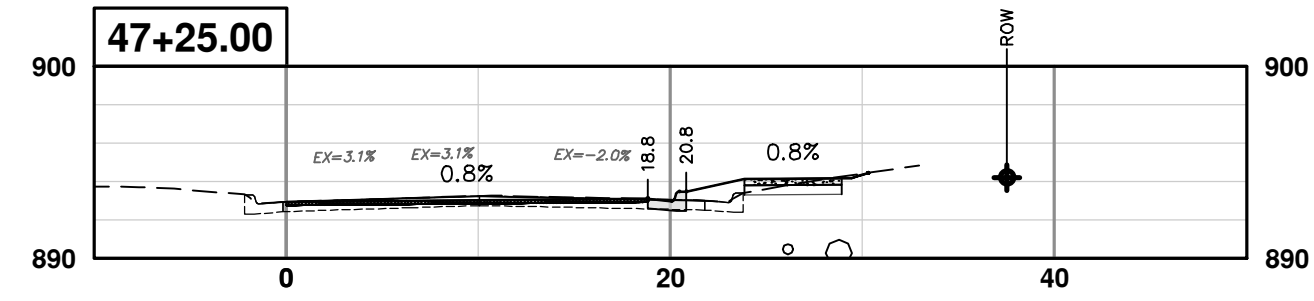
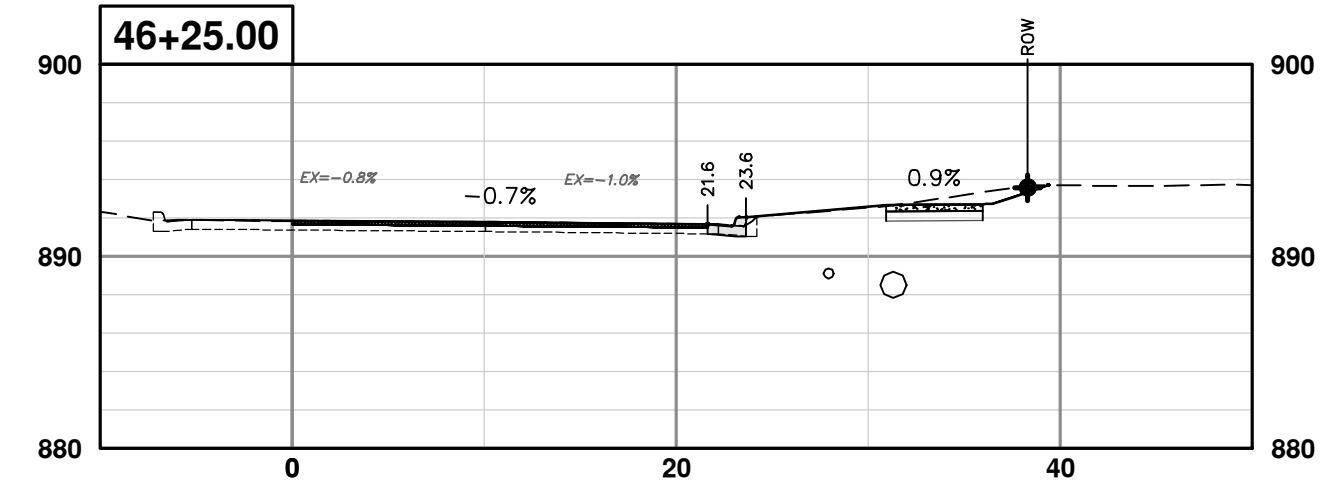
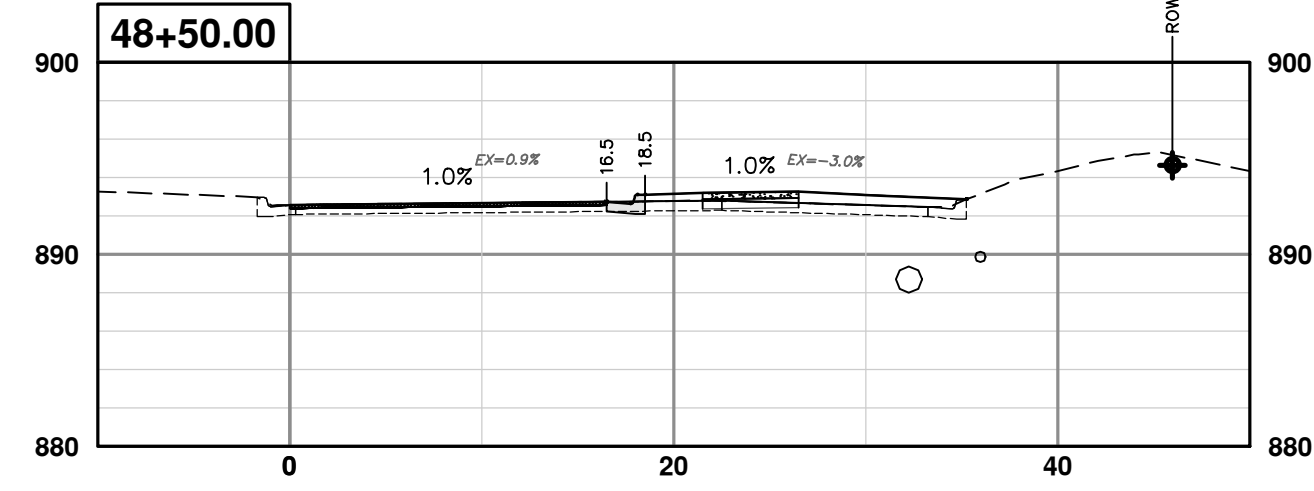
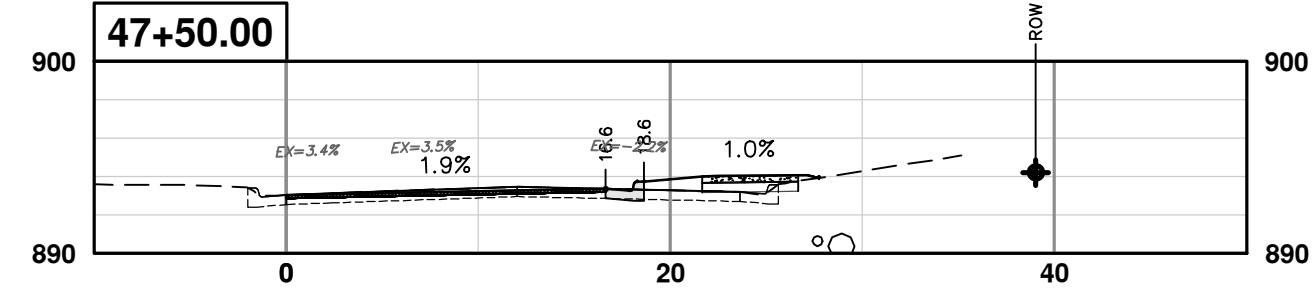
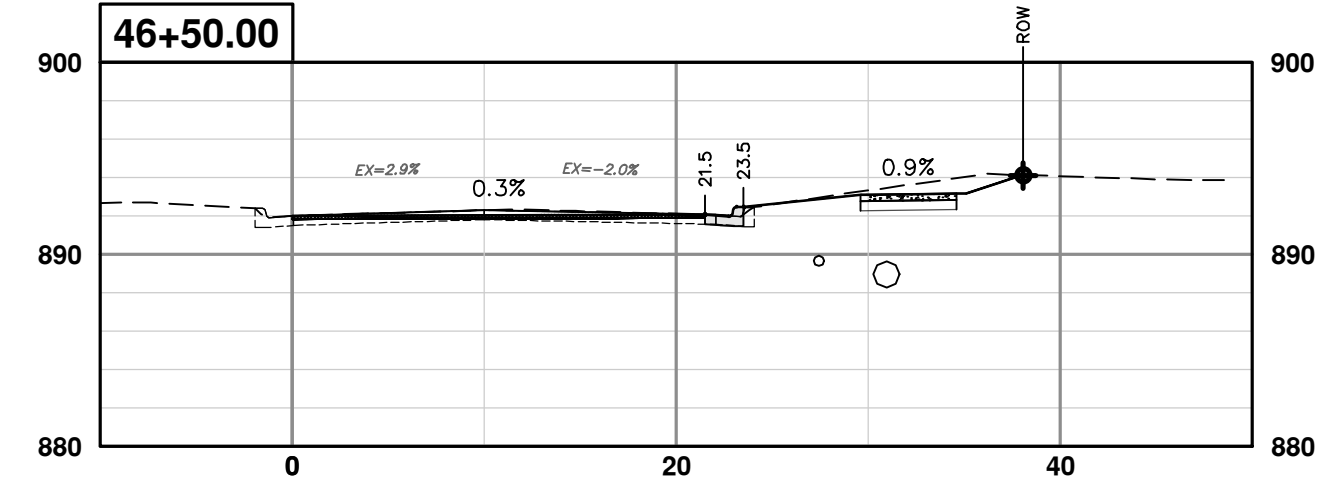
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**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
CROSS SECTIONS  
NORTHBOUND 42+75 TO 45+50

SHEET No. **68 OF 74**  
SCALE PLAN: 1" = 10'  
PROFILE: 1" = 10'  
DRAWING No. **2021023-68**

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00	OUT TO BID	4-27-23			

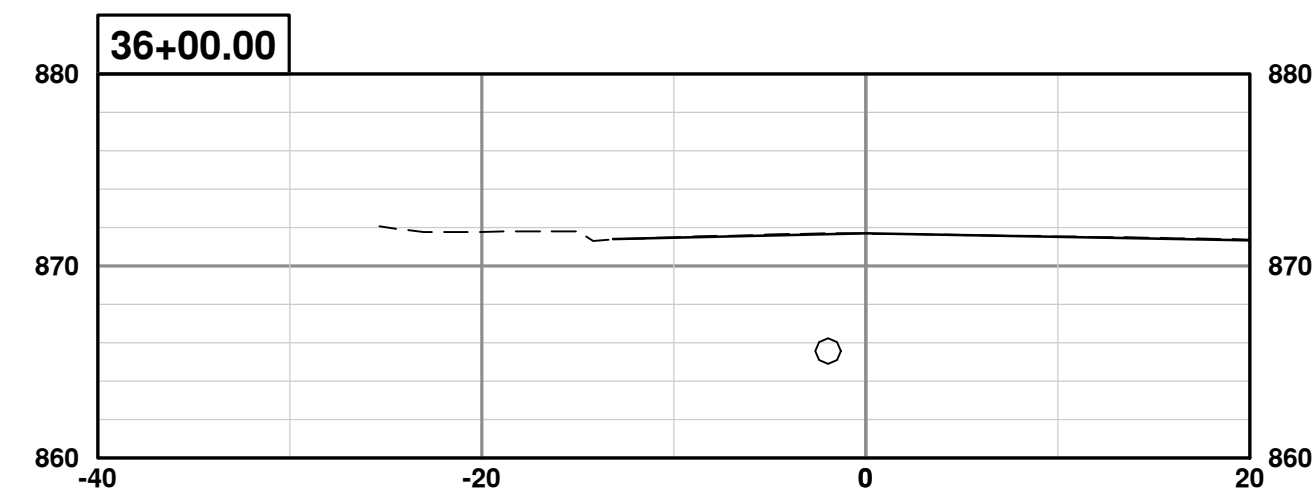
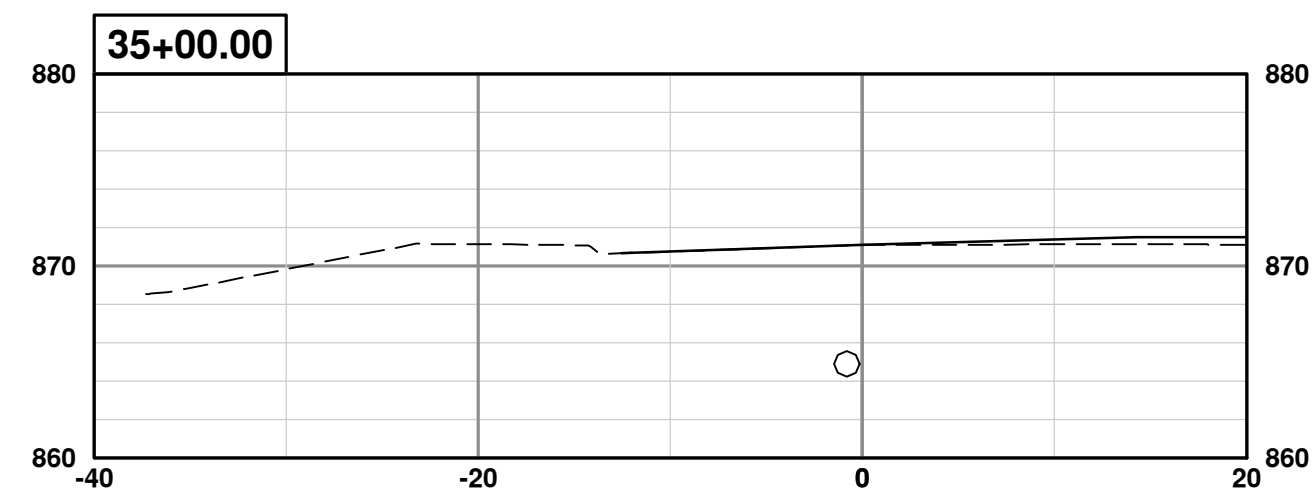
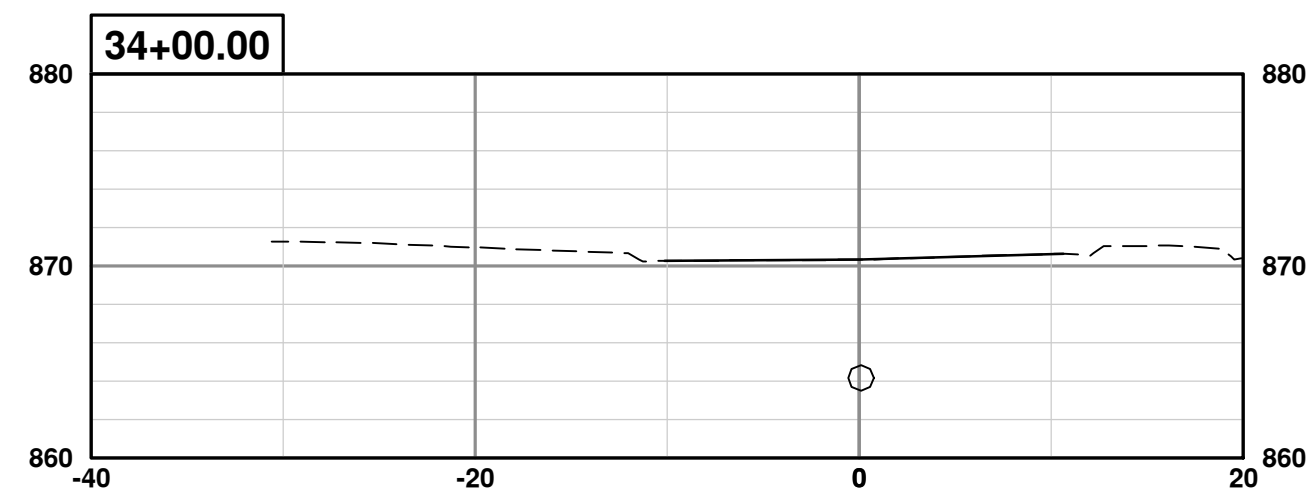
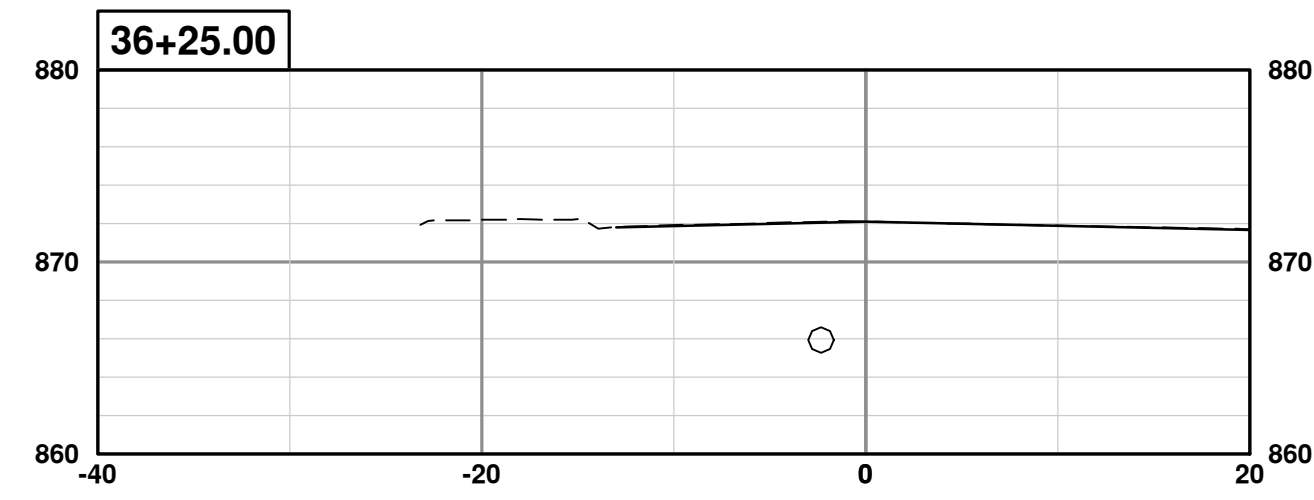
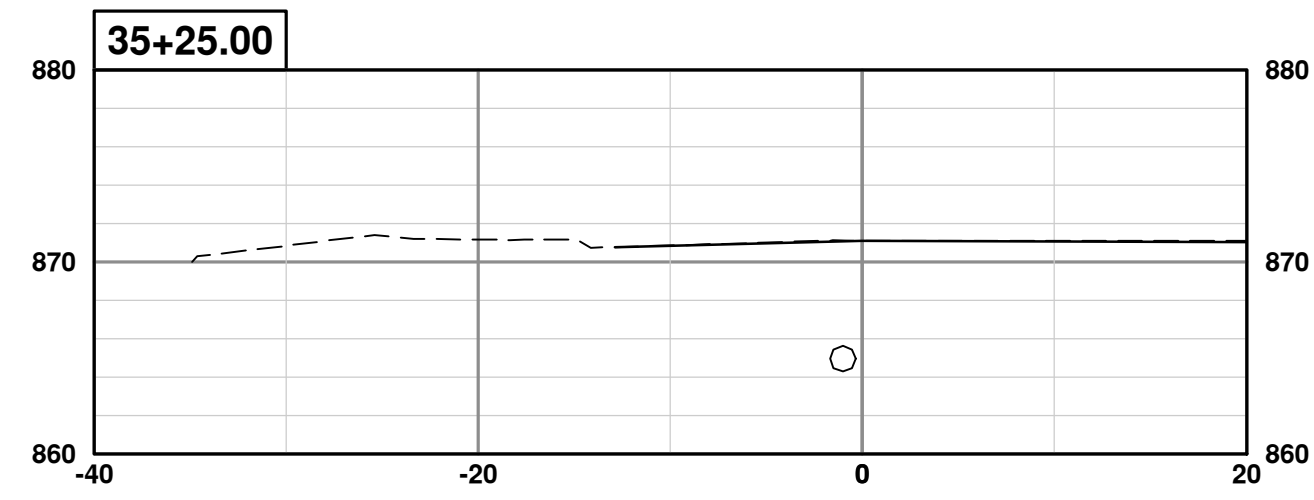
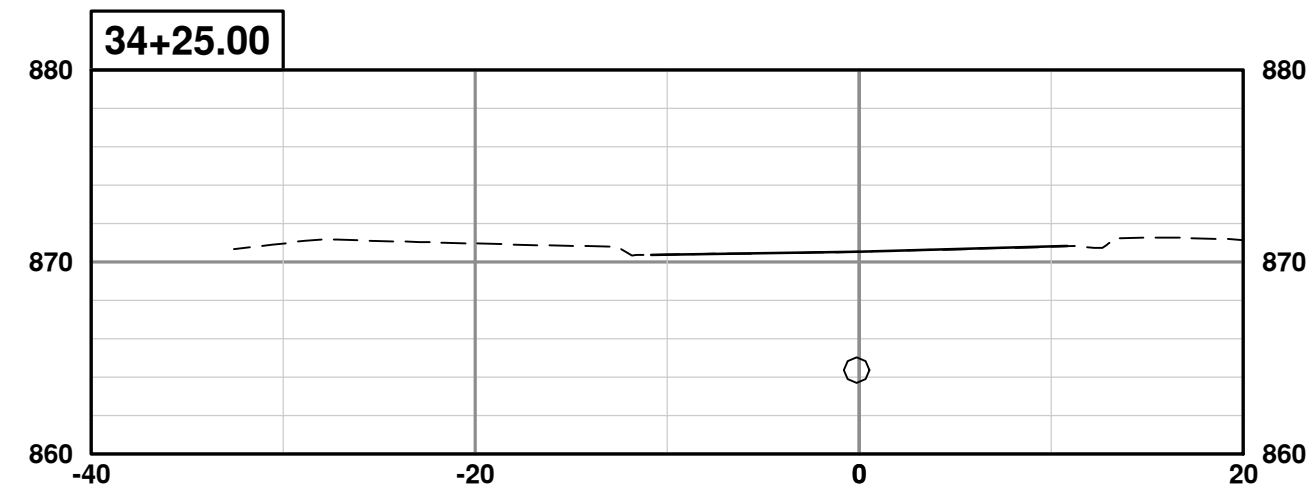
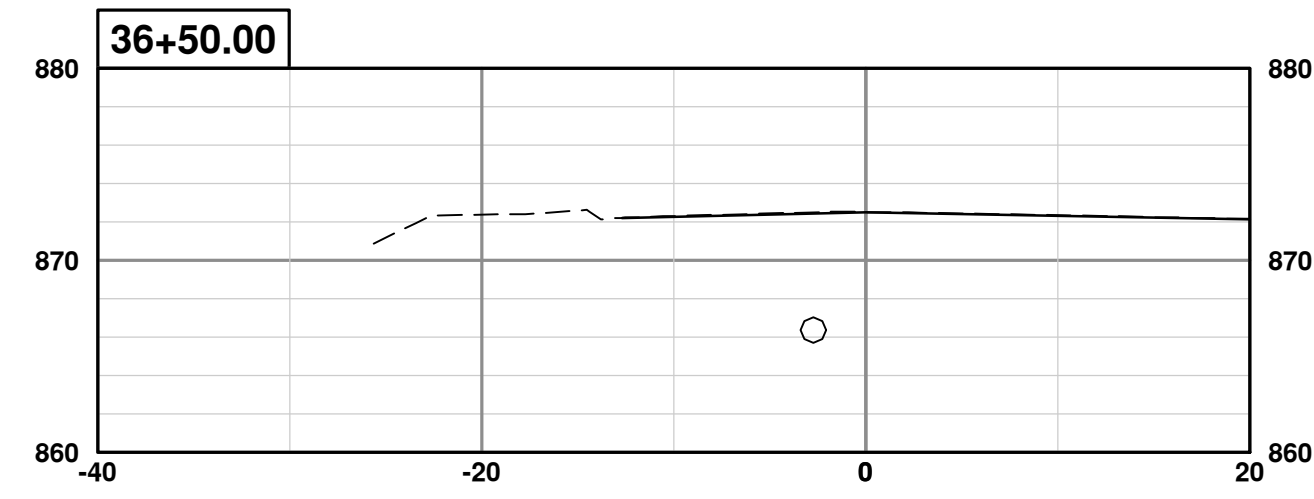
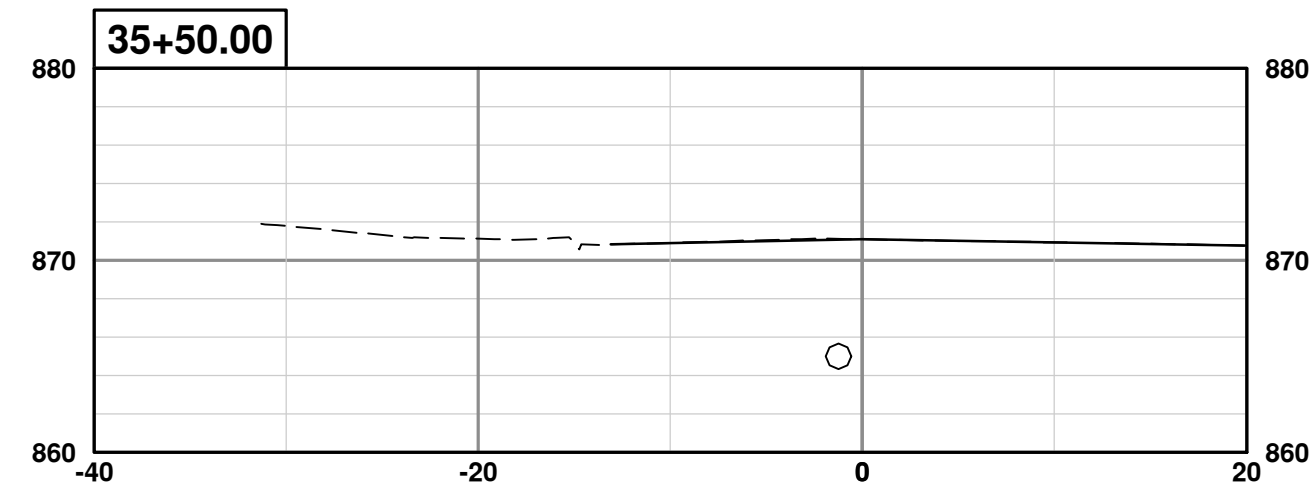
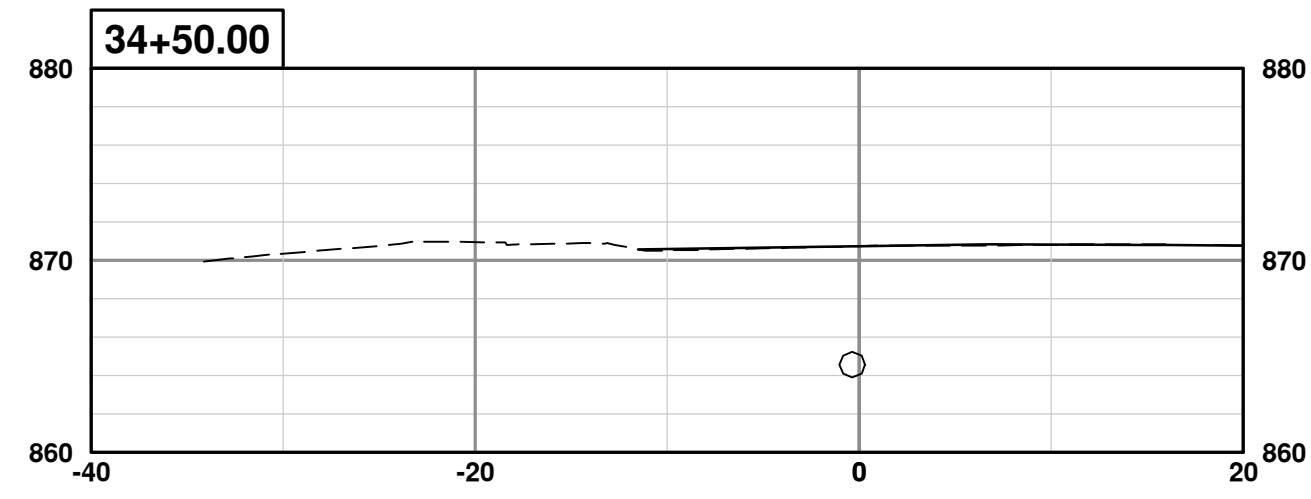
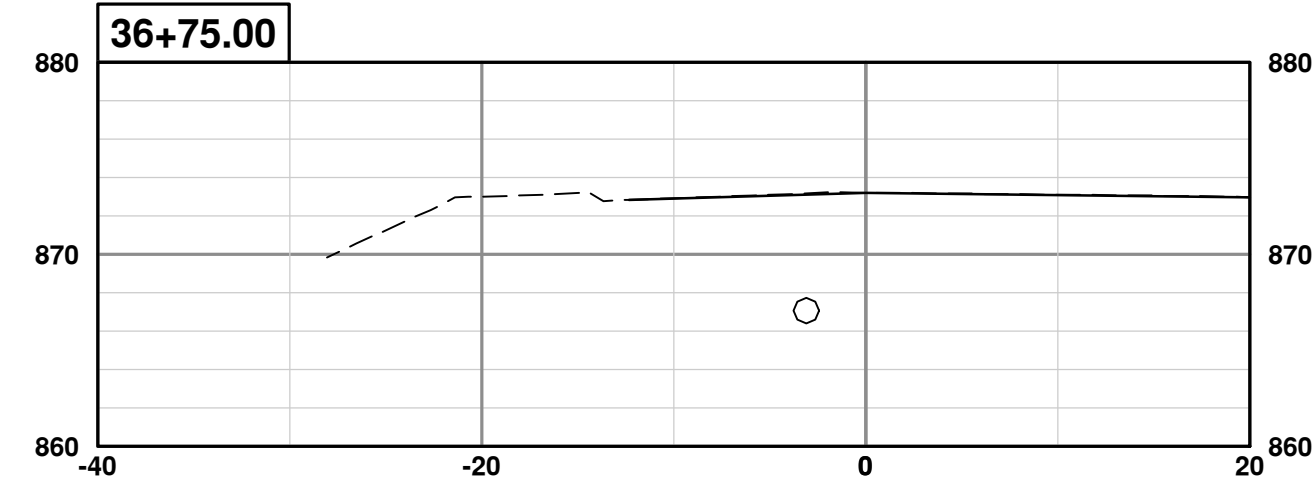
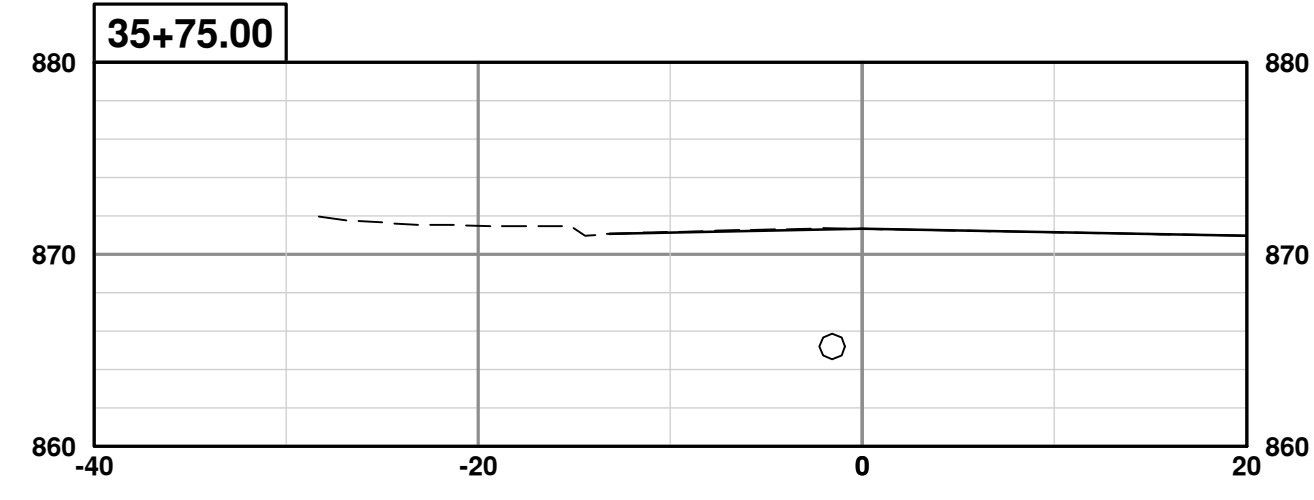
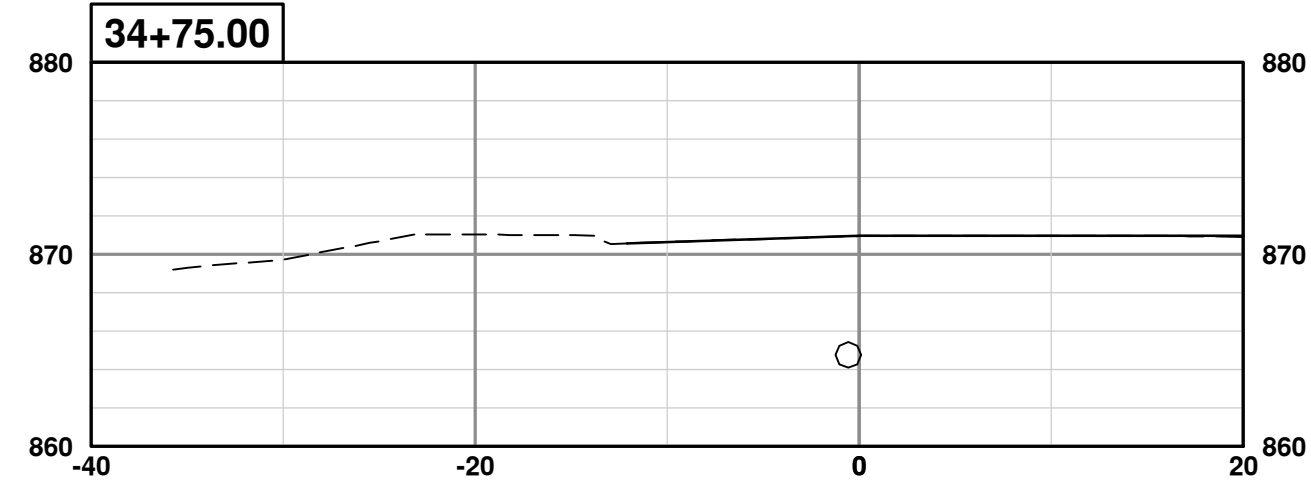
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**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
CROSS SECTIONS  
NORTHBOUND 45+75 TO 49+25

SCALE PLAN: 1" = 10'  
PROFILE: 1" = 10'

DRAWING No. 2021023-69

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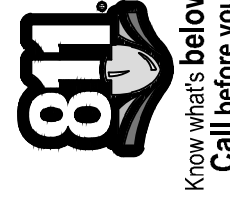
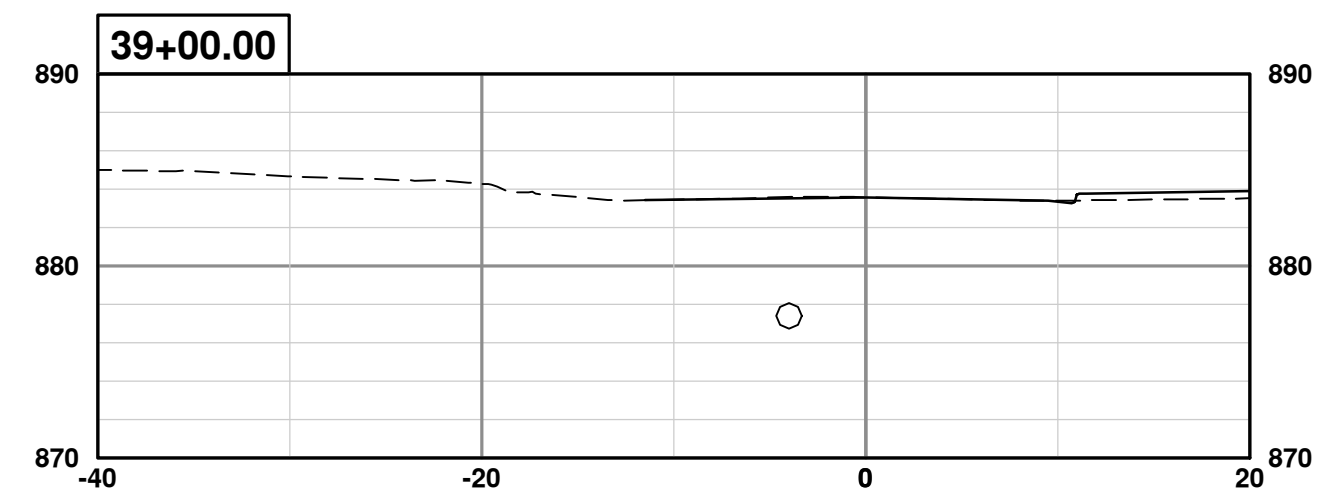
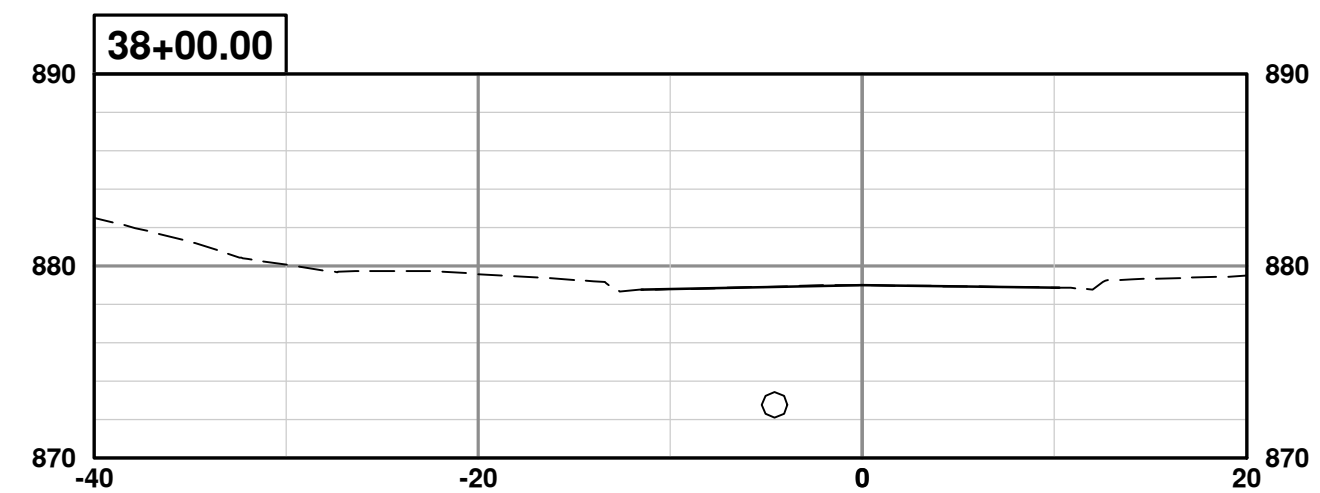
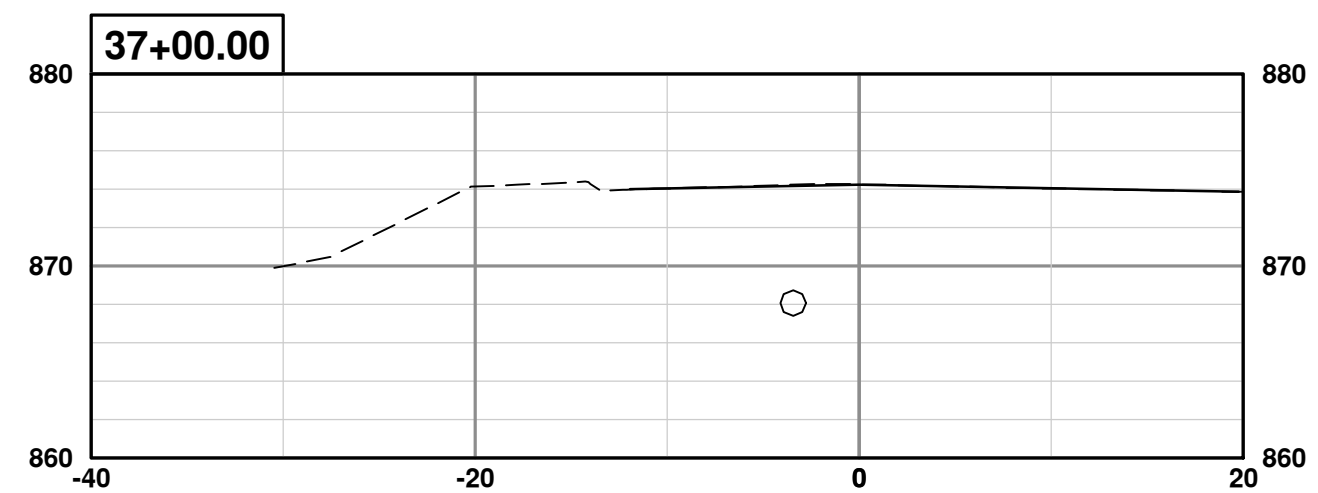
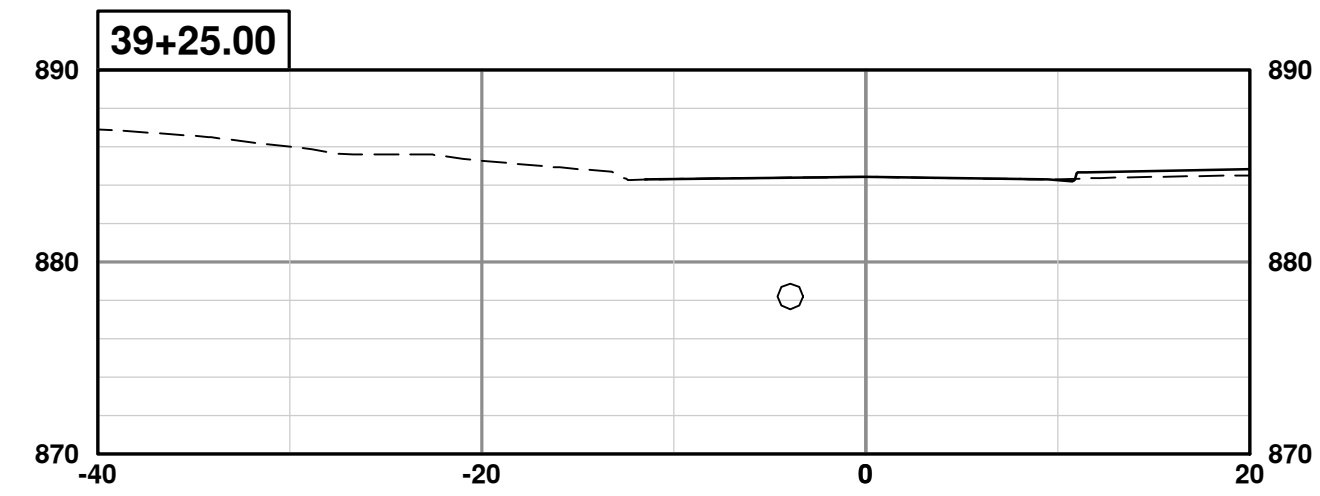
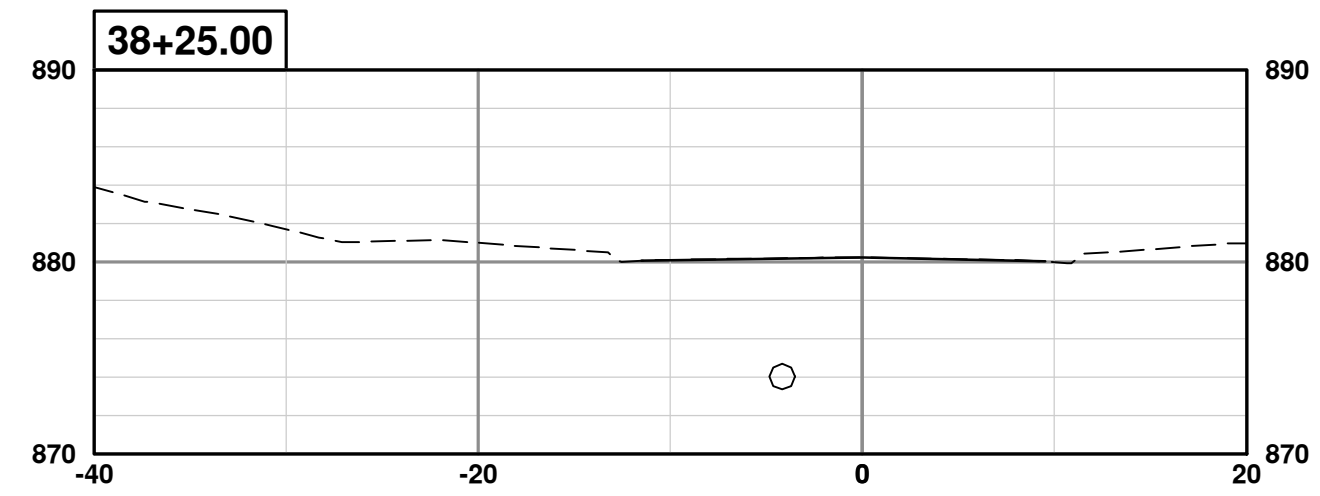
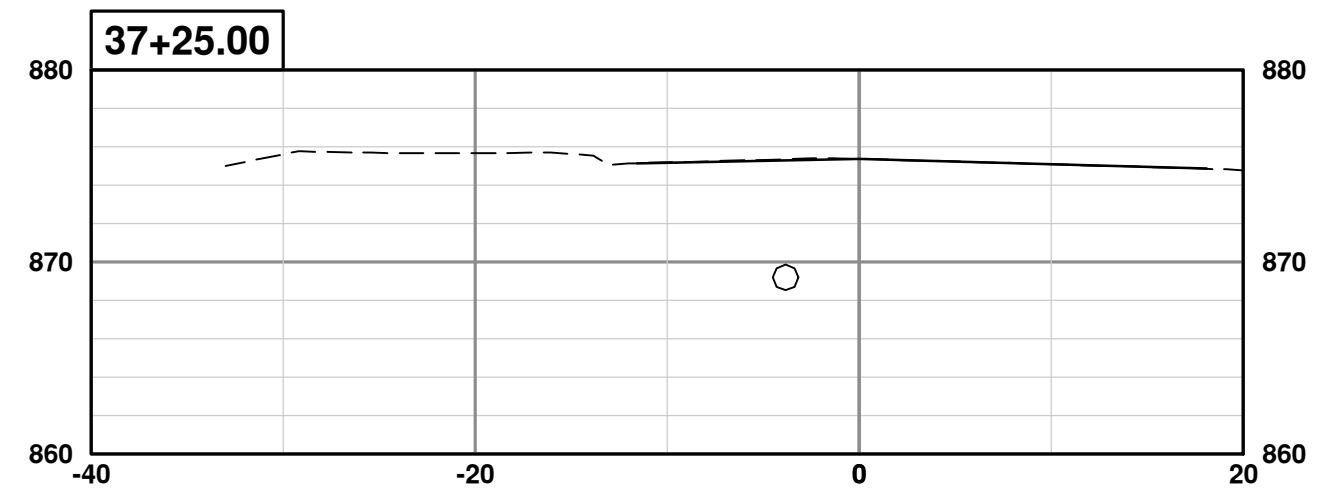
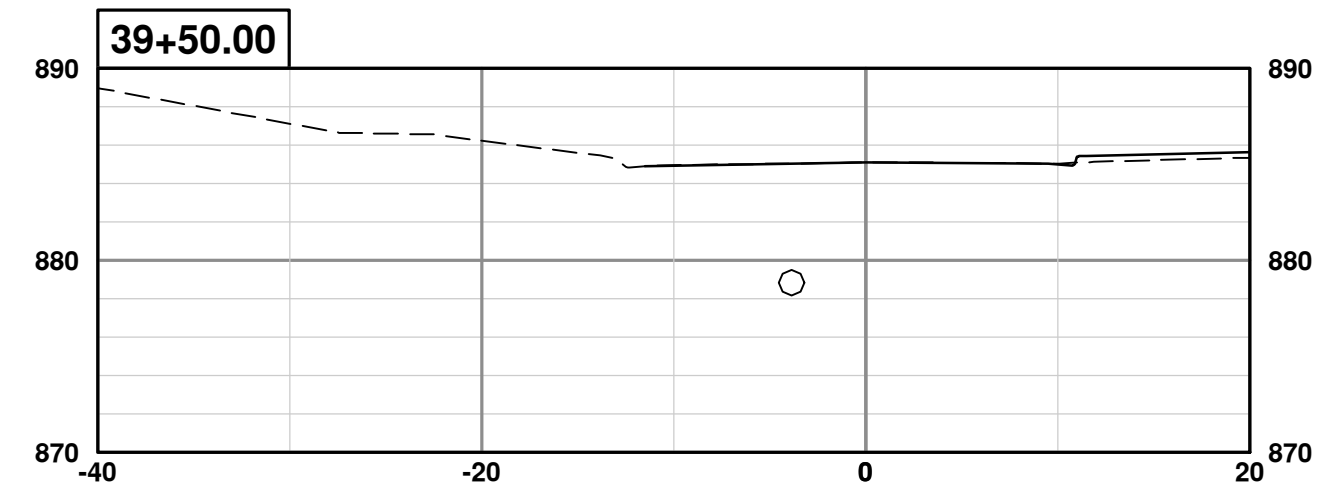
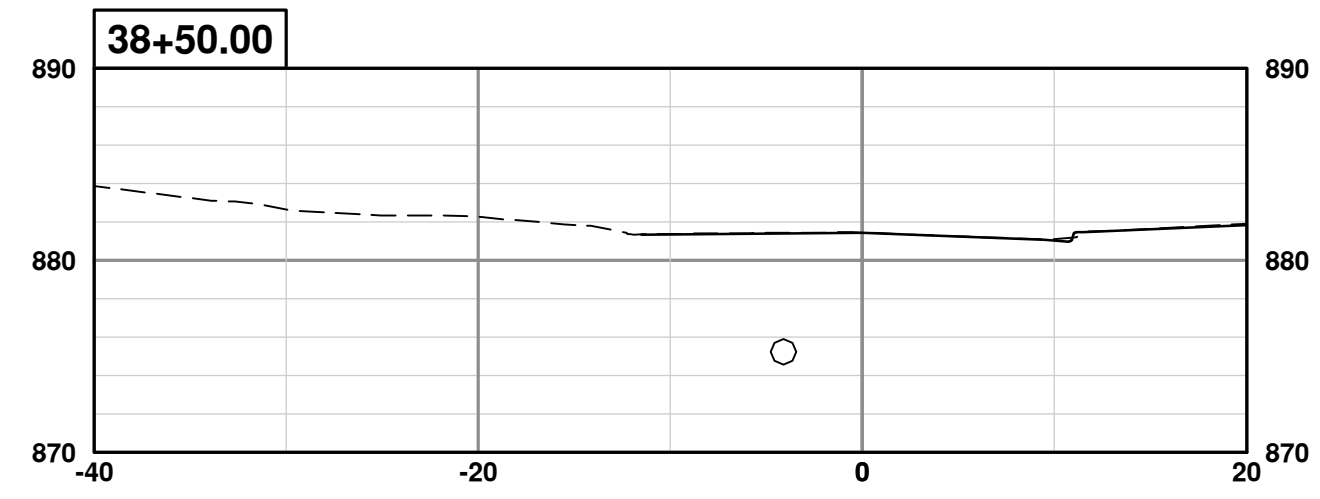
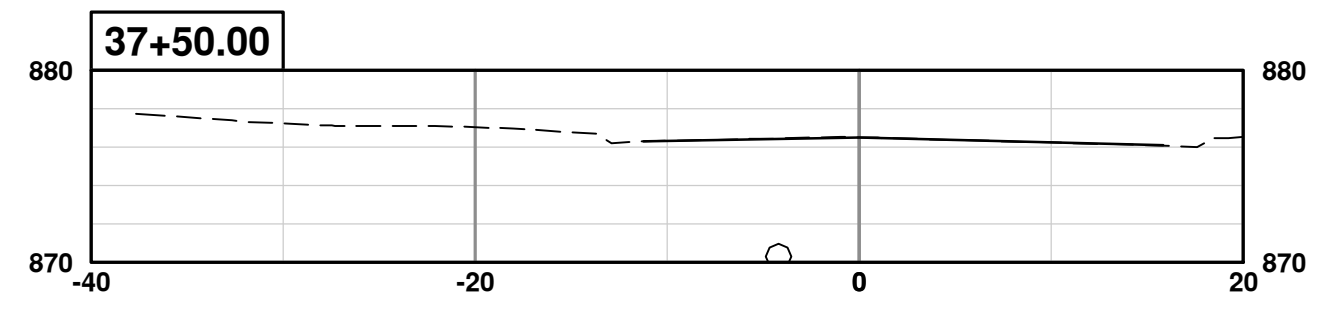
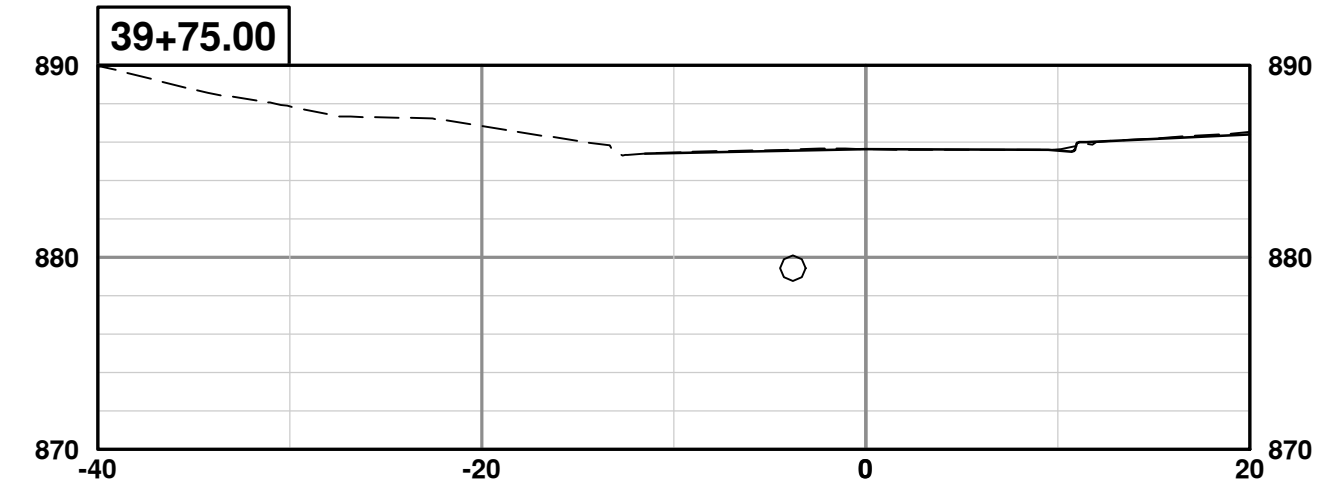
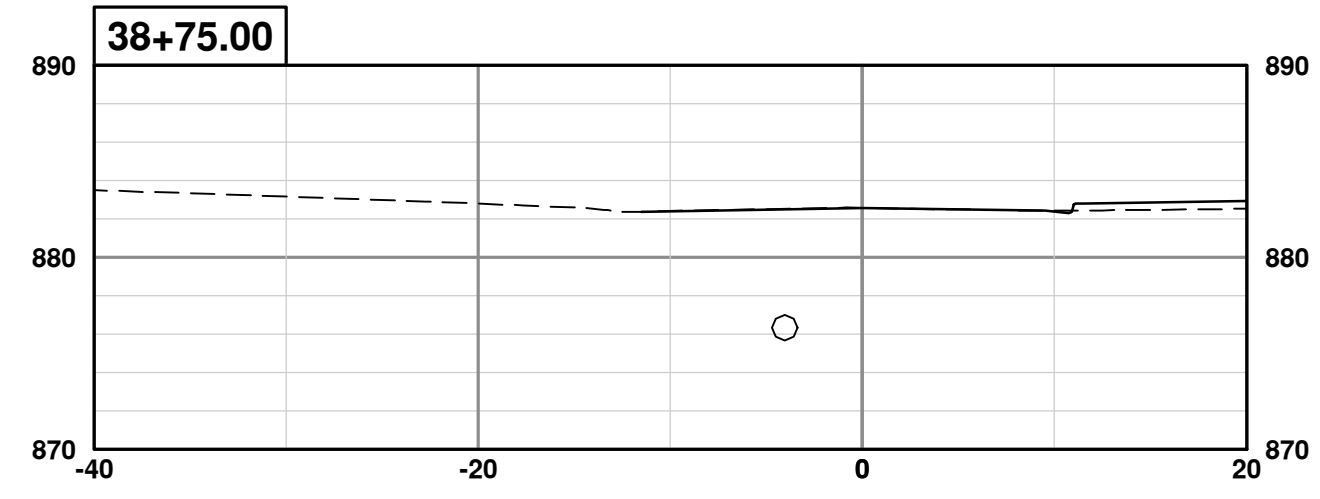
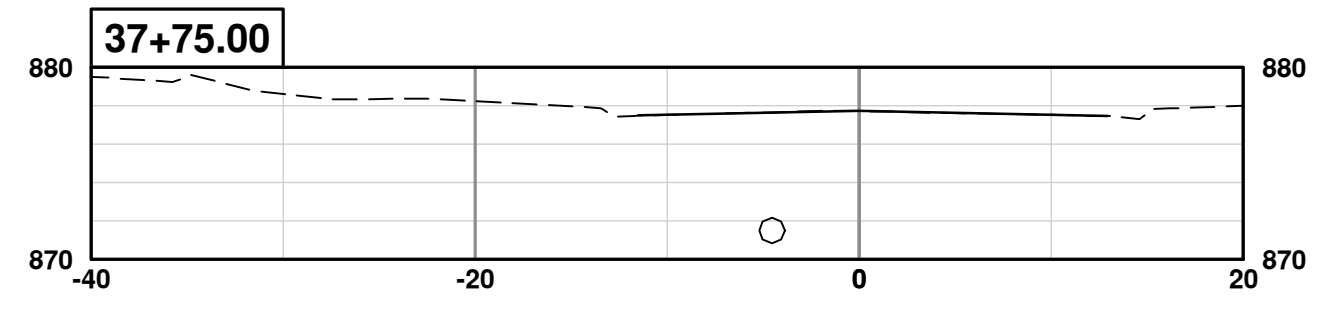


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**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
CROSS SECTIONS  
SOUTHBOUND 34+00 TO 36+75

SCALE PLAN: 1" = 10'  
PROFILE: 1" = 10'

DRAWING No. 2021023-70

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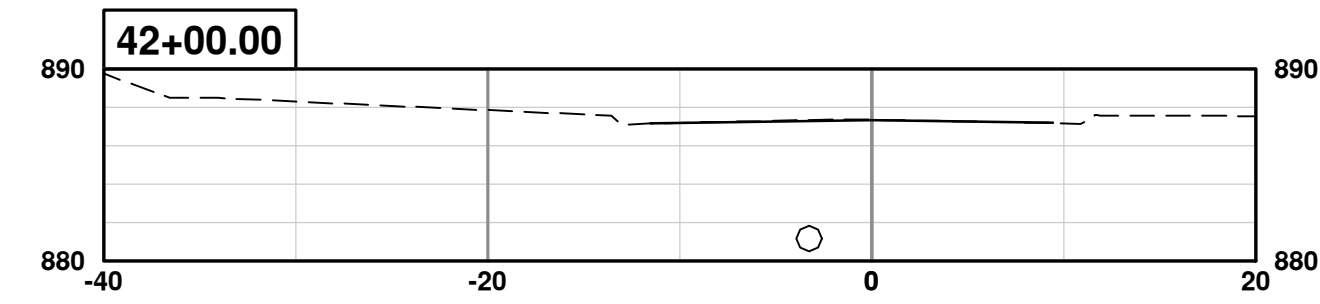
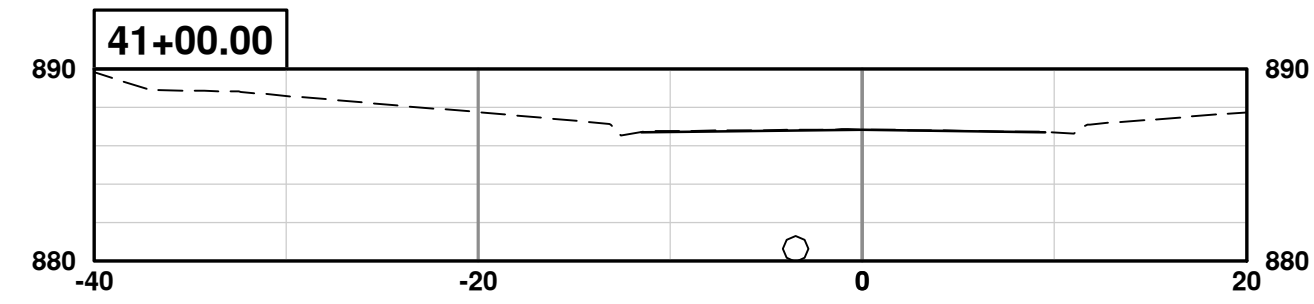
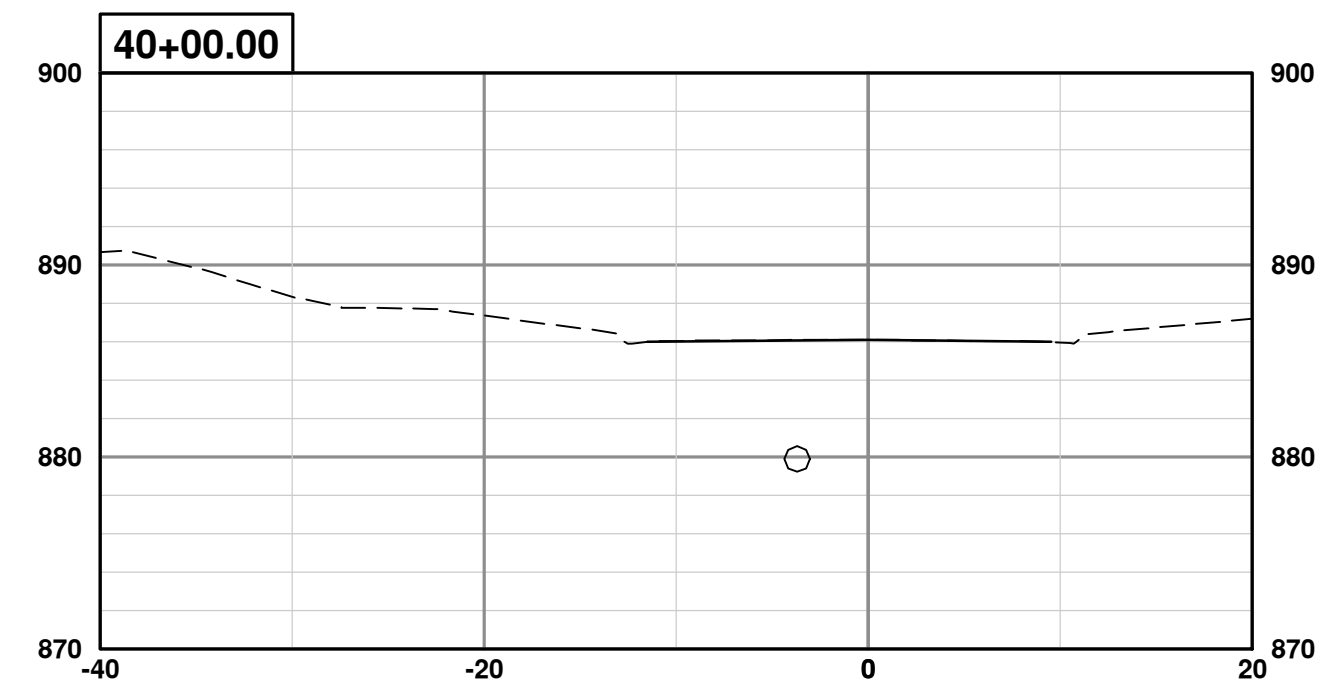
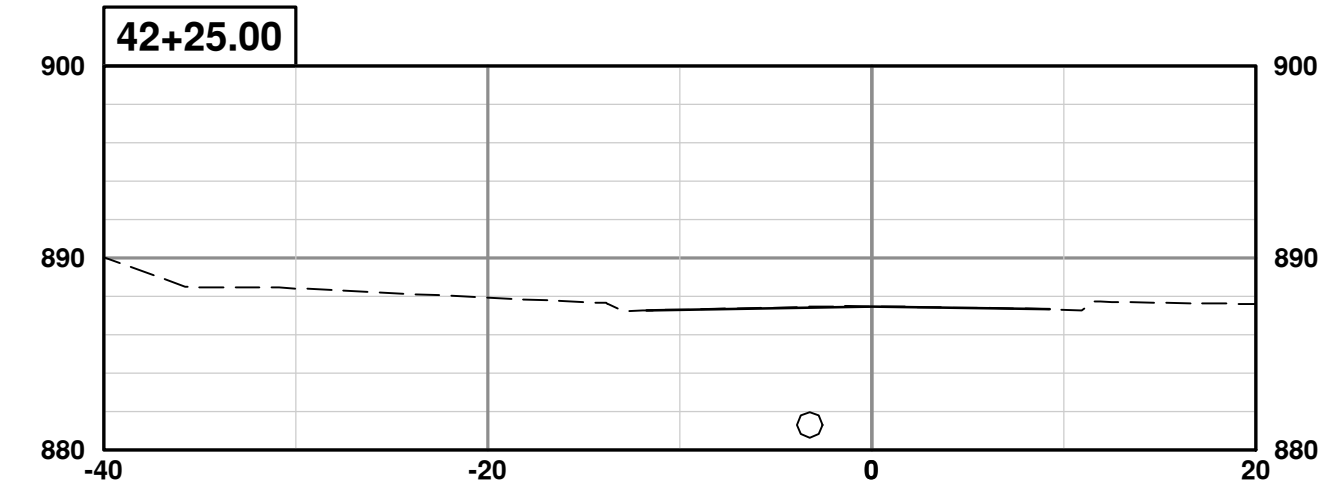
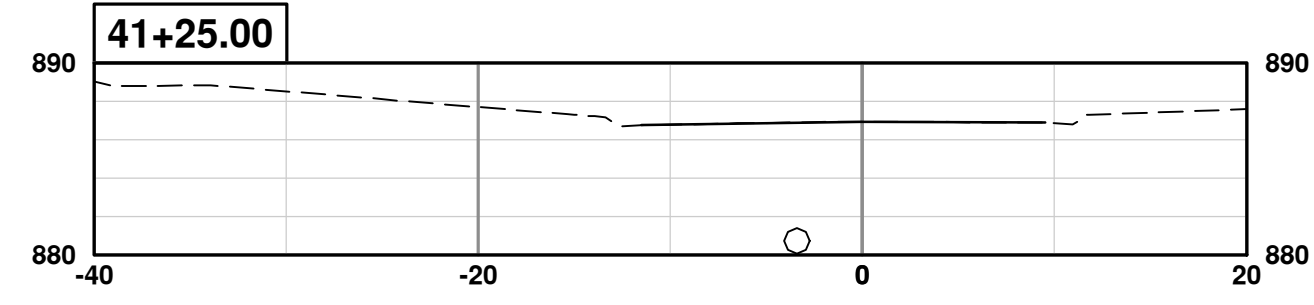
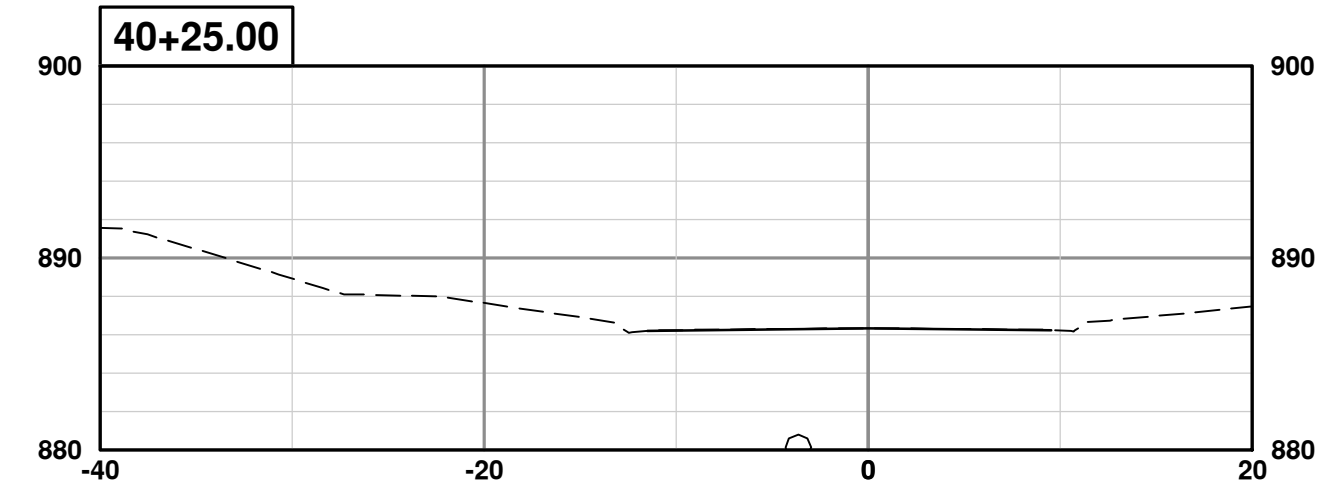
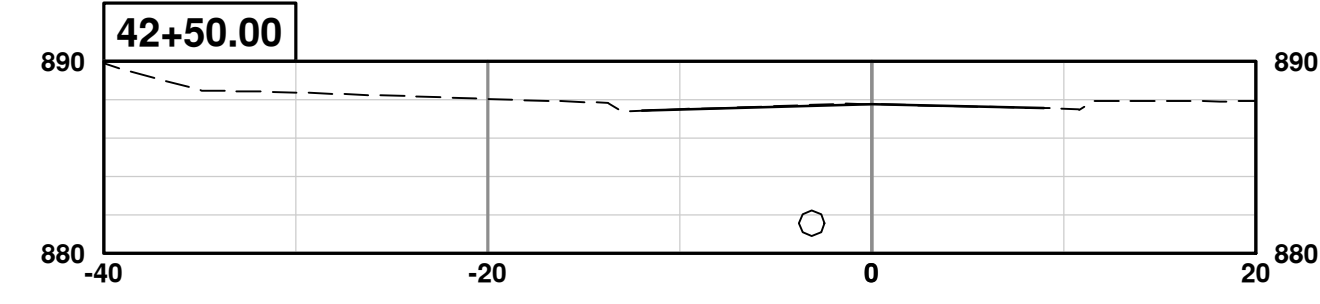
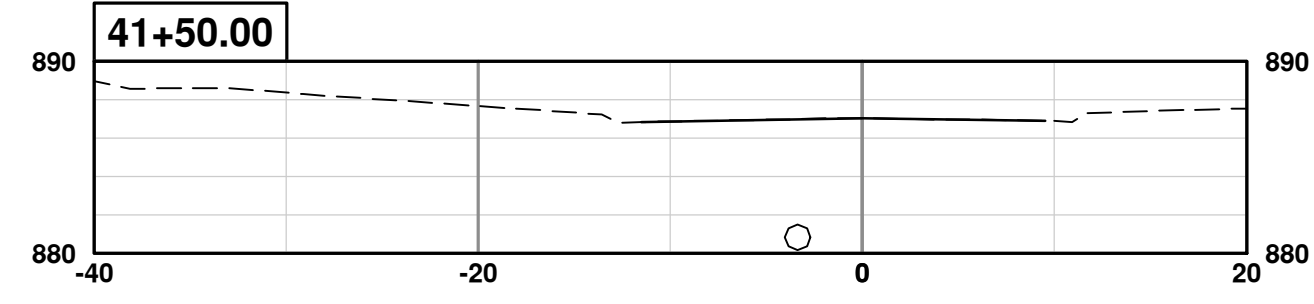
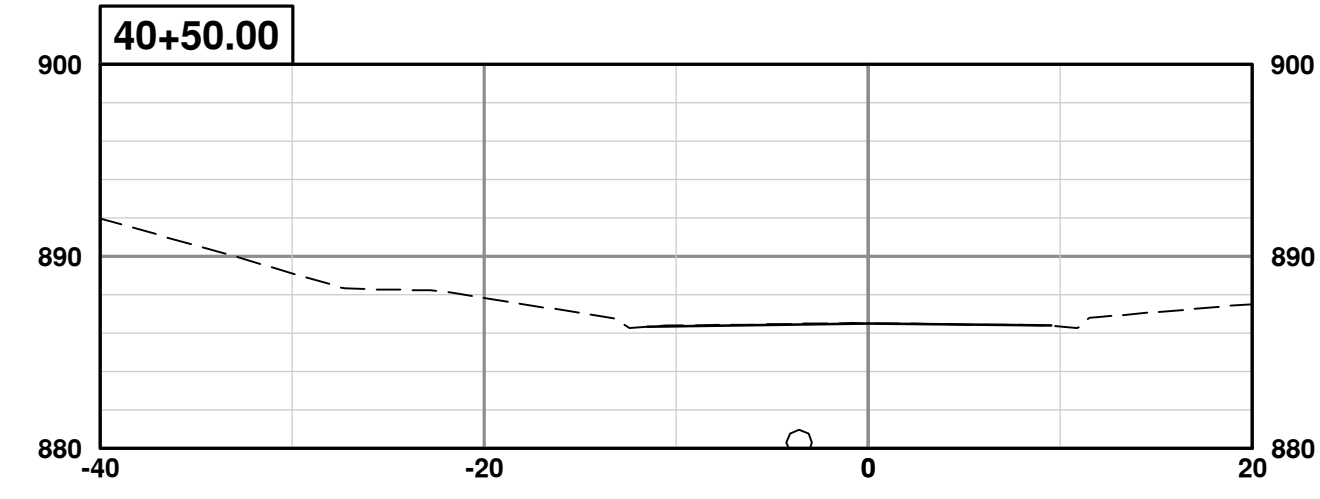
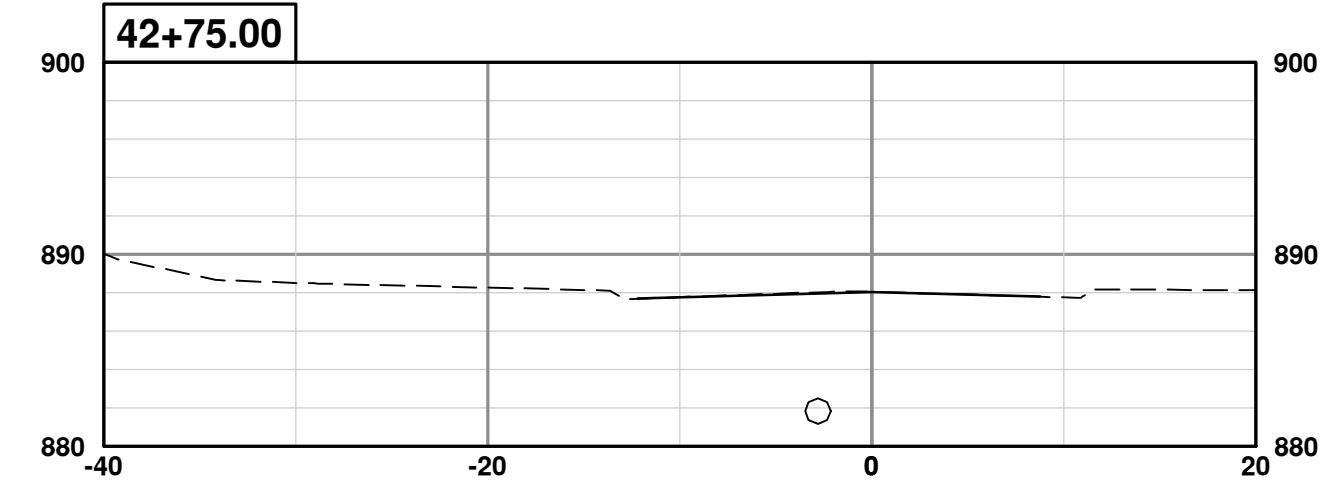
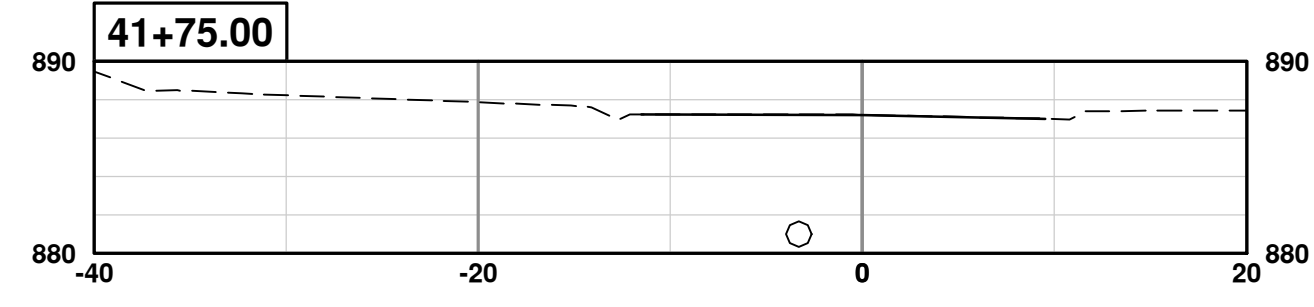
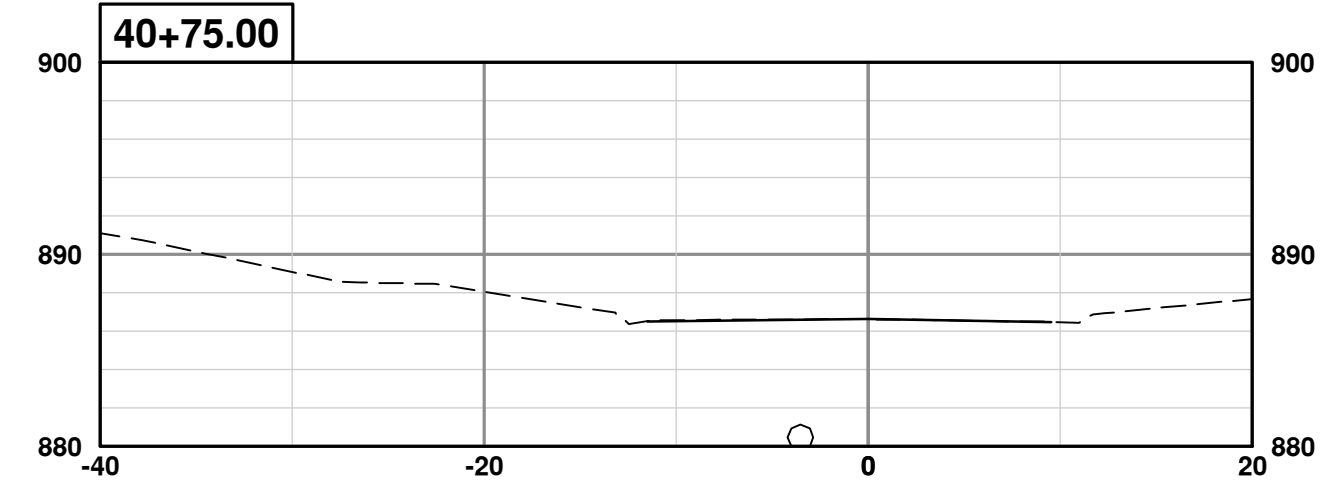


**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
CROSS SECTIONS  
SOUTHBOUND 37+00 TO 39+75

SCALE PLAN: 1" = 10'  
PROFILE: 1" = 10'

DRAWING No. 2021023-71

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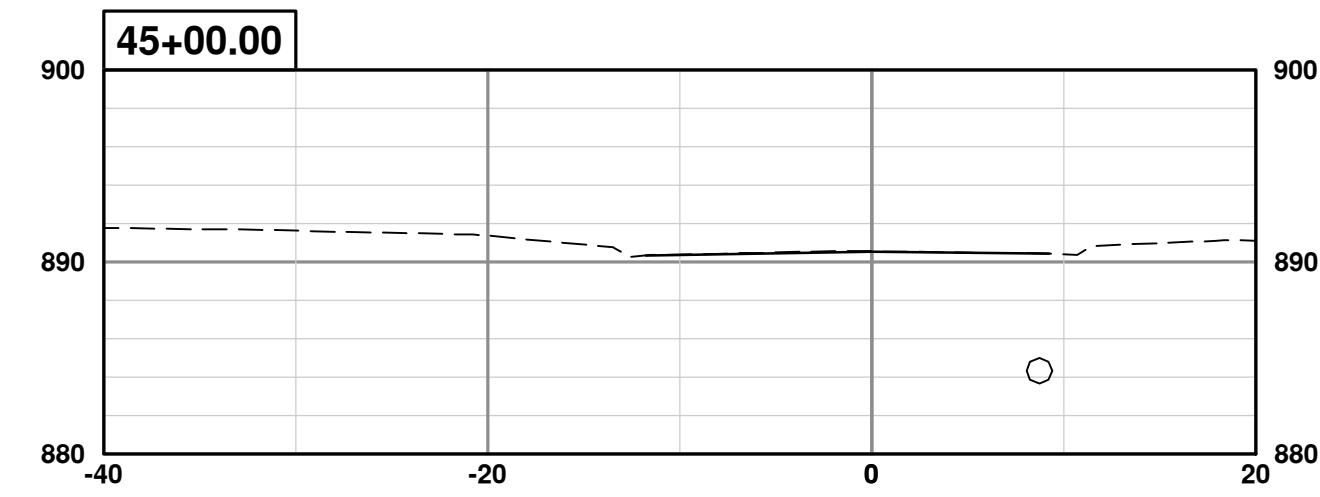
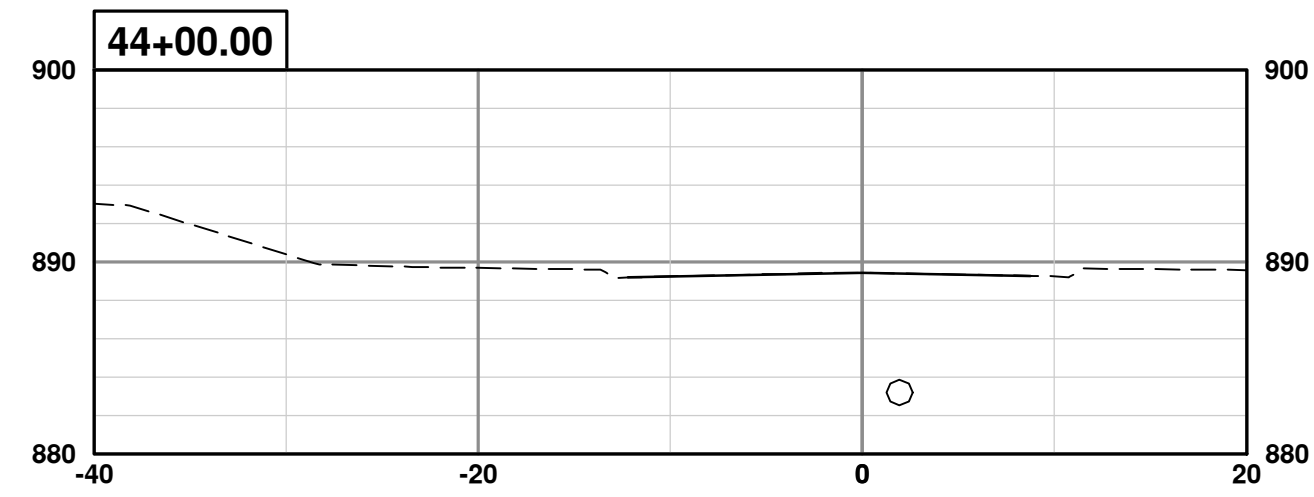
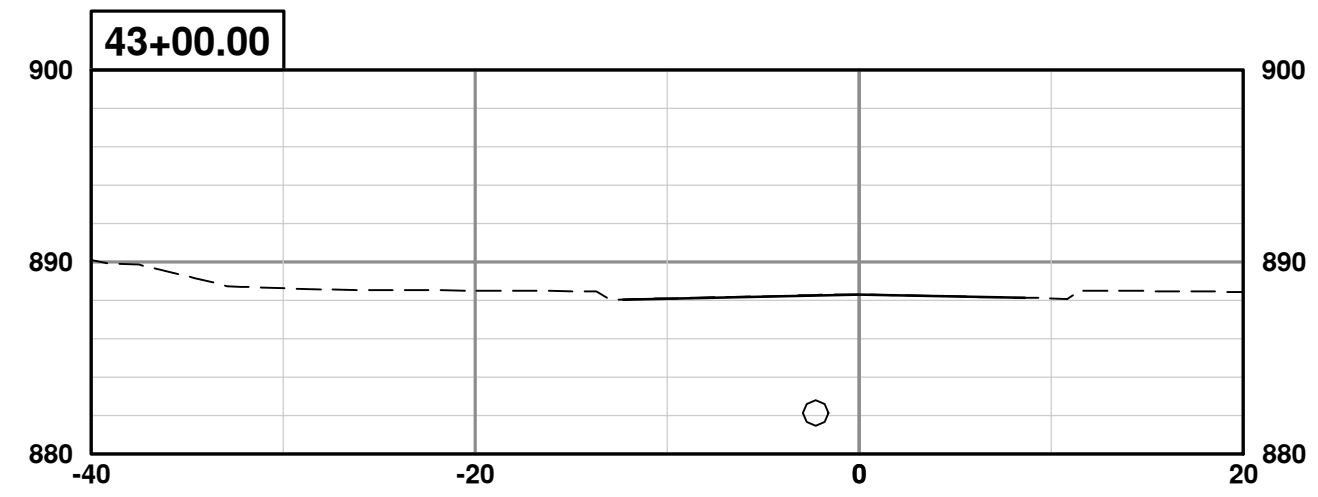
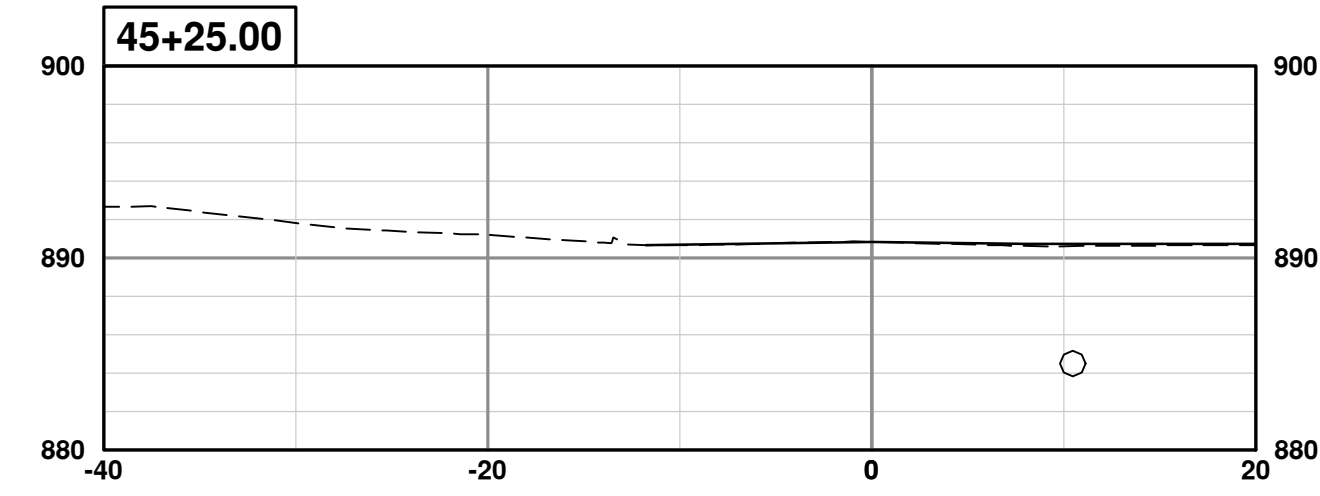
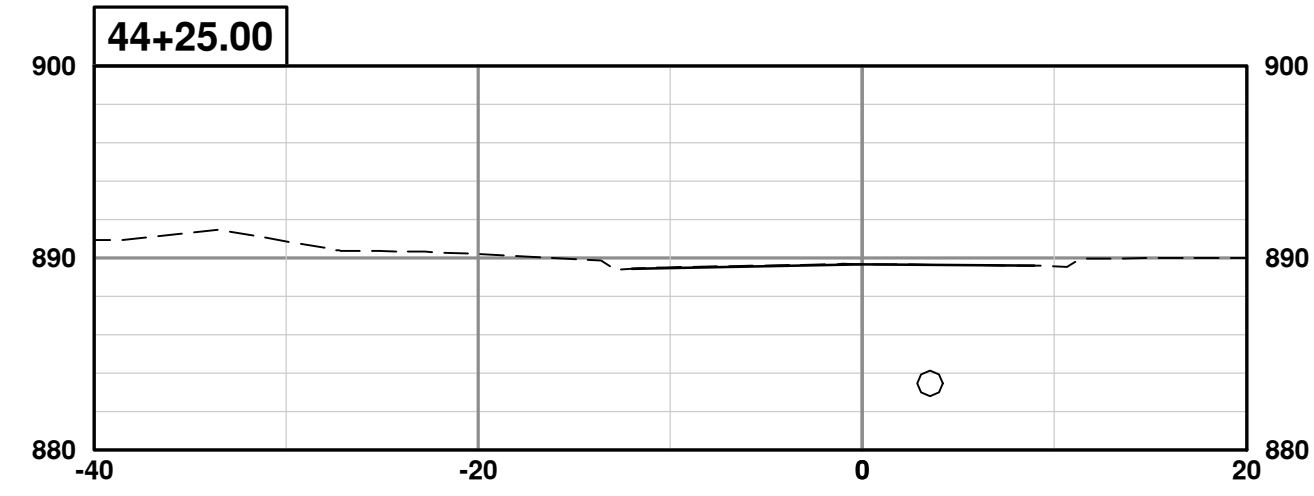
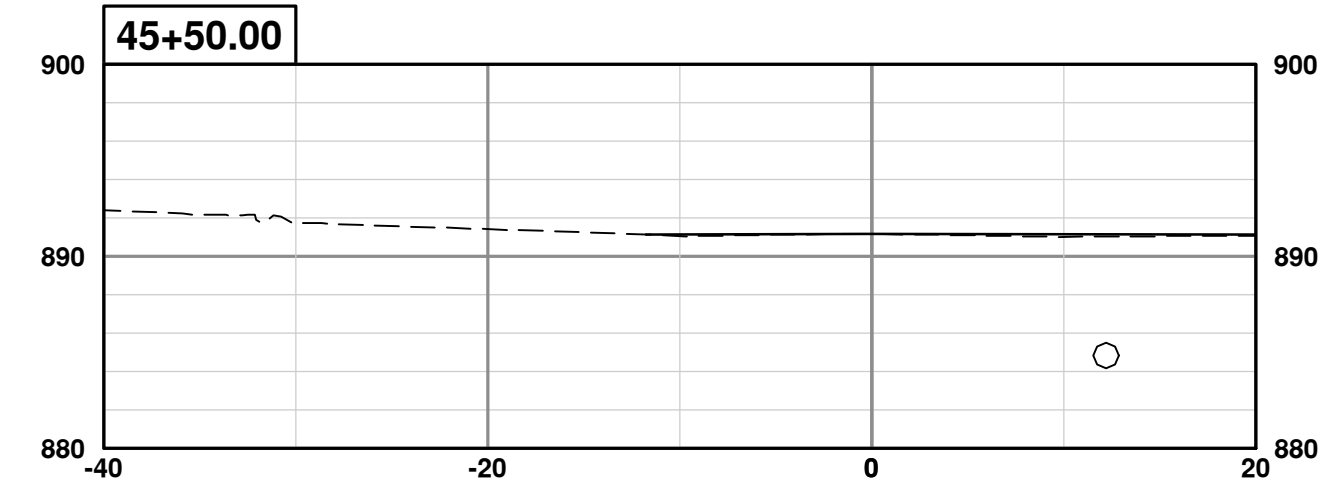
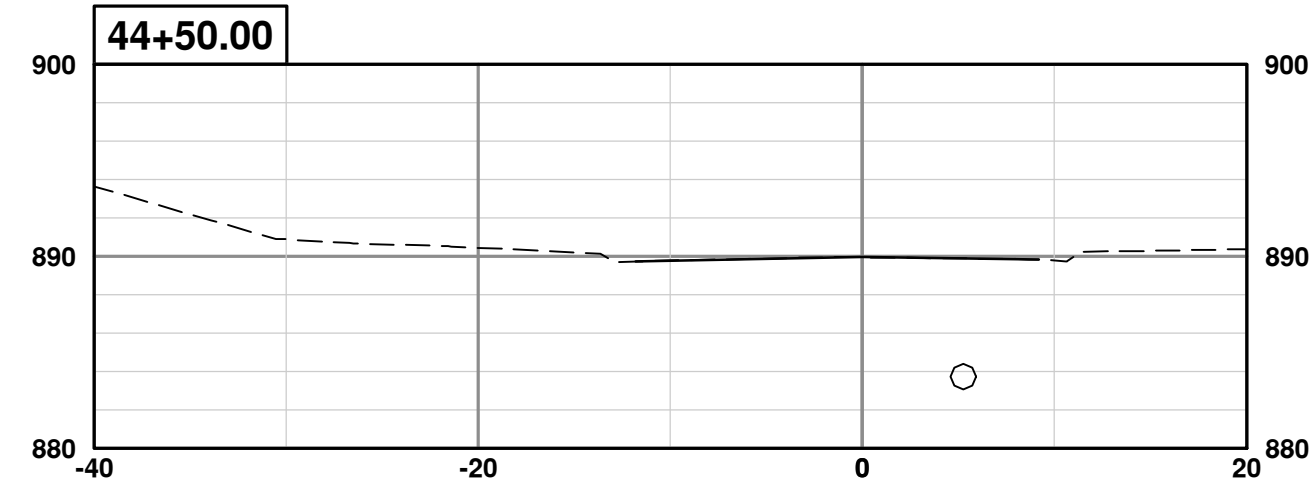
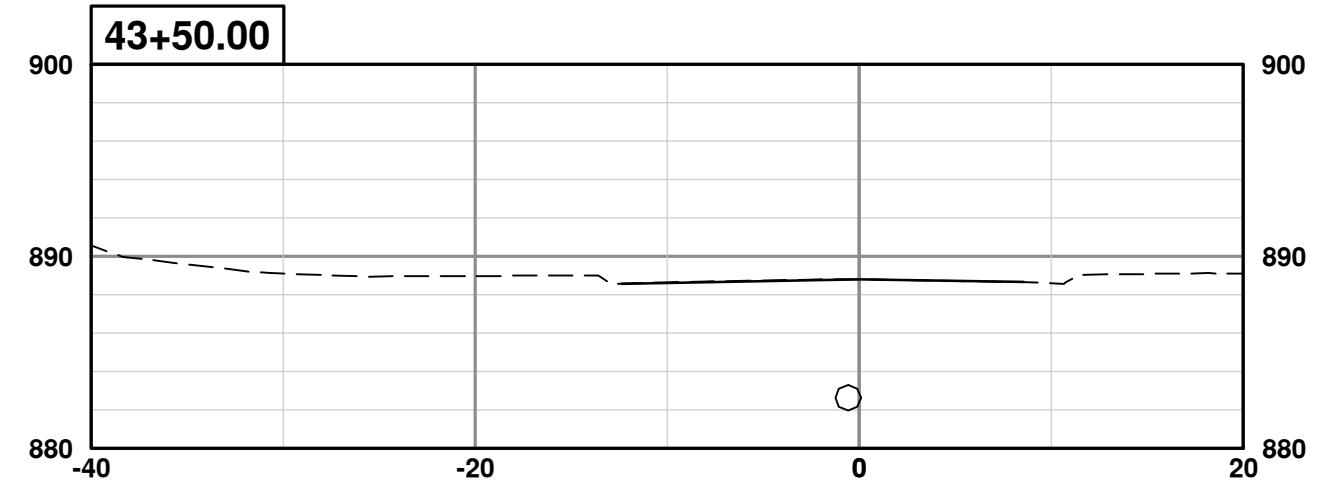
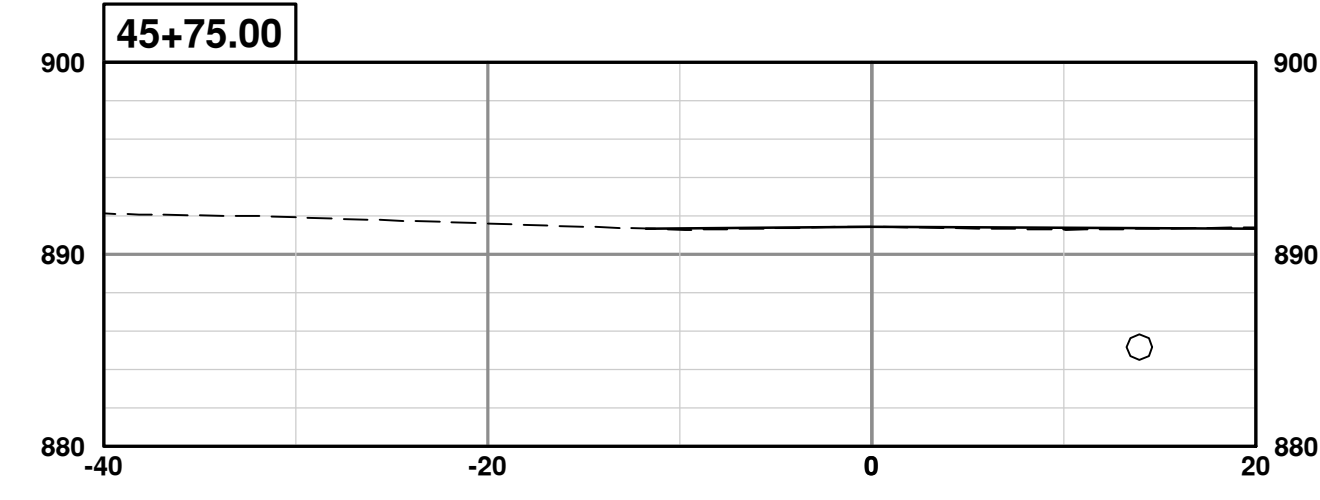
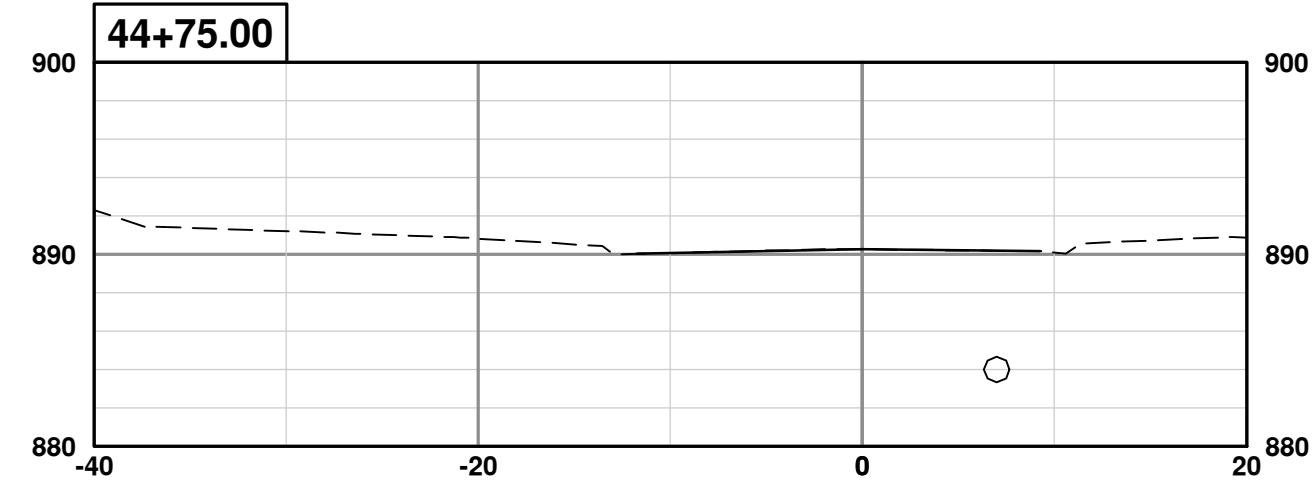
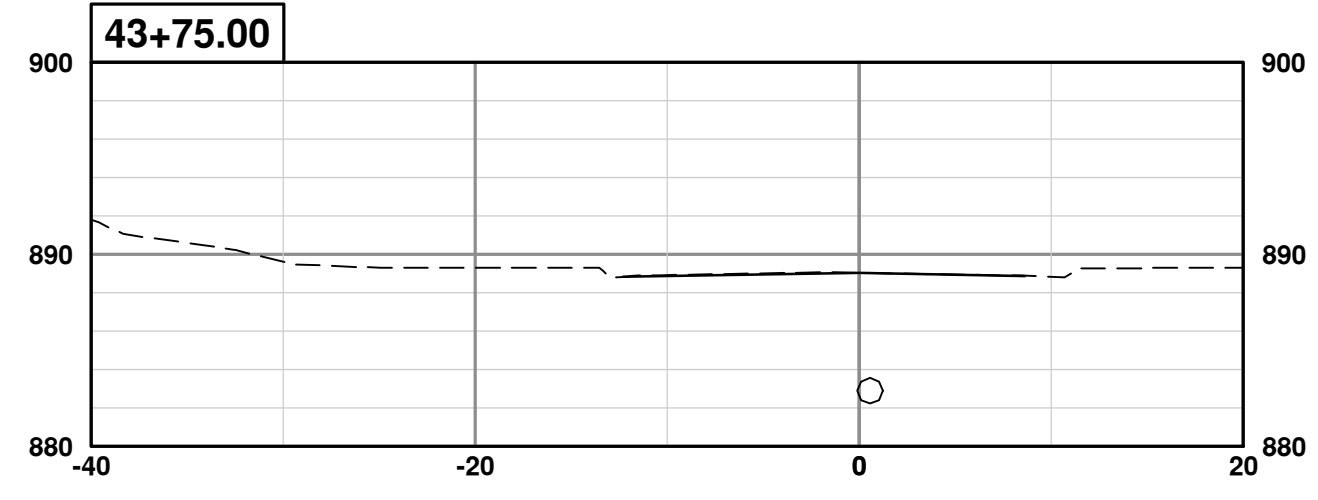


**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
CROSS SECTIONS  
SOUTHBOUND 40+00 TO 42+75

SCALE PLAN: 1" = 10'  
PROFILE: 1" = 10'  
DRAWING No. 2021023-72



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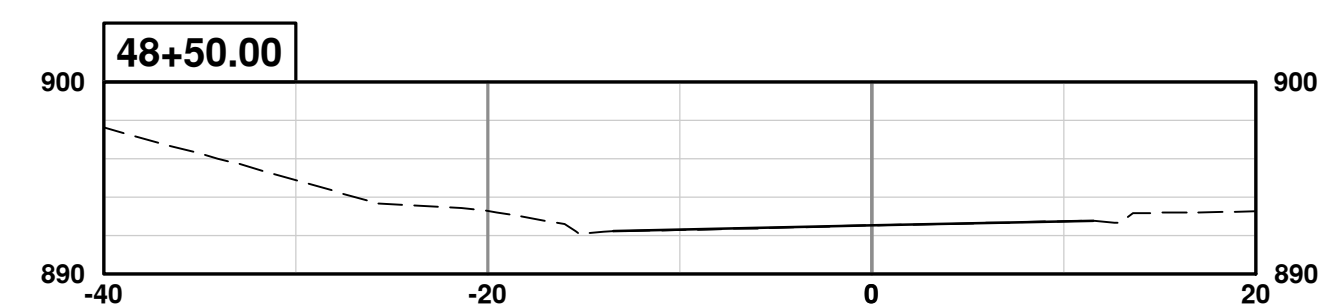
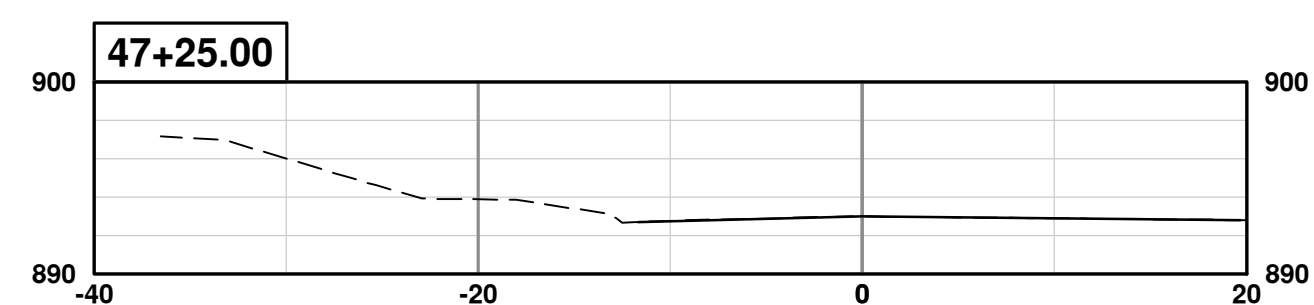
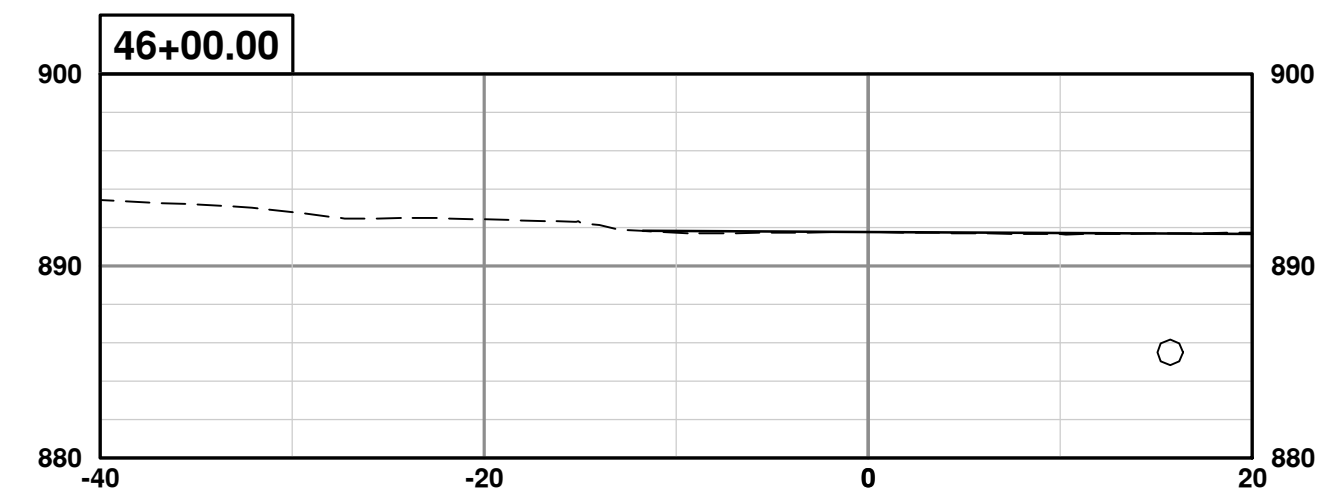
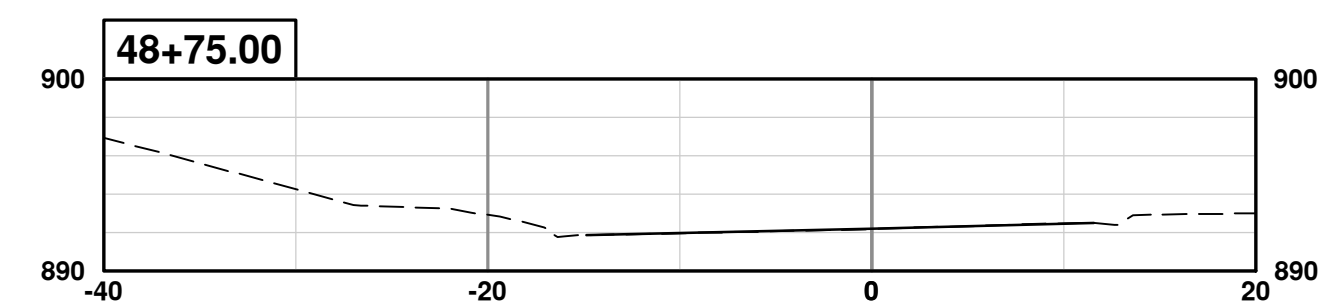
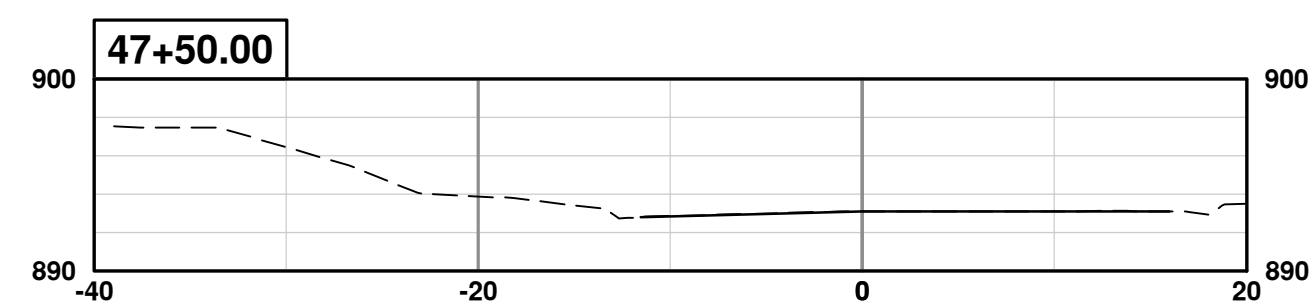
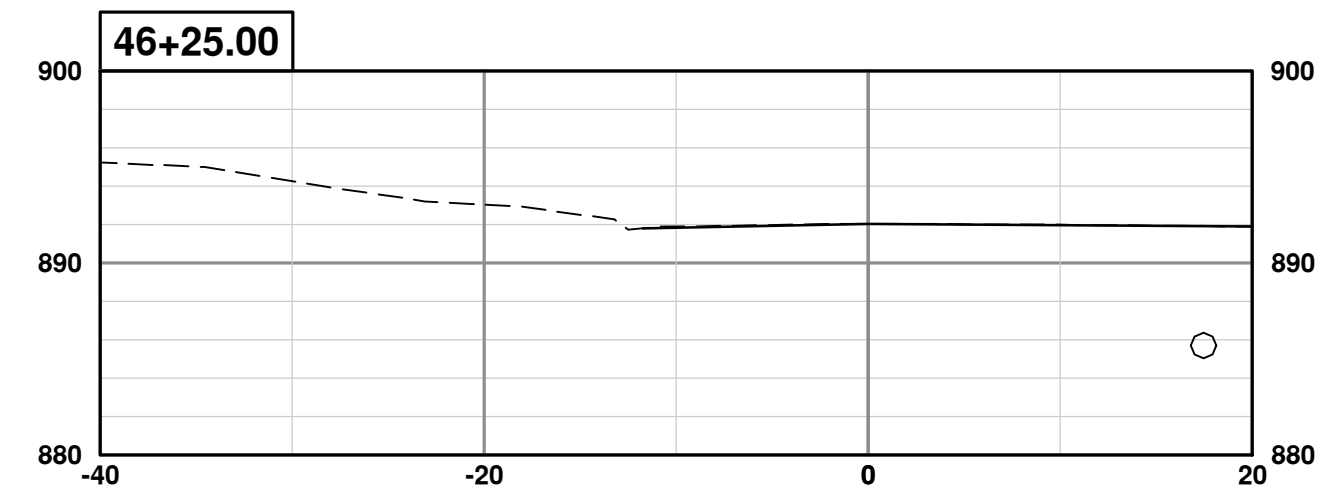
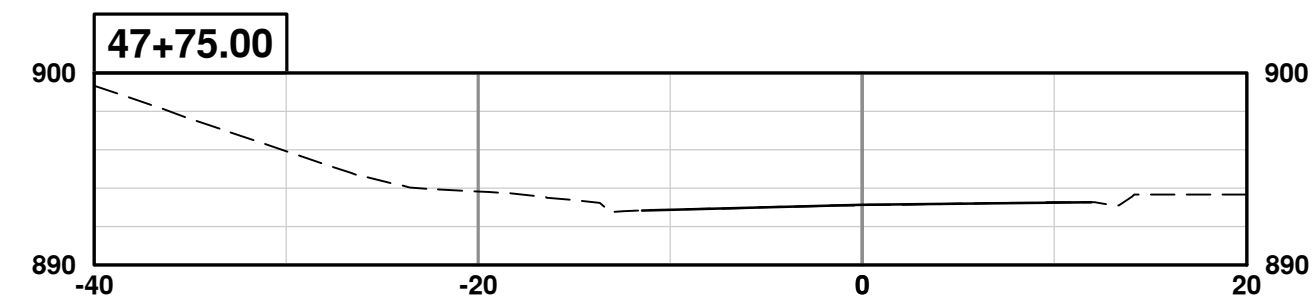
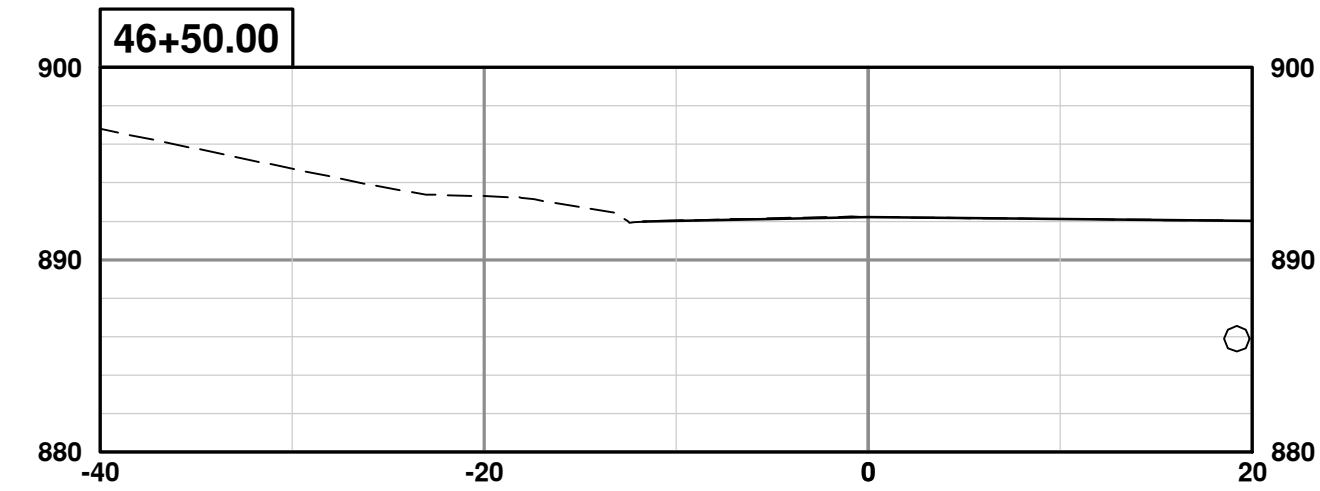
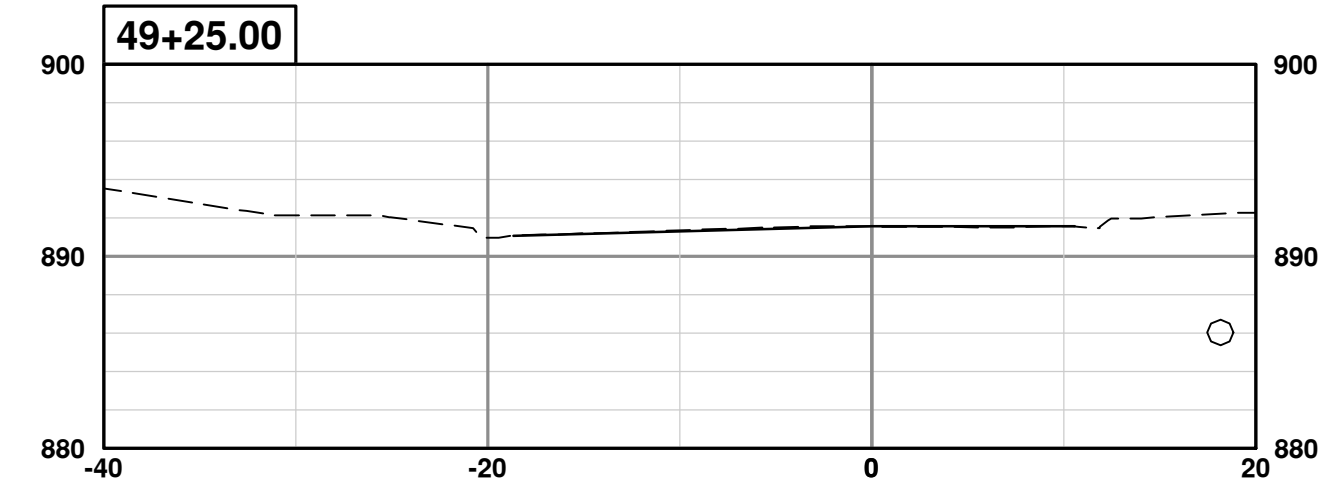
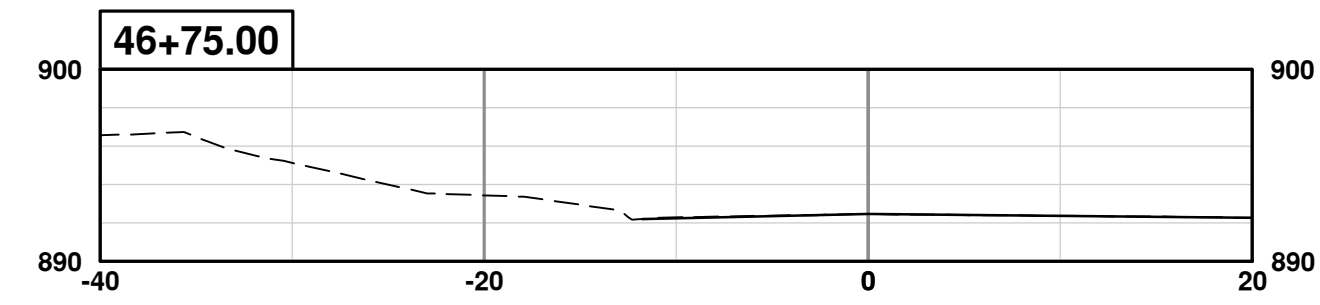
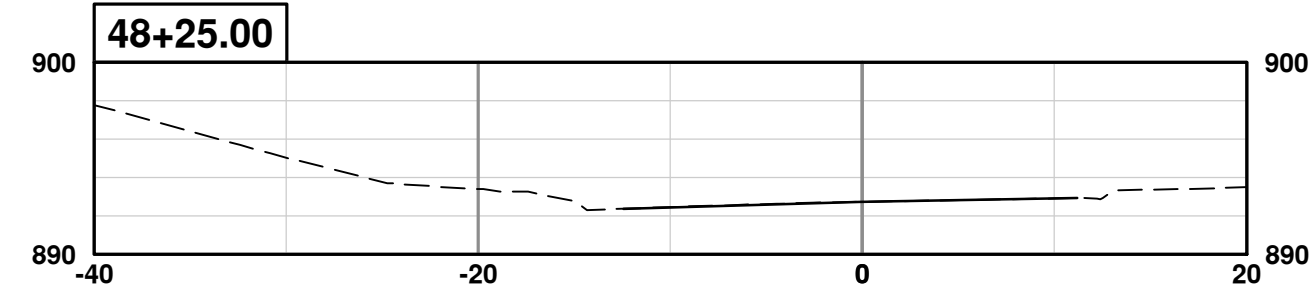
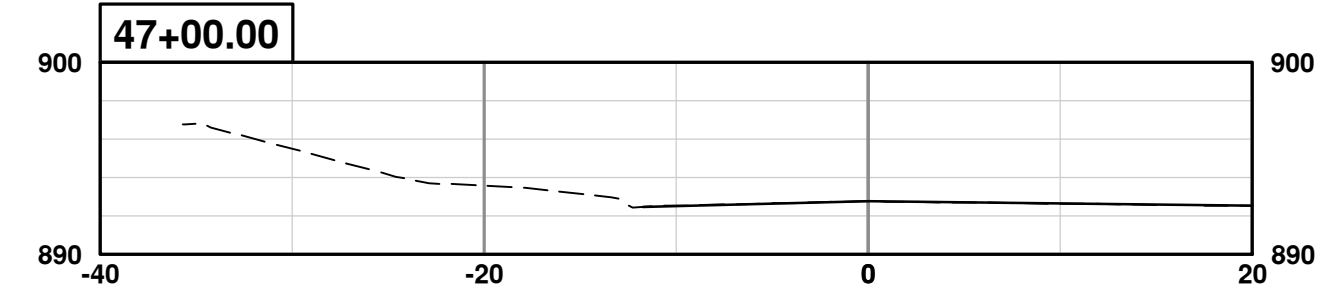
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**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
CROSS SECTIONS  
SOUTHBOUND 43+00 TO 45+75

SHEET No. **73 OF 74**  
SCALE PLAN: 1" = 10'  
PROFILE: 1" = 10'  
DRAWING No. **2021023-73**

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**CITY OF ANN ARBOR - PUBLIC SERVICES - ENGINEERING**  
**EARHART ROAD IMPROVEMENTS (GEDDES - US23)**  
CROSS SECTIONS  
SOUTHBOUND 46+00 TO 49+25

SCALE PLAN: 1" = 10'  
PROFILE: 1" = 10'

DRAWING No. **2021023-74**